



Master Plan Amendment #2020-00003
CDD Concept Plan #2020-00003
Zoning Text Amendment #2020-00006
2412, 2514 & 2610 Richmond Highway; 2500 Oakville Street; 400 Fannon Street; 300, 403, 405 & 420 Swann Avenue; 400 Calvert Avenue

Application	General Data	
Project Name: Oakville Triangle	PC Hearing:	December 1, 2020
	CC Hearing:	December 12, 2020
	If approved, expiration:	January 1, 2031 (10 years)
	Existing Site Area:	560, 410 square feet (12.87 acres) excl. ROW
Location: 2412, 2514 & 2610 Richmond Highway, 2500 Oakville St, 400 Fannon St, 300, 403, 405 & 420 Swann Ave, 400 Calvert Ave	Zone:	CDD #24 (Coordinated Development District)
	Proposed Use:	Mixed Use
	Gross Floor Area:	1,370,000 sf (excl. below grade parking garages)
Applicant: BRE/DP (Stonebridge), represented by Duncan Blair, Attorney	Small Area Plan:	Potomac West Small Area Plan; Oakville Triangle and Route 1 Corridor Vision Plan and Urban Design Standards & Guidelines Overlay
	Green Building:	Compliance with City's Green Building Policy

Purpose of Application

Approval of amendments to the Potomac West Small Area Plan chapter of the Master Plan through the Oakville Triangle & Route 1 Corridor Vision Plan and Urban Design Standards & Guidelines overlay, amendments to CDD#24 Concept Plan, and initiation of and text amendment to the Zoning Ordinance for a mixed-use development of the Oakville Triangle with Conditions.

*MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan Amendments*

Special Use Permits, Modifications, and other Approvals Requested:

Public hearing and consideration of requests for:
(A) amendments to the Potomac West Small Area Plan chapter of the Master Plan through the Oakville Triangle & Route 1 West Corridor Vision Plan overlay to facilitate the Inova Healthplex, including an amendment to the mix and location of proposed uses to include medical care facilities; an amendment to building heights on Blocks B and D; an amendment to the framework streets; and an amendment to the location of open spaces within District 2 (Oakville Triangle); (B) an amendment to the previously approved CDD#2014-0002 Concept Plan to amend the definition of retail uses; an amendment to the location and mix of uses and add medical care facility use; an amendment to the location of open spaces; an amendment to building heights on Blocks B and D, and an amendment to street network; and,
© initiation of and text amendment to the Zoning Ordinance to amend the provisions of Section 5-602 to add medical care facilities and amend the allowable uses within CDD #24/Coordinated Development District #24; zoned: CDD #24/Coordinated Development District #24 (Oakville Triangle & Route 1 West Corridor and Vision Plan)

Staff Recommendation: APPROVAL WITH CONDITIONS

Staff Reviewers:

Richard Lawrence Jr., AICP, Principal Planner (P&Z), Richard.lawrence@alexandriava.gov;
Ashley Labadie, AICP, Urban Planner (P&Z), ashley.labadie@alexandriava.gov
Jeffrey Farner, Deputy Director (P&Z), Jeffrey.farner@alexandriava.gov
Carrie Beach, Division Chief, P&Z, Carrie.Beach@alexandriava.gov
Robert Kerns, Division Chief (P&Z), robert.kerns@alexandriava.gov
Karl Moritz, Director, P&Z, Karl.moritz@alexandriava.gov
Melanie Mason, Watershed Management Planner, T&ES, Melanie.mason@alexandriava.gov
Ryan Knight, Civil Engineer IV, T&ES, ryan.knight@alexandriava.gov
Megan Olneyik, Urban Planner III, T&ES, megan.olneyik@alexandriava.gov
Lu Zhang, Civil Engineer III, T&ES, lu.zhang@alexandriava.gov

CITY COUNCIL ACTION DECEMBER 12, 2020: City Council closed the public hearing and approved the Planning Commission recommendation.

PLANNING COMMISSION ACTION, DECEMBER 1, 2020: On a motion by Commissioner Brown, and seconded by Commissioner Koenig, the Planning Commission voted to initiate Master Plan Amendment #2020-00003. The motion carried on a vote of 5-0, with Commissioner Goebel and Commissioner Ramirez abstaining.

On a motion by Commissioner Brown, and seconded by Commissioner Koenig, the Planning Commission voted to approve Master Plan Amendment #2020-00003. The motion carried on a vote of 5-0, with Commissioner Goebel and Commissioner Ramirez abstaining.

On a motion by Commissioner Brown, and seconded by Commissioner Koenig, the Planning Commission voted to approve CDD Concept Plan #2020-00003 subject to the recommended edits in staff memo dated December 1. The motion carried on a vote of 5-0, with Commissioner Goebel and Commissioner Ramirez abstaining.

***MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan Amendments***

On a motion by Commissioner Brown, and seconded by Commissioner Koenig, the Planning Commission voted to initiate Text Amendment #2020-00006 subject to the recommended edits in staff memo dated December 1. The motion carried on a vote of 5-0, with Commissioner Goebel and Commissioner Ramirez abstaining.

On a motion by Commissioner Brown, and seconded by Commissioner Koenig, the Planning Commission voted to approve Text Amendment #2020-00006 subject to the recommended edits in staff memo dated December 1. The motion carried on a vote of 5-0, with Commissioner Goebel and Commissioner Ramirez abstaining.

Reason: The Planning Commission agreed with the staff analysis. Commissioner Brown, Commissioner Koenig commended the process of considering the CDD Concept Plan and Master Plan Amendments prior to considering DSUPs. Commissioner Koenig expressed his support of the evolved changes under current circumstances, especially the open space. Chair Macek reinforced the fact that the proposed changes are reasonable and align with the intent of the Plan. Chair Macek and Vice Chair McMahan commended the community process.

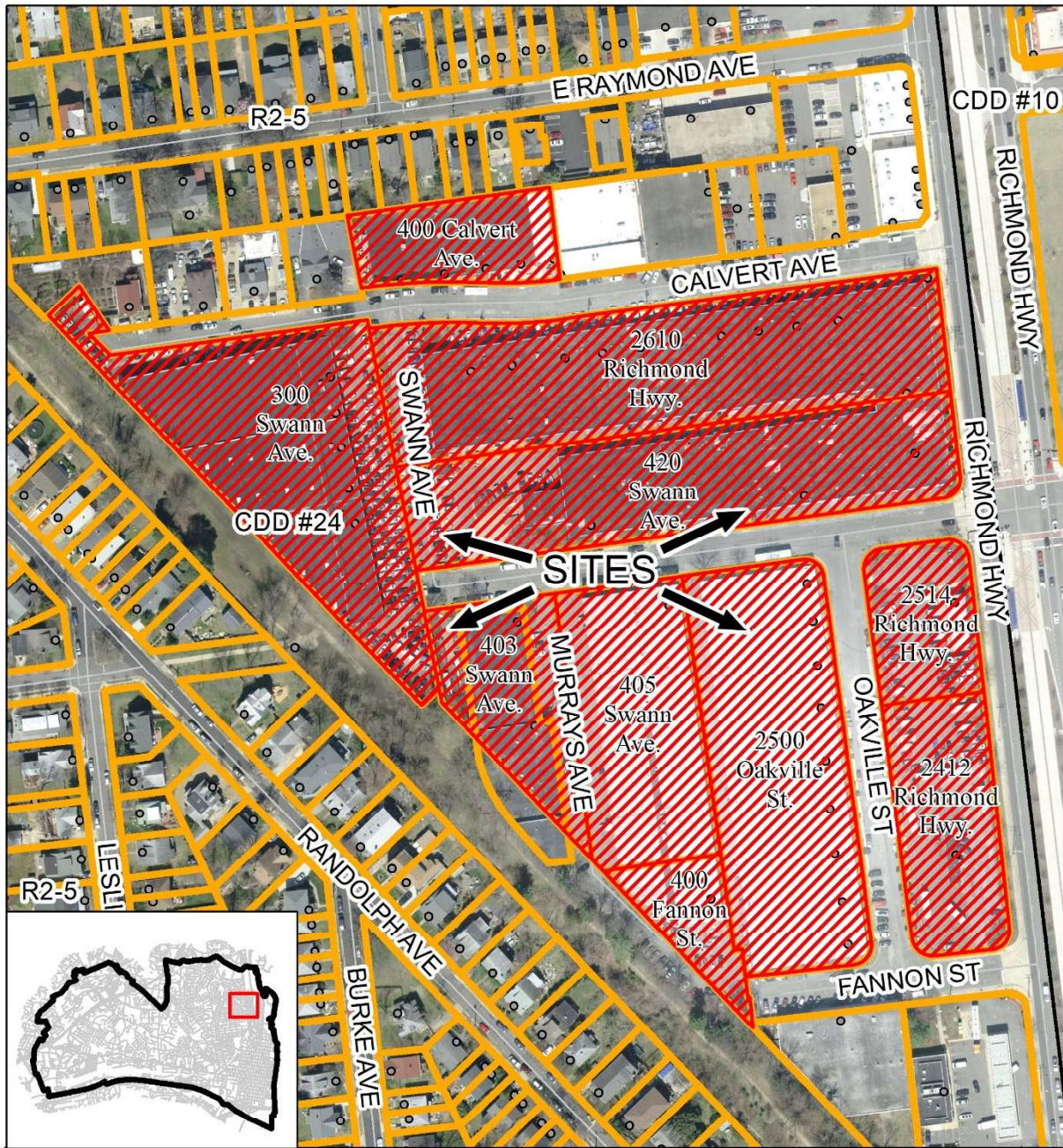
Speakers:

Duncan Blair, attorney representing Stonebridge, spoke in support of the project describing its role in revitalizing the west side of Route 1.


Doug Firstenberg, applicant and Principal at Stonebridge, spoke in support of the project. He expressed that the reduction of retail is to ensure delivery of meaningful retail executed well considering the changes in the retail market. He recognized the long-term benefits of the healthplex as well as the extended terms of housing affordability. He briefly explained a general phasing of development delivery and emphasized a robust community engagement process.

Cathy Puskar, attorney representing INOVA Healthcare Services, spoke in support of the project and emphasized the need to approve additional edits outlined in the staff memo dated December 1.

*MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan Amendments*




Master Plan Amendment #2020-00003
Coordinated Development District Concept Plan
Amendment #2020-00003; Zoning Text Amendment #2020-00006
2412, 2514, and 2610 Richmond Highway;
300, 403, 405, and 420 Swann Avenue;
2500 Oakville Street; 400 Fannon Street; 400 Calvert Avenue





PROJECT LOCATION MAP

I. SUMMARY

A. Recommendation

Staff recommends approval of the requested amendments to the Potomac West Small Area Plan chapter of the Master Plan through the Oakville Triangle & Route 1 West Corridor and Vision Plan (Plan) overlay, to the previously approved CDD#24 Concept Plan subject to staff recommendations, and the initiation of a text amendment to the Zoning Ordinance. Consistent with the original intent and vision of the Plan for an urban mixed use neighborhood, these amendments enable redevelopment of Oakville Triangle anchored by Inova Healthplex as a new medical facility use, a use that was not contemplated at the time of the Plan's original approval but is consistent with the vision for an active mixed use environment. Given national shifts in the retail, office and hotel markets, this new use is a positive addition to the Route 1 Corridor, one that complements the pending arrival of the Amazon Headquarters in Arlington, the Virginia Tech Innovation Campus in North Potomac Yard, and the Potomac Yard Metrorail Station.

The amendments also enable expansion of the open space network, with the addition of new consolidated open space adjacent and connected to the existing Mount Jefferson Park. As prescribed by the Plan, the proposed redevelopment maintains retail and maker spaces, environmental sustainability measures in compliance with the Green Building Policy, a mix of multifamily and townhouse residential uses, dedicated affordable housing, community gathering spaces, and a robust street framework that integrates bicycle facilities.

Future reviews for this project will include and Development Site Plan (DSP) for the site infrastructure and Development Special Use Permits (DSUPs) for individual buildings.

B. Requested Approvals

1. Amendments to the Potomac West Small Area Plan chapter of the Master Plan through the Oakville Triangle & Route 1 West Corridor and Vision Plan overlay, including:
 - an amendment to the mix and location of proposed uses to include medical care facilities;
 - an amendment to building heights on Blocks 7 and 6;
 - an amendment to the framework streets; and
 - an amendment to the location of open spaces.
2. Amendment to the previously approved CDD#2014-0002 Concept Plan, including:
 - an amendment to the definition of retail uses;
 - an amendment to the location and mix of uses and addition of medical care facility use;
 - an amendment to building heights on Blocks B and D;
 - an amendment to street network; and
 - an amendment to the location of open spaces
3. Initiation of and text amendment to the Zoning Ordinance, including:
 - an amendment to the provisions of Section 5-602 to add medical care facilities and amend the allowable uses within in CDD #24/Coordinated Development District #24 including definition of retail; zoned: CDD #24/Coordinated Development District #24 (Oakville Triangle & Route 1 West Corridor and Vision Plan)

C. Summary of Benefits

The major benefits of this project include:

- Redevelopment of an under-utilized land area (approximately 13 acres) consistent with the vision established in the Plan;
- Improvements to the City’s Mount Jefferson Park completed by the property owner (estimated cost of approximately \$2 million);
- Provision of a new 0.7-acre public open space adjacent to Mount Jefferson Park;
- 65 units of affordable housing at 60% Area Median Income (AMI) for 35-year term
- Glebe/Route 1 improvements (estimated cost of approximately \$350,000);
- Expanded street grid and connections;
- Bikeshare stations and Cycle track;
- Available indoor and outdoor community meeting space;
- Pursue LEED-ND or comparable neighborhood certification
- A framework for future buildings to be compliant with the Green Building policy through the DSUP process;
- Extensive implementation of Green Infrastructure; and
- Undergrounding of utilities and improved streetscape along Route 1.

Staff finds this substantial package of benefits provides mitigation of the proposed development and transforms this site into an urban, mixed-use, walkable neighborhood supported by transportation choices, diverse housing options including dedicated affordable units, an expanded open space network, improved environmental impacts, and stronger connectivity to surrounding areas.

D. Site History, Location, Background

The property is located on acreage that was once a small plantation established by the Swann family in the nineteenth century until the 1920s. The plantation was called Oakville for the mature oak trees that dotted the plantation. Mount Jefferson Park, which bounds the Oakville Triangle site to the west, is the former right-of-way of the W&OD railroad. The rail line was constructed in the early 1900s and was the eastern boundary of the historic Town of Potomac, which was originally platted in 1894.

Over time the Triangle developed with warehouse buildings and the Route 1 Corridor became one of the only industrial areas in the eastern part of the City, where a wide variety of businesses have operated since. More recently, with Potomac Yard development, the Route 1 Transitway (Metroway) and Metrorail station near Glebe Road and Potomac Avenue, interest in redevelopment has expanded.

The Oakville Triangle & Route 1 Corridor Vision Plan and Design Standards & Guidelines (Plan) adopted by City Council in 2015 established a long-term (20 year) vision for the area. It envisioned urban scale blocks defined by a framework of streets with improvements for pedestrians, cyclists, and drivers. It addressed future infrastructure, land uses, building heights, design standards, and community amenities, including affordable housing and open space while guiding public and private investment. It built on the strengths of the planning area, including its industrial heritage and home to a diverse array of local businesses, the character of the adjoining neighborhoods, the

MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan Amendments

Metroway and Metrorail station, and the planned and existing uses within Potomac Yard, as they were understood at the time.

Key to implementation of the Plan vision is the redevelopment of Oakville Triangle, the subject site of this application and the largest development site in the Plan area located between Calvert Street and Fannon Street along the west side of the Route 1 Corridor (Figure 1). Adjoining the site to the west is the Mount Jefferson Park and the Del Ray and Lynnhaven neighborhoods; and, to the east, across Route 1, are the Potomac Yard neighborhoods.

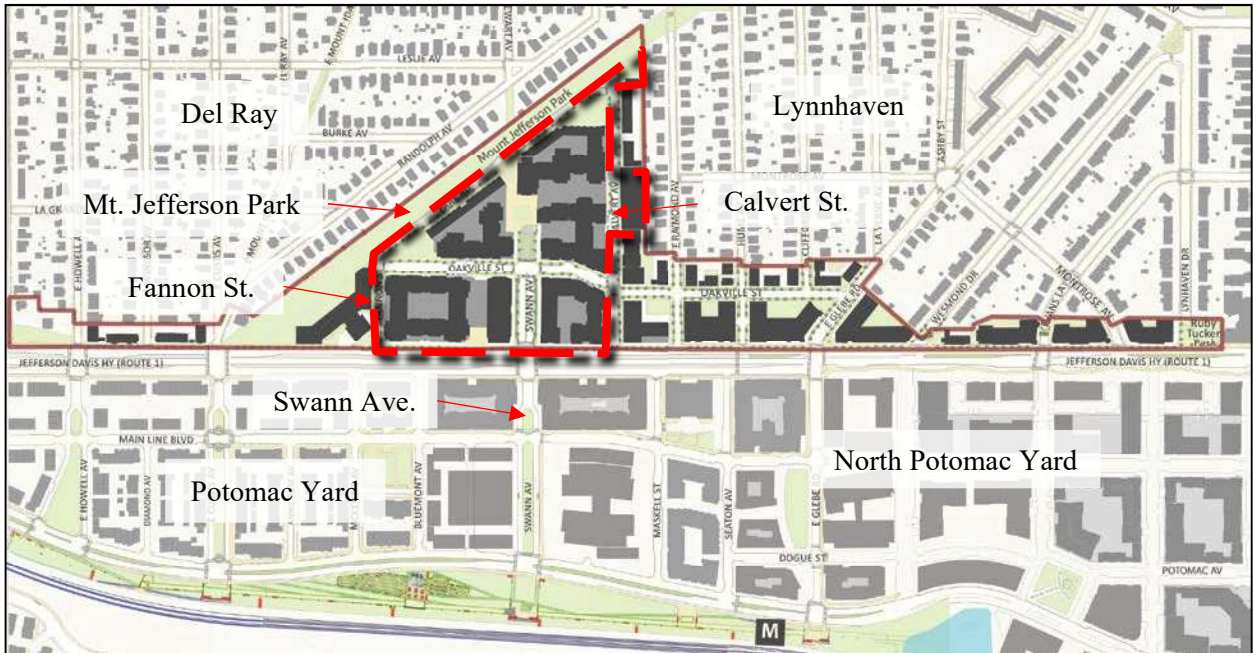


Fig. 1: Oakville Triangle & Route 1 Corridor Vision Plan Area and surroundings with Oakville Triangle Site (Subject Site) outlined in dashed red line

The 2015 Plan recommended coordinated development district (CDD) zoning for the entire plan area to encourage coordination among property owners and a coordinated approach to infrastructure and amenities. City Council approved CDD #24 (CDD#2014-0002) in January 2016 as a rezoning for all properties within the entire plan area (REZ#2015-0004). The Oakville Triangle site, the subject site, is governed by CDD #24 and the approved CDD Concept Plan that was approved concurrently. The CDD Concept Plan included the overall development program and high-level design for the Oakville Triangle site. No development special use permits (DSUPs) were immediately pursued.

Since the 2016 approval, significant changes locally and nationally precipitated the Applicant's reconsideration of the original development program. In November 2018, Amazon announced the location of its second headquarters in Arlington and Virginia Tech announced its Innovation Campus in North Potomac Yard, both of which are driving and will continue to drive development trends in the area. In addition, significant changes in the retail, office, and hotel markets have been intensified by the COVID-19 pandemic. The proposed amendments, responsive to these changes

are presented and analyzed below.

II. STAFF ANALYSIS

A. *Master Plan Amendments*

The Oakville Triangle & Route 1 Corridor Vision Plan and Urban Design Standards & Guidelines (The Plan) was adopted by City Council in October 2015. The Plan establishes the long-term (20 year) vision and framework for the planning area to guide public and private investment. Rather than potential piecemeal redevelopment of the west side of the Route 1 corridor, the Plan enables a comprehensive and coordinated approach for redevelopment and builds on the strengths of the planning area. These strengths include: the industrial heritage of the site, the character of the adjoining neighborhoods, the Route 1 Transitway (Metroway) and Metrorail station, and the planned and existing uses within Potomac Yard.

During the community process that informed the vision and recommendations of the Plan, community priorities and benefits were discussed and balanced. The priorities established by the community were transportation infrastructure improvements, enhancement of Mount Jefferson Park, highlighting the industrial character of the corridor by integrating light industrial/maker uses within mixed-use redevelopment, ensuring successful retail, provision of affordable housing and other community amenities such as additional open spaces, and pedestrian, bicycle, and streetscape improvements.

The staff recommended amendments are strategically targeted within the Oakville Triangle site (District 2) of the Plan. The proposed Master Plan Amendments include:

- Amendment to the land use map to adjust uses within the area to incorporate the Inova Healthplex medical care facility use and amend the location of retail uses
- Amendment to the building heights map to amend heights on Blocks 6 and 7
- Amendment to retail use definitions
- Amendment to the location of open spaces within Oakville Triangle
- Amendment to framework streets map to remove a portion of Park Road, extended Swann Avenue west of Oakville Streets, and updates to street cross-sections
- Amendments to the development table to update density within the Oakville Triangle blocks consistent with the CDD Concept Plan;

Land Uses

Given national shifts in the retail, office and hotel markets, the applicant has requested amendments to the location of uses within the Oakville Triangle site to ensure near-term redevelopment of the site in compliance with the vision and intent of the Plan. One of the key land use changes proposed is the integration of the Inova Healthplex medical facility use (Healthplex). The Healthplex, located on the southern portion of Block 4 at the intersection of Fannon Street and Route 1, will provide a variety of medical services including medical professional offices, imaging and radiology services, multi-specialty outpatient services, and an emergency room. The medical care facility will replace the office use within Oakville Triangle.

MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan Amendments

Proposed residential density within the Oakville Triangle site is generally consistent with the approved Plan. The proposed density (multifamily residential and townhouse units) is anticipated to remain the same across the four development Blocks, decreasing slightly from 1,030,200 sq. ft. to 1,030,000 sq. ft. The primary change is the residential use type on Block 6 from multifamily residential to townhome development. The plan proposes approximately 90 townhomes on Block 6, however, the final unit count may vary but will remain within the allocated gross floor area (GFA), to be reviewed through the DSUP process. The conversion of this block from multifamily to townhomes and the resulting reduction in building height is consistent with the Plan recommendation to create an appropriate transition from the low-density, single family homes west of the site to higher density development along Route 1.

Additionally, the Plan anticipated about 145,300 sq. ft. for hotel use within Oakville Triangle. Although this use is not being pursued by the applicant at this time, to provide future flexibility in the Plan, staff recommends hotel uses be allowed in the Plan area to be determined during the Development Special Use Permit (DSUP) process.

Retail

The 2015 Plan anticipated 193,900 sq. ft of retail within Oakville Triangle’s Blocks 4-7 along Route 1, Swann Avenue, and Oakville Street, with a large majority of the retail on Block 6. Since 2015, the retail sector has experienced significant shifts that began before the COVID-19 Pandemic and have intensified during the pandemic, prompting a significant reduction in retail in the current proposal. The applicant is requesting that the amount of required retail be reduced to approximately 65,000 sq. ft. primarily because the proposal no longer includes the previously planned large format retail on Blocks 5 and 6. The applicant continues to provide retail on Blocks 4 and 7 along Route 1 and Swann Avenue, creating a vibrant retail environment as envisioned by the Plan. Despite the significant reduction in the amount of retail, the amount and location of retail is still sufficient to provide a critical mass of retail on the ground floor to activate the street and create a strong retail corridor.

Along with the proposed amendment to the location of retail uses, the applicant has requested amendments to the Plan’s definition of retail uses to provide greater flexibility in location of types of retail uses. The 2015 Plan specified three retail categories (primary, secondary, and maker uses) and assigned locations for each as part of the retail strategy for the site. Understanding the ongoing changes in the retail environment, staff supports the request for greater flexibility in the types and location of various retail uses in compliance with the Zoning Ordinance to enable a vibrant and successful retail environment. The updates to the retail definitions are defined in more detail in *Section 3. Uses*, in the CDD conditions, and included for consideration as part of the Zoning Text Amendment.

Building Heights

The applicant has requested the following amendments to building heights to reinforce the gateway at Swann Ave and provide building heights appropriate for townhouse development;

- Block 6: decrease the maximum building height from 75’ to 55’;
- Block 6: decrease the minimum building height from 60’-50’ to 40’

MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan Amendments

- Block 7: increase building heights from 90-100 feet on the southern portion and up to 75-85 feet on the northern portion;

Block 6

The 2015 Plan allowed for maximum building heights that ranged from 45' to 75' to accommodate liner townhome units to the west and large format retail and multi-family residential uses at the center of the block. The proposed heights now requested are consistent with the heights for townhouse style development ranging between 45' to 55'. The minimum height amendments on this block from 50' to 40' enable a variety of heights and support townhome configuration on this block.

Block 7

Building heights in the 2015 Plan for Block 7 ranged between 75 feet on the northern portion of the block to 90 feet on the southern portion of the block at the intersection of Swann Avenue and Route 1. The proposed amendment to increase the height in this location from 75 to 85 feet aligns with current building code for wood over podium construction. The increase in height at the southern portion from 90 to 100 feet provides the opportunity to provide increased building height at the Swann and Route 1 intersection to reinforce the gateway element as envisioned by the Plan.

The proposed amendments are consistent with the intent of the Plan and provide a variety of building heights and transition heights from the lower density single-family homes to the west to the higher density development to the east; a key recommendation of the Plan.

Framework Streets

The street framework is one of the fundamental elements of the master plan that sets the stage for the urban environment and how people use, access and traverse a site. Some adjustments may be considered and appropriate if they maintain the intent of the Plan and provide additional benefits to connectivity, pedestrian safety, or the public realm. With the proposed development program for Oakville Triangle, the applicant proposes some revisions that Staff finds appropriate.

The applicant proposes to extend Swann Avenue west to connect to Park Road and to remove the segment of Park Road from Swann Avenue to Fannon Street. Not constructing this road segment results in minimal reduction of the street framework and enables provision of open space where the road would have been, resulting in consolidated open space adjacent to Mount Jefferson Park and a large central open space for the site.

Street cross-sections from the 2015 Plan are proposed to be updated to reflect this amendment. The proposed street cross-sections are consistent with the CDD Concept Plan and reflect the character and intent of the street as identified in the Plan.

- Swann Avenue-East cross-sections are reduced because the previous land uses required additional vehicular travel lanes which are no longer needed. The proposed cross-section ensures prioritization of pedestrians and cyclists, minimizing crossing distance across Swann Avenue, and enhancing the pedestrian streetscape.
- Swann Avenue-West, previously anticipated as a terminating street with hardscape plaza, is reduced in width and is now proposed to continue west and connect to Park Road. The

MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan Amendments

proposed cross-section provides an appropriate transition from the higher density mixed-use development to the east to the lower scale residential development to the west.

- Fannon Street Interim cross-sections are proposed to maintain the same cross-section as the 2015 Plan with some modifications to the location of travel lanes to accommodate the passenger layby area for safe access to the Inova Healthplex medical care facility use.
- Oakville Street cross-section is proposed to maintain the same cross-section as the 2015 Plan with modifications to the eastern portion of Oakville Street adjacent to Blocks 4 and 7 to accommodate underground utilities. These areas will include public access easements and streetscape consistent with the rest of the site area.
- Calvert Street maintains the same cross-section as the 2015 Plan with adjustments to reflect the adjacent townhome uses on Block 6.
- Park Road cross-section, now proposed to be a private street with public access easement, preserves the park-adjacent character of the street and continues to provide site circulation for Oakville Triangle.

Any revision to the street framework in a small area plan is a significant amendment to be carefully considered; generally, staff does not recommend removal of a framework street. However, staff recommends approval because the amendments will:

- Allow enhanced circulation for pedestrians through the site;
- Allow for the separation of the buildings;
- Provide an increased useable central open space adjacent to Mount Jefferson Park;
- Provide pedestrian scale streets; and
- The resulting building and block sizes remain consistent with the intent of the Plan.

Proposed cross-sections can be viewed in **Attachment #3**.

Location of Open Spaces

The location of publicly accessible open spaces within Oakville Triangle was informed by the network of streets and adjacent building footprints on the associated development blocks. With the new development program proposed and the configuration of adjacent streets, the location and configuration of open spaces have also been modified. The proposed configuration provides the opportunity to consolidate two smaller open spaces on Blocks 5 and 6 into a consolidated central open space of approximately 0.7 acres. This provides a more useable open space that can accommodate a variety of uses. The applicant has been working with staff and the community to ensure the design of the 0.7-acre public open space is informed by community feedback. The formal development application for this new open space will follow a subsequent approval.

With the relocation of open spaces within the Oakville Triangle, the proposal is consistent with the Plan recommendations and ensures that the townhouse development proposed for Block 6 will be consistent with the open space requirement for a minimum of 15% ground-level open space. Additionally, for Oakville Triangle mixed-use project (Blocks 4, 5, and 7), the open space requirement has increased from 15% ground level open space to 20% ground level open space to ensure that a total of 40% on-site open space, to include roof-top amenity areas, are provided within the project area.

MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan Amendments

Development Table Amendments

The following amendments proposed to the development summary table in the Plan are consistent with the proposed CDD Concept Plan and are only for the development blocks within the Oakville Triangle Site (Blocks 4-7). The total development totals have been updated to reflect these land use amendments. Per the Plan, GFA does not include areas designated for service or parking structures.

Table 1: Oakville Triangle Development Table Amendments

(Proposed figures are shown in red)

Block	Residential MF	Residential TH	Office/ Medical Care Facility	Retail	Hotel
4	307,000 310,000	0	135,200 115,000	46,000 40,000	
5	162,600 200,000	35,100 0		31,000 0	
6	377,400 0	31,000 250,000		81,700 0	
7	117,100 270,000	0		35,200 25,000	145,300 0
Total (Approved) (Proposed)	964,100 780,000	66,100 250,000	135,200 115,000	193,900 65,000	145,300 0

In sum, residential density went from 1,030,200 sq. ft. to 1,030,000 sq. ft.; retail density decreased from 193,900 sq. ft. to 65,000 sq. ft., office density decreased from 135,200 sq. ft. to 115,000 sq. ft. with the medical care facility use (Inova Healthplex) utilizing the office density, and hotel use was removed from the development program.

The proposed amendments are consistent with the intent and vision of the Plan to create a vibrant mixed-used urban neighborhood and enable the redevelopment of the Oakville Triangle property, anchored by Inova Healthplex as a new medical facility use that was not contemplated at the time of the Plan’s original approval. In addition to these amendments, additional plan updates are recommended by staff to ensure consistency throughout the document and align with the proposed CDD Concept Plan in compliance with the Plan’s vision. A detailed list of plan text and graphic updates can be found in *Attachment 3*.

B. CDD Concept Plan Amendment & Consistency with the Master Plan Amendments

The CDD Concept Plan approved by City Council in 2016 included high-level designs reinforcing the concepts envisioned in the Small Area Plan. CDD #24 Concept Plan establishes:

- The boundaries of the Oakville Triangle CDD Concept Plan;
- Development levels for the blocks;
- Proposed road network, pedestrian, and bicycle paths;
- Proposed Open Space within the CDP; and

MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan Amendments

- Proposed locations of public rights of way and public access easements.

As part of this amendment, the applicant proposes updating the CDD Concept Plan and CDD Conditions to align with the proposed Master Plan Amendments. Key updates to the Oakville Triangle Corridor Vision Plan that will be mirrored in the CDD Concept Plan include;

- An update to the building heights map to amend building heights on Blocks B and D;
- An update to the land use map to reallocate uses within the Concept Plan area to incorporate the Inova Healthplex;
- An update to the land use map to amend the location of retail uses;
- Revisions to allowable retail uses;
- An update to the location of open spaces;
- Revisions to the development table to amend densities within the Oakville Triangle blocks; and
- Revisions to framework streets to remove a portion of Park Road (New Road A) between Swann Avenue and Fannon Street and updated street-cross-sections.

As with the amendments to the Plan, the changes above facilitate the location of the Inova Healthplex within Phase 1 redevelopment of Oakville Triangle and an updated development program for the rest of the site.

The existing CDD Conditions were adopted in 2016 with the original CDD Concept Plan. The proposed amendments update the CDD conditions to reflect the changes in the development and infrastructure phasing, mix of uses, updated standard practices, City policies, regulatory guidelines, and removing conditions no longer applicable based on the proposed development program. Staff finds the proposed amendments will bring the CDD Concept Plan in alignment with the applicant's proposal. Amendments to the CDD Conditions can be categorized within these general themes:

- Refinement of conditions based on changes to standard City practices between 2016 and 2020.
- Refinements to required infrastructure conditions based on a greater understanding of the proposed development envelope and system capacity.
- Changes to conditions related to phasing and timing to ensure development occurs holistically in Phase 1 to deliver a vibrant and mixed-use community.
- Removal of conditions which have been met or are no longer applicable.

C. CDD Text Amendment

Staff has drafted an amendment to the CDD Zoning Table, in Section 5-600 of the Zoning Ordinance as it relates to the zoning for CDD #24. This text amendment requires the initiation of the amendment by the Planning Commission and approval by City Council and is being processed in conjunction with the CDD Concept Plan Amendment and the applicant's other requests. See Table 3 below.

1. Street and Block Network

Like the Plan and previously approved CDD Concept Plan, the proposed CDD Concept Plan outlines a network of new streets that replace existing streets of the Triangle site forming four primary development blocks. Four existing streets (Swann Avenue, Oakville Street, Fannon Street and Calvert Avenue) will remain in name, but portions will be vacated and/or realigned to allow for the new development. Three of the streets, Calvert, Swann and Fannon, will provide direct access to and from Route 1, consistent with the previous approval. However, in addition to the signalized intersection at Swann and Route 1, a new signalized intersection at Fannon and Route 1 will facilitate the proposed medical care facility. Unlike the previous approval, Swann will extend west beyond a relocated Oakville Street through what was a pedestrian plaza and connect to a new street (New Street A) parallel to Mount Jefferson Park which connects with Calvert Street to the north. Swann will provide a dedicated off-street bicycle path connecting directly to the existing multi-use path in Mount Jefferson Park. New Street A will only connect Calvert Avenue to the north with Swann to the south, instead of connecting to Fannon Street, as per the previous approval. By eliminating the connection from Swann to Fannon, a larger open space can be provided that connects directly to Mount Jefferson Park. Figure 3 demonstrates the proposed CDD Concept Plan framework streets and block network.



Fig. 3: Proposed CDD Concept Plan with Framework Streets and Development Blocks (Triangle Site boundary in green)

2. Phasing

As the primary development area within the Oakville Plan area, the Triangle site will contribute significantly to the infrastructure, transportation and open space improvements necessary to allow redevelopment of the larger plan area. Development of the Oakville Triangle site is phased to ensure that adequate infrastructure, particularly transportation, sanitary and stormwater infrastructure, is in place to support proposed development as construction occurs.

The site is anticipated to develop in two broad phases: Phase 1 will consist of all infrastructure

MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan Amendments

improvements, including all framework streets and streetscapes, BMPs, and the Swann Ave./Route 1 and Fannon St./Route 1 intersections, Block A1 (multifamily ground floor retail), Block A2 (/medical care facility/Inova Health Plex), Block B (residential with ground floor retail), Block C (publicly accessible park), and Block D (townhomes). In Block B, there will be ground level residential units designed not to preclude future maker space conversion fronting on Calvert Avenue that are meant to be compatible with the existing industrial oriented uses on the north side of the street. Phase 1 will also include construction of the improvements to Mount Jefferson Park per DSP2015-0025.

Phase 2 will consist of the remainder of Block C, where the existing self-storage building will remain in the interim until it is redeveloped as multifamily residential. All future buildings will be required to meet the requirements of the Oakville Triangle Route 1 Vision Plan in terms of bulk, mass and height at the time of DSUP review.

Staff has included several conditions of approval which address the interim aspects of the site during a phased construction to ensure that the area is maintained as usable as possible, with construction activities and incomplete phases adequately screened.

3. Uses

As discussed, the proposed changes in land use are responsive to both local dynamics such as the new Amazon Headquarters, Virginia Tech Innovation Campus, and the new Potomac Yard Metro Station, as well as major market changes in the retail, office and hotel sectors that have intensified during the pandemic. As a result, the project proposes a shift from office and hotel to medical care facility and residential and less square footage for retail space than was previously envisioned and approved in the Plan and the CDD Concept Plan.

Blocks C and D remain residential but at a lower density than previous approvals as Block C now incorporates a 0.7-acre open space and Block D is now envisioned as townhomes. Block B maintains mixed-use residential and retail uses but no longer incorporates hotel or office. Block A also maintains residential and retail uses but replaces the previous office use with a medical care facility.

Retail is proposed along Route 1 and Swann Ave. on Blocks A and B, and residential units along Calvert Street on Blocks B and D will be designed not to preclude future retail use. Retail, as per this request, is defined to include retail shopping establishments, personal service uses, amusement enterprises, and restaurants, as defined in the Zoning Ordinance, with the following exceptions:

- a. Retail shopping establishments shall not include appliance stores and auto parts stores;
- b. Personal service uses shall not include appliance repair and rental, contractors' offices, laundromats, and pawnshops;
- c. Personal service uses on the ground floor, shall be limited to a frontage less than 30 feet within the required retail areas, unless additional frontage is permitted with an approved Special Use Permit;
- d. Ground floor uses in the residential units along Calvert Avenue shall be utilized for Home Occupations, maker space, retail and restaurant uses as defined by the Zoning Ordinance with the exclusions stated in subparagraph (a) above; and
- e. Other similar pedestrian-oriented uses as approved by the Director of P&Z to meet the

MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan Amendments

intent of providing active pedestrian-oriented uses are allowed.

Staff considers the proposed mix and location of uses appropriate and balanced for the area and provides the use diversity intended in the Plan. Because the medical care facility has specific functions and circulation needs, it has been proposed on the south side of Block A along Fannon and Route 1. This approach enables the remainder of Block A and all of Block B to be developed with a commercial product that ensures the lively street frontage with ground floor retail envisioned by the Plan.

4. Density and Height

CDD #24 and the Oakville Triangle/Route 1 Corridor Vision Plan establish the development controls, such as bulk and setback requirements as well as density and height for all new development in the CDD area. Although no buildings are proposed with this submission, the future development of individual blocks must comply with these regulations at the time of development special use permit review. Total Gross Floor Area (GFA) proposed in this request is 1,370,000 sq. ft. (inclusive of above grade parking and service areas), while the previous approval included approximately 1,725,662 sq. ft. While the total number of residential units in this request is 920 and the previous approval included 1,050, the number of provided affordable units remains the same at 65 units. The amended condition of the CDD provides flexibility in the maximum number of units permitted within the allowable GFA pursuant to additional infrastructure and supporting analysis conducted as part of the DSUP process. The following table provides a comparison of the previously approved CDD Concept Plan to this proposal:

Table 2: Proposed and Previously Approved Development Square Footage

		Previously Approved CDD Concept Plan				Proposed CDD Concept Plan			
		Block A	Block B	Block C	Block D	Block A (2)	Block B (2)	Block C (2)	Block D (2)
Residential	Multifamily (max. GFA)	307,000	117,100	162,600	377,400	310,000	270,000	200,000	-
	Multifamily Residential Units (max. units)	326	125	172	399	330	260	240	-
	Townhouse (max. GFA)	-	-	35,100	31,000	-	-	-	250,000
	Townhouse (max. units)	-	-	14	14	-	-	-	90
Office	Office (max. GFA)	135,200	-	-	-	-	-	-	-
Hotel	Max. GFA	-	145,300	-	-	-	-	-	-
Retail/ Maker Space	Max. GFA	46,000	35,200	31,000	81,700	40,000	25,000	-	-

MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan Amendments

Medical Care Facility	Max. GFA					115,000	-	-	-
		n/a	n/a	n/a	n/a				
Above Grade Parking	Max. GFA					75,000	30,000	10,000	-
		n/a	n/a	n/a	n/a				
Service /BOH/ Loading	Max. GFA					25,000	10,000	10,000	-
		n/a	n/a	n/a	n/a				
	Maximum Total (sq. ft.)(1)	488,200	297,600	228,700	490,100	565,000	335,000	220,000	250,000

Notes:

1. Excludes below grade parking structures from calculation.
2. Includes service/loading in calculation.

Building Height

As envisioned in the Plan, building heights are generally taller at the intersection of Route 1 and Swann Avenue as a gateway element and transition down to lower-scaled heights near Mount Jefferson Park and the adjacent Del Ray neighborhoods. The applicant is requesting the following height modifications to accommodate the proposed mix of uses and ensure the transition envisioned by the Plan:

Block B(7)

- An increase from 90 to 100 feet along Swann Ave., which mirrors the approved height on Block A to accentuate this area as the gateway, per the Plan.
- Increase the height for the remainder of the block from 75 to 85 feet.

Block D(6)

- A decrease from 75' to 55' across the entire block to accommodate the change in use from high density multifamily to townhouse.

As the applicant progresses the design for the buildings on each block, the requirements for height and transitions (where the buildings step down for compatibility with existing neighborhoods) will be further evaluated by staff and subject to approval through individual DSUPs.

5. Affordable Housing

The proposed amendments remain consistent with the City's Housing Master Plan, which recommends focusing affordable housing in areas with the greatest potential for increased density and mixed-use development, and encourages housing options affordable to a range of incomes and household types, including workers critical to the future economic competitiveness and sustainability of the area. Oakville Triangle, North and South Potomac Yard, and the Route 1 Corridor host (and will continue to attract) a wide variety of employers, including retail, continuum of care, hospitality, office, maker and other businesses. The redevelopment will also introduce a

MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan Amendments

significant number of jobs, at various skill and wage levels, related to a planned Healthplex medical care facility. The success of these businesses, in part, relies on the availability of a diverse workforce. Providing affordable housing in close proximity to jobs and transit will help improve workers' and residents' quality of life, reduce congestion, lessen economic leakage, and strengthen the City's tax base.

The applicant will provide a total of 65 affordable set-aside rental units comprising 63 studios and 2 two-bedroom units affordable to households with incomes at 60% of the area median income (exclusive of utilities) for a period of 35 years; the term of affordability was extended from the previously approved 25 years with this CDD Amendment. The units will be provided proportionally as the multifamily rental buildings in the master development are delivered to market. The applicant presented an Affordable Housing Plan (AHP) (**Attachment #2**) to the Alexandria Housing Affordability Advisory Committee (AHAAC) on November 5. Committee members observed that the term of affordability is not consistent with the City's standard 40-year term, and that the proposed unit mix does not reflect the overall unit mix in the master development. Staff indicated that the additional ten-year extension of the affordability term — offered in response to the proposed change in the development program (no additional density is being sought by the applicant)—will result in a substantial increase in the affordable housing contribution. Staff also noted that the unit mix had been negotiated as part of the prior development approvals when the master development was envisioned to have a larger service sector component (a hotel and more retail), and that the smaller unit sizes maximized the number of affordable units that could be provided. AHAAC approved the AHP.

6. Open Space and Amenities

Open space for the project consists of both on-site provisions and improvements to the adjacent Mount Jefferson Park. As prescribed by the Plan, a total of 40% on-site open space will be provided within the development as useable ground level open space or rooftop amenity space for the project. The on-site open space, per this request, is located to ensure a network of open space is provided to meet the various needs of residents, workers and visitors to the Triangle. The design of all open space will continue to develop with the forthcoming DSUPs for each block and will be in compliance with the proposed master plan amendments and CDD Concept Plan.

Mount Jefferson Park

As previously approved, the design and construction of Mount Jefferson Park Improvements (DSP2015-00025) will be paid for by the Oakville Triangle site developer, as part of the developer contributions package. Mount Jefferson Park is directly adjacent to the Oakville Triangle site and one of the existing warehouse buildings is located within the city-owned, POS-zoned land. The Development Site Plan improvements span the portion of Mount Jefferson Park from E. Raymond Avenue on the north to Route 1 on the south – the section of the park which is contained within the Plan's study area, and which is the closest to the Oakville Triangle development. The park design includes improvements to stormwater management and drainage, improvements to the north-south multi-use trails, enhancements to the dog exercise area, various landscape and planting improvements, and interpretive elements that recall the site's former use as a rail line. Once improved, all land will remain in public ownership as public, ground-level open space. These improvements will provide a large and enhanced amenity space for the residents of this new development area, in addition to the park's current users. Improvements to Mount Jefferson Park do not count towards the on-site open space percentage required for the Triangle site.

Proposed CDD Concept Plan

While the Oakville Triangle & Route 1 Corridor Vision Plan and previously approved CDD Concept Plan outline several ground level open space areas that contribute to the overall minimum requirement of 15% ground level open space, the proposed CDD Concept Plan provides nearly 22% at ground level due to the change of uses (primarily residential) and reconfiguration of blocks with the western extension of Swann Avenue. A majority of the ground level open space is achieved through the 0.7 acre on Block C. This replaces the 0.48-acre pedestrian plaza located at the terminus of Swann Avenue included in the previously approved CDD Concept Plan. The proposed Block C park design is being developed in a concurrent DSUP with community engagement. It will provide an open space transition from Mount Jefferson Park and will no longer be bifurcated by a parallel road, as previously approved. Until the existing self-storage on Block C develops, it will be thoughtfully screened with vegetation on the park side.

Consistent with the previously approved CDD Concept Plan will be a small parcel (Block E) at the end of Calvert Avenue (Figure 4). The parcel is directly adjacent to Mount Jefferson Park and affords the benefit of extending a green open space from the Park onto the end of Calvert Avenue. Currently, part of the parcel is used for access and unloading by the operator of the garden nursery (Nature by Design) abutting the property. In order for the nursery to maintain operations without being negatively impacted, the area was slightly modified from the previously approved CDD Concept Plan to allow for some access by the current operator of the nursery while also serving as publicly accessible open space.

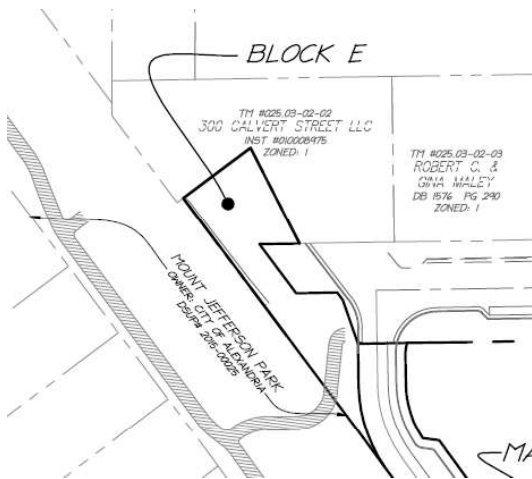


Fig. 4: Block E

The neighborhood park provided on the west side of Block D has shifted to the east side of the block and will be designed and developed with the future Block D DSUP. Also provided, as per the previously approved CDD Concept Plan, is the open space on the south side of Block C, which will also be designed and developed in its final configuration when the self-storage building is redeveloped. Blocks A1, B and C will provide landscaped rooftop open space with amenities to complete the 40% requirement. The applicant has agreed not to count the private sidewalks along Oakville Street as open space.

*MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan Amendments*

In addition to the required 40% open space, Block A2 (medical care facility/Inova Health Plex) will provide a rooftop open space accessible to users from the second floor waiting area. Block D, due to its change in use from high density multifamily to townhomes, will provide 15% ground level open space consistent with typical townhouse developments across the city. An additional 10% rooftop open spaces will also be provided on this block. For comparison, the location and configuration of open space proposed in the previous and proposed CDD are provided in Figures 5 and 6.

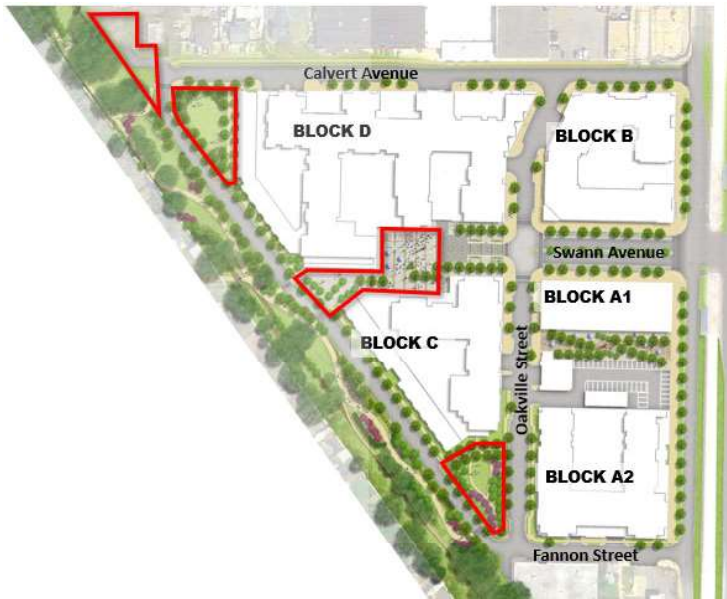


Fig. 5: On-site Publicly Accessible Open Space (red outline) in previously approved Oakville Triangle CDD Concept Plan

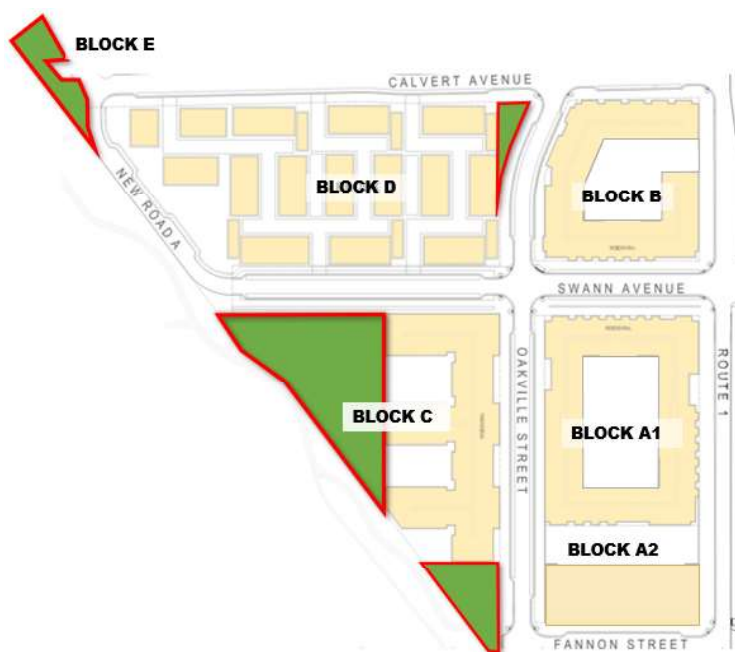


Fig. 6: On-site Publicly Accessible Open Space (red outline) in proposed Oakville Triangle

CDD Concept Plan

Community Amenities and Public Art

As envisioned in the Plan, community amenities, including public art and community meeting spaces, will be provided. Each phase of development will be expected to meet or exceed the City's Public Art Policy and provide a public art plan outlining locations for public art in highly visible locations or on private property. This allows the opportunity to provide public art with individual DSUPs or allow the applicant to consolidate the public art contributions towards larger pieces of public art. The project will also provide an indoor community meeting space of at least 1,400 sq. ft., per the previously approved CDD conditions, as well as an outdoor community gathering space in the consolidated open space on Block C, a new amenity provided in response to community feedback.

7. *Transportation*

As part of the 2015 Plan, a Multimodal Transportation Study was conducted to determine the existing and future transportation conditions of Route 1 intersections and neighborhood streets, based on the land use assumptions and proposed program for the Oakville development, which resulted in an overall acceptable level of service, with recommended mitigations. With the current evolution of the Oakville Triangle development program, the City requested an updated study to understand the differences in impacts to the transportation network.

Based upon the ITE Trip Generation Manual and a maximum of 75% vehicle mode split, the proposed land use change would generate approximately 560 vehicle trips in the AM Peak Hour and 650 vehicle trips in the PM Peak Hour. As compared to the approved CDD plan, this would equate to approximately 215 less vehicle trips in the AM Peak Hour and 220 less vehicle trips in the PM Peak Hour.

This updated impact study included other scenarios at the Fannon Street and Route 1 intersection, as well as the Swann Avenue and Route 1 intersection, that evaluated the specific needs for the Inova Healthplex which will be located at the corner of Fannon Street and Route 1. Currently, to access the site from Route 1 southbound, a motorist is required to U-turn at the Swann Avenue and Route 1 intersection. Due to potential emergency egress and ingress needs and to facilitate efficient site circulation, the applicant proposes an added traffic signal with full access to and from Fannon Street from Route 1. In addition, staff directed the applicant to evaluate Swann Avenue with both one- and two- lane approaches for the Route 1 intersection.

Based on this updated Multimodal Transportation Impact Study, staff concurs that the proposed development will result in minimal change to the transportation network's performance as compared to the 2015 approved plan. Further, neither the new traffic signal at the Fannon Street and Route 1 intersection that would allow full access operation, nor the two-lane approach on Swann Avenue at the Route 1 intersection significantly impact performance. As a result, the proposed plan generally creates the same or less overall intersection or arterial delay.

The development continues to align with the City's guiding principle of encouraging multimodal use and decreasing single-occupancy vehicle travel. The site includes bike routes and dedicated bike facilities along Swann Avenue to connect to Mount Jefferson Park. This site will also include

*MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan Amendments*

various locations for bikeshare and adequate bike parking. In addition, all streets within the site prioritize pedestrian comfort and safety. A strong emphasis was placed upon pedestrian and cyclist connectivity throughout the site and to nearby attractions such as the proposed Potomac Yard Metro Station.

The intent of this updated impact study was to accurately assess the differences between the approved CDD plan and the proposed CDD plan. Additional, more detailed analyses based upon current traffic data and/or assumptions will be required for future Development Special Use Permit (DSUP) submissions.

Parking

In 2015, the City approved new parking standards for multifamily buildings which consider proximity to metro and other transit, as well as neighborhood amenities and affordable housing. Using these standards, a minimum and maximum parking requirement can be calculated based on the number of bedrooms in the building. The City completed a similar review of the commercial parking standards in 2018 and updated the minimum and maximum requirements for specific commercial uses based on square footage and proximity to transit.

The Oakville CDD will use these refined parking ratios from the Zoning Ordinance for multifamily and commercial uses. Parking ratios for all other uses are also proposed to be consistent with the zoning ordinance, unless a parking requirement reduction or excess SUP is submitted with the DSUP applications. Parking counts for each block will be provided with the block DSUPs.

The mix of uses proposed for the overall development presents a great opportunity for shared parking. In particular, medical care facilities, retail, and residential uses typically have different peak parking hours. Through careful coordination, the applicant can maximize parking efficiency for the entire development by sharing parking across uses. As each building is reviewed for their DSUP approval, staff will review any proposed shared parking to ensure parking adequate to meet the demand. Interim parking on undeveloped blocks may be required to accommodate parking until those blocks are developed with the anticipated parking (similar to Potomac Yard).

Similar to other residential developments recently approved, and consistent with the City's Transportation Master Plan and several Small Area Plans, the cost of parking for the multifamily units is required to be unbundled from the cost to rent or purchase a unit. This is intended to maximize efficiencies in the garage by ensuring parking spaces are used by the people requiring them and not simply assigned to all households regardless of their vehicle ownership. This also passes the cost of the parking on to the actual users of the spaces rather than all residents, which improves affordability for residents without a vehicle and creates an incentive for lower vehicle ownership.

The provision of above grade parking (wrapped by buildings) is considered beneficial at this location in order to serve the retail uses in the project. The bulk of the parking is to be accommodated in below grade garages.

8. *Stormwater, Water, and Wastewater Management/Environmental Sustainability*

With the intent of complying with the City’s Eco-City Charter and the Plan, staff recommends the applicant prepare a full Water Management Master Plan (WMMP) that coordinates water supply, stormwater, and wastewater systems. The WMMP integrates the management of stormwater, use of potable water, and generation of municipal wastewater to reduce the use of potable water and reducing wastewater generation through water conservation. The implementation of WMMP minimizes the negative impacts of the development on the sewer infrastructure and improves the instream habitat for Four Mile Run, the Potomac River and the Chesapeake Bay. The WMMP will be required with the first preliminary DSUP submission.

Stormwater Management Master Plan

The applicant will develop a Stormwater Management Master Plan (SWMMP) designed to meet the requirements of the Virginia Stormwater Management Program (VSMP) Regulations and the Chesapeake Bay Act in accordance with Article XIII of the Zoning Ordinance for control of stormwater quality. Water quantity and runoff reduction will be addressed by each individual DSUP. The SWMMP will demonstrate overall compliance with stormwater quality requirements, as well as demonstrating compliance during each phase of development.

The goal of the SWMMP is to reduce the generation of stormwater pollutants for the entire development. Consistent with the Plan’s goal for environmental sustainability, development in Oakville Triangle will incorporate green infrastructure in the public realm through the use of permeable paving and urban bioretention areas in the streetscape design to promote environmental sustainability. Implementing innovative green infrastructure techniques into the public realm streetscape provides stormwater quality treatment as well as ancillary benefits such as reduction of the heat island effect, runoff reduction and reducing impacts to climate change, while providing a green amenity to the community. Streets in Oakville will receive stormwater treatment to the extent feasible through the use of urban bioretention areas, or BMP tree wells, designed per the City’s Green Streets and Sidewalks guidelines. Buildings will be treated by a combination of vegetated roofs, urban bioretention planters and manufactured treatment devices.

Wastewater Management

Staff has worked with the applicant to assess the impact of the proposed development in Oakville Triangle on the City-owned collector sewers and the Potomac Yard Trunk Sewer (PYTS). The PYTS was designed and constructed, and since time of the previous approval will be upgraded, to meet the municipal wastewater conveyance needs of Potomac Yard, along with meeting future needs of the city. This includes the diversion of wet weather flows from the Four Mile Run Pumping Station, separation of combined sewer flows in the King/West portion of the CSS area as redevelopment occurs, and limited development along the Route 1 Corridor.

As part of the sewer analysis conducted under the North Potomac Yard - Virginia Tech development (NPY-VT) project, staff used available growth forecasts for Oakville Triangle. In the Oakville Triangle CDD, the amount of wastewater generation is in alignment with the estimates used in the sewer analysis of the NPY-VT project. As part of the Preliminary Infrastructure Plan submission of the Oakville project, a sanitary sewer adequate outfall analysis was performed by the applicant on both the City’s collector sanitary sewers and the PYTS to determine where flow

MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan Amendments

capacity was exceeded and where infrastructure upgrades would be required. The sanitary sewer adequate outfall analysis was performed in accordance with the City's Memo to Industry 06-14. As part of the analysis, staff allowed for a 35% sanitary flow reduction below the wastewater flow factors stated in the Memo to Industry for low flow fixtures being adopted by the development project. This is consistent with the analysis that was approved for the North Potomac Yard CDD.

As part of the Oakville sanitary sewer adequate outfall analysis, it is determined that the PYTS requires no capacity improvements other than those improvements to be completed under the NPY-VT development project. Regarding the City's collector sanitary sewers, the applicant has identified two sanitary sewers where improvements in sanitary sewer conveyance are required. A summary of these improvements is provided below:

- a. 8-inch Ductile Iron (DI) sewer between manhole 7506 and manhole 7509 - the sanitary sewer adequate outfall analysis indicates that this 95 feet of sewer does not have sufficient capacity as shown on Sheet C-601 of the Infrastructure Plan and shall be upsized to a 12-inch Polyvinyl Chloride (PVC) pipe and laid at a slope of 0.49%.
- b. 12-inch PVC sewer between manhole 7512 and manhole 7515 – the 98 feet of sewer also does not have sufficient capacity per the sanitary sewer adequate outfall analysis on Sheet C-601 of the Infrastructure Plan. The sewer shall be upgraded to a 16-inch PVC pipe to serve the proposed development.

The Preliminary Infrastructure Plan submission dated October 21, 2020 estimates a peak sewer flow of 1.07 MGD using peaking factor of 4.0 and a 35% reduction in sanitary flow for using low flow fixtures. This flow is equivalent to 73 townhome units, 796 multifamily units and 141,300 square feet of office/retail use. If the applicant proposes increases to the number of residential units and square footages for office/retail uses (or comparable uses) than what is shown above, additional analysis and/or improvements will be required as part of the DSUP process.

Wastewater flows from this development will be treated at the AlexRenew wastewater treatment facility. Based on City development forecasts, it is projected that the City will reach its treatment capacity allocation sometime after 2040. As part of the City's sanitary sewer master planning process, alternatives for acquiring additional treatment plant capacity will be studied, which will be funded through sewer tap fee revenues collected citywide.

Environmental Sustainability

With the redevelopment of the site, the applicants will provide a significant amount of on-site remediation through the removal and containment of contaminated soils. In addition, the predominantly impervious site will recapture more than an additional acre of pervious surface area through the provision of ground level open space and through the use of sustainable paving materials. Open spaces and streetscapes within the site will incorporate native species and biodiversity standards as established by the City's Landscape Design Guidelines.

Stormwater management will also be employed across the site incorporating green infrastructure techniques into the public realm streetscape that will provide stormwater quality treatment as well as through individual buildings with a combination green roof, bioretention planters and other treatment devices. Incorporation of green infrastructure in the public realm is a key principle of

MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan Amendments

the Plan to promote environmental sustainability as outlined in the SWMMP.

Green Building Policy

The buildings within the CDD area will incorporate the development standards set forth in the City’s Green Building Policy at the time of redevelopment, which means that the buildings will go from LEED Certified under the last Concept Plan to a minimum of LEED Silver. Although the CDD Concept Plan does not include building designs beyond building footprints, the Green Building Policy requirements will apply to each building in the CDD area and will be fully evaluated through the subsequent DSUP submission reviews.

To advance sustainability across the site, the applicants will pursue LEED-ND or another comparable certification for the entire neighborhood. Neighborhood certifications move green certifications beyond individual building to apply and coordinated sustainable practices in a comprehensive matter to create well-connected communities. In addition, staff has also conditioned that the rooftops of multifamily buildings be designed to be solar ready through the provision of pull-wire ready conduit for potential future roof PV systems. The intent is to ensure buildings designed now do not preclude the future installation of sustainable technology.

D. Zoning Text Amendment

The subject site is zoned CDD #24. The CDD #24 zoning in the Zoning Ordinance regulates the mix of uses, permitted densities and building heights in Oakville Triangle. As part of this application, Staff proposes amendments to Section 5-602(A) of the Zoning Ordinance in the CDD #24 Zoning Table, to amend the “Uses” section to include medical care facilities as an allowable use and amend retail uses to expand the number of proposed uses which are appropriate for consideration within Oakville Triangle as a vibrant, mixed-use neighborhood.

Table 3: CDD #24 Zoning Table with Proposed Revisions
(Revisions are shown as underlined text.)

CDD Number	CDD Name	Without a CDD Special Use Permit	With a CDD Special Use Permit		
			Maximum Development	Maximum Height	Uses
24	Oakville Triangle and Route 1 Corridor	The CSL, I and R2-5 zone regulations shall apply to the properties as generally depicted within	The development controls for each block including gross Floor area (GFA), the size of public open spaces, ground level open spaces, the land	Heights and height transitions shall be as depicted in the approved Oakville Triangle and Route 1 Corridor Vision Plan and Urban	1) Mixed-use development to include <u>amusement enterprises; child care home; day care center; health and athletic club, health professional office; home professional office; park and community recreation</u>

*MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan Amendments*

		<p>Figure 23 of the approved Oakville Triangle and Route 1 Corridor Vision Plan and Urban Design Standards and Guidelines.</p>	<p>uses, and the ground floor uses shall comply with the Oakville Triangle and Route 1 Vision Plan and Urban Design Standards and Guidelines.</p> <p>All streets, blocks, sidewalks, building forms, building volumes, building heights, land uses, screening of parking, retail design, signage, open space and associated elements shall comply with the Oakville-Route 1 Route 1 Vision Plan and Urban Design Standards.</p> <p>Any variation from the standards shall require approval by the City Council as part of the DSUP or associated approval application(s)</p>	<p>Design Standards and Guidelines, <u>as may be amended.</u></p>	<p><u>buildings; outdoor dining; valet parking; hotel, restaurant, business and professional office, residential multifamily dwelling; townhouse dwelling; home for the elderly; nursing home,; parks and open spaces; private school (commercial); private school (academic); personal service public schools; special utility.</u></p> <p>2) <u>Retail shopping establishment and personal service; light assembly, service and crafts; and maker uses as defined in the approved Oakville Triangle and Route 1 Corridor Vision Plan and Urban Design Standards and Guidelines.</u></p> <p>3) <u>Community Facilities as defined in the approved Oakville Triangle and Route 1 Corridor Vision Plan and Urban Design Standards and Guidelines.</u></p> <p>4) <u>Other similar pedestrian-oriented uses as approved by the Director of P&Z to meet the intent of providing active pedestrian-oriented uses are allowed.</u></p> <p>5) <u>Medical Care Facilities</u></p>
--	--	--	--	---	---

E. Developer Contributions

In addition to standard contributions as part of redevelopment, the applicant will provide developer contributions as outlined in the Plan and subsequent rezoning to include the following:

- Mount Jefferson Park Improvements;
- Lane modification improvements at Swann Avenue and Route 1;
- New signalized pedestrian crossing and intersection improvements at Fannon Street and Route 1;
- Streetscape and underground of utilizes on Route 1 frontage with Phase 1 redevelopment;
- Provision of four publicly accessible open spaces within Oakville Triangle, including the construction and maintenance of Block C open space adjacent to Mount Jefferson Park;
- Provision of 2 bikeshare locations;
- An increase in E. Glebe Road & Route 1 Phase 1 contributions from \$200,000 to \$350,000;
- Provision of 65 on-site affordable housing units at 60% Average Medium Income (AMI). The applicant has increased the term of affordability from 25 years to 35 years.

Per the 2015 Plan, it was anticipated that the applicant would provide an additional contribution in the amount of \$1.4 million dollars towards signal phasing and intersection improvements at E. Glebe Road from new-North South Road to Route 1. Since that time, with the Phase 1 redevelopment of North Potomac Yard and associated improvements, this funding is no longer needed for these intersection improvements. As a result, this contribution was apportioned to increase the E. Glebe and Route 1 Phase 1 contribution from \$200,000 to \$350,000 and utilize the additional funds to increase the terms of affordability for the 65 set aside units from 25 to 35 years.

III. COMMUNITY

With the COVID-19 pandemic emergency and the Governor's March 2020 directive to limit in-person gatherings, the applicant team worked with staff to provide virtual community meetings to ensure community engagement and participation during this process.

From March through May of 2020, the applicant team, in conjunction with Inova Health Systems, held three virtual community meetings to provide an overview of the proposed amendments to facilitate the integration of the Inova Healthplex into the Oakville Triangle site. Meeting topics addressed the change in the land use development program, site access and circulation to accommodate the Healthplex, and transportation impacts. The intent of these meetings was to inform the community of the proposed amendments to the Small Area Plan and CDD Concept Plan, solicit feedback, and address any community concerns prior to development concept plans for individual buildings being submitted to the City.

In July, the applicants worked with the City to solicit community feedback to assist in the development of design proposals for the new public open space within Oakville Triangle. An

MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan Amendments

online community feedback portal was available for two weeks, where the community was able to select design elements, features, and themes that could be considered and used to develop design concepts. The applicants provided project updates in their August virtual meeting presenting the first conceptual building designs, providing updates to the applicants transportation analysis presenting at a prior meeting, and reported themes received from the community engagement feedback to inform the design concepts for the new public open space.

The virtual community meetings held in September and during the October and November Parks & Recreation Commission meetings were focused specifically on advancing the concept designs for the new public open space, which will follow a separate approval process.

At the applicant’s final virtual community meeting in November, the team provided updated building designs and site renderings for the proposed development of the three buildings on Blocks A1, A2, and B; updates on the central open space designs; updates related to sustainability plans for Oakville Triangle, and updates related to the project review and approval schedule.

Table 3: Community Engagement

Date	Meeting Forum
March 26, 2020	Virtual Community Meeting
April 16, 2020	Virtual Community Meeting
May 7, 2020	Virtual Community Meeting
August 4, 2020	Virtual Community Meeting
September 24, 2020	Virtual Community Meeting
October 15, 2020	Virtual Parks & Recreation Commission Meeting
November 5, 2020	Virtual Alexandria Affordable Housing Advisory Committee
November 10, 2020	Virtual Community Meeting
November 19, 2020	Virtual Parks & Recreation Commission Meeting

The applicant also presented the proposal to the Alexandria Affordable Housing Advisory Committee (AHAAC) November 5th virtual meeting. The meeting was advertised and open to the public. The applicant’s proposal, which increased the period of affordability from 25 years to 35 years for 65 set-aside affordable units at 60% AMI was accepted by AHAAC. The applicant’s proposal received a vote of approval from the committee.

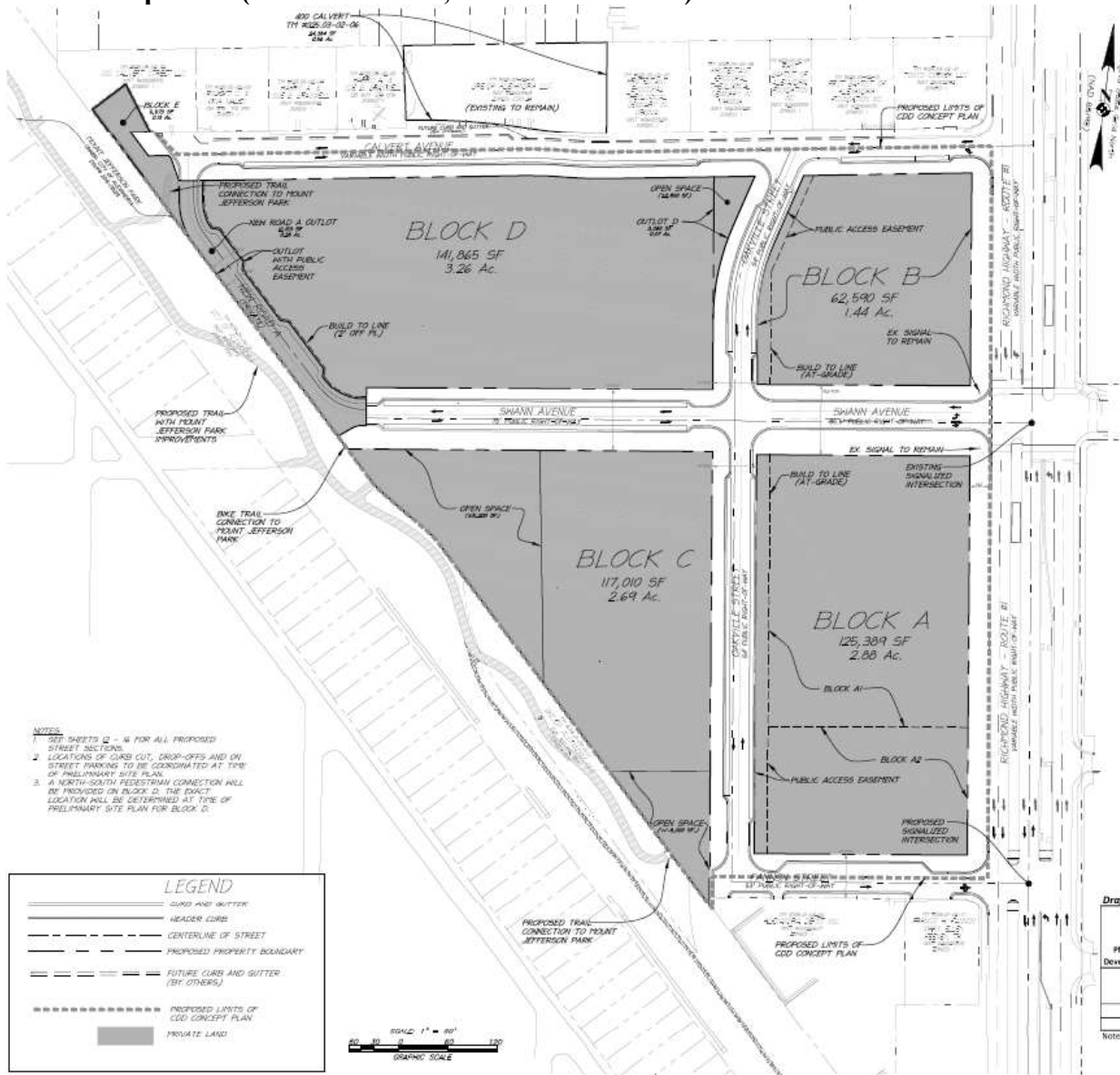
IV. CONCLUSION

Staff recommends approval subject to compliance with all applicable codes, ordinances, and the following staff recommendations.

MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan Amendments

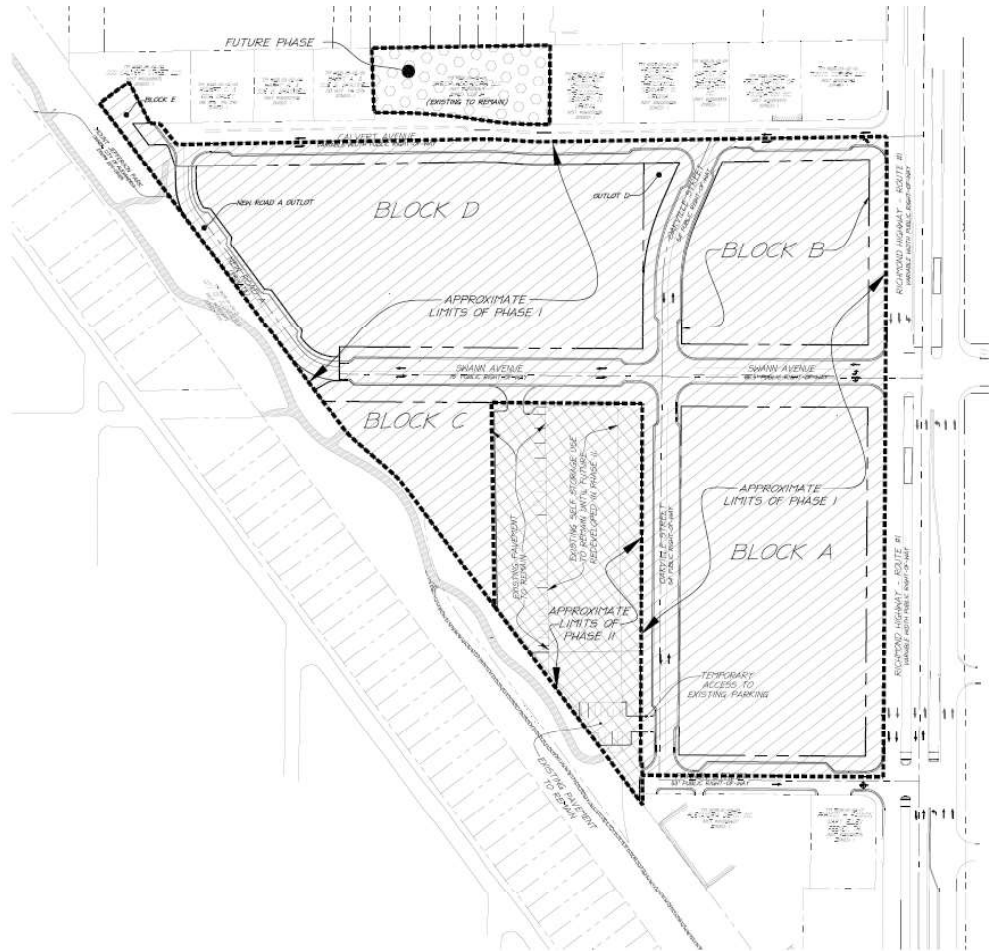
V. GRAPHICS

CDD Concept Plan (dated 9.15.2020, revised 11.03.2020)



MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan Amendments

CDD Phasing Plan



DATE	REVISION
1-16-21	CDD AMENDMENT



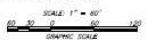
LEGEND

- CURBS AND GUTTER
- HEADER CURB
- CENTRELINE OF STREET
- PROPERTY BOUNDARY
- FUTURE CURB AND GUTTER (BY OTHERS)
- PHASE I OF CDD CONCEPT PLAN
- PHASE II OF CDD CONCEPT PLAN
- FUTURE PHASE

NOTES:
 1. PHASES MAY BE FURTHER DIVIDED INTO SUB-PHASES IN ACCORDANCE WITH DRSP WITH PRELIMINARY SITE PLAN APPROVAL.

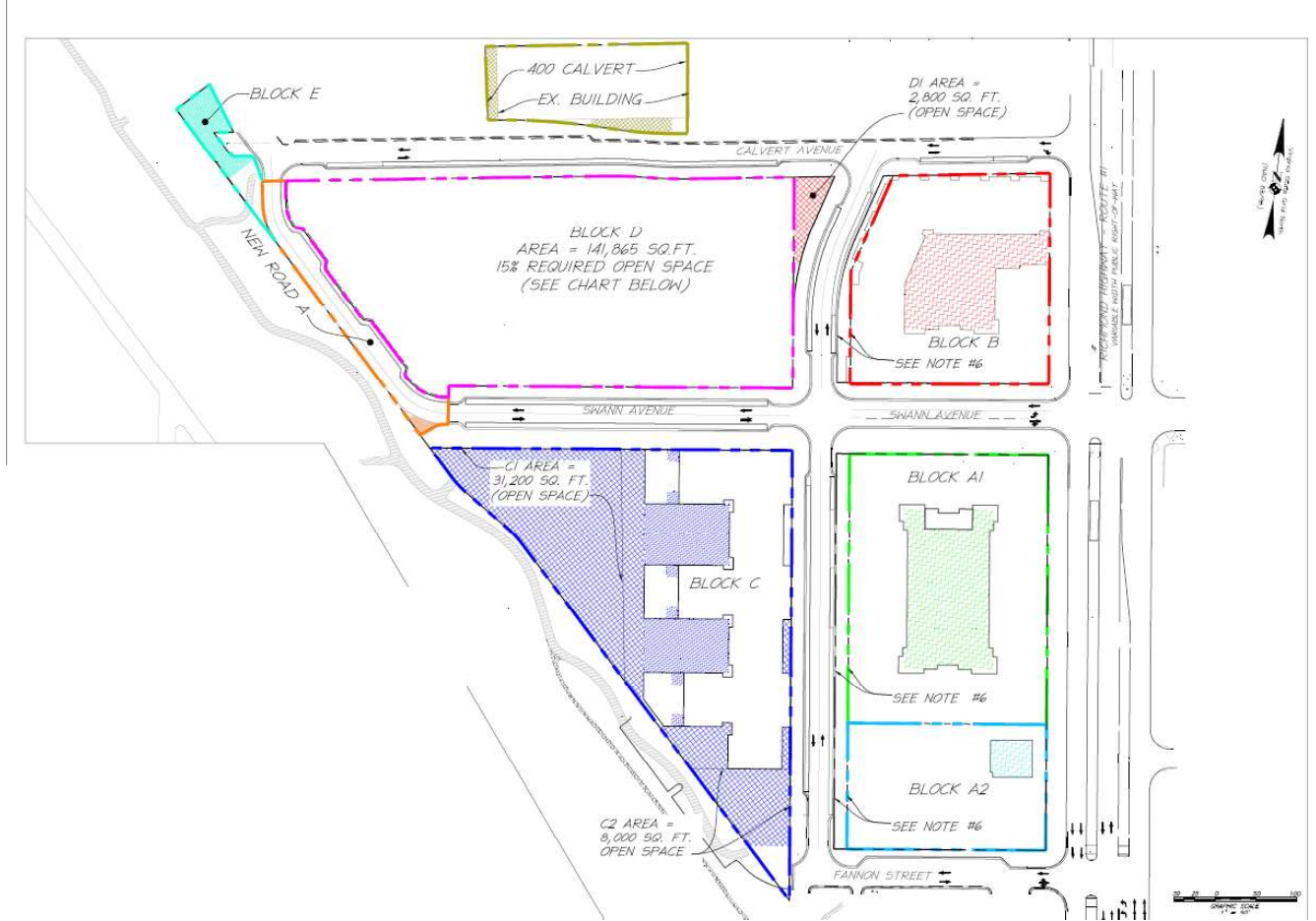
PHASING NOTE:
 THE INITIAL PHASE OF DEVELOPMENT SHALL CONSIST OF INFRASTRUCTURE, BLOCK A, B, C AND BLOCKS D OF OPEN SPACE ON BLOCK C. THE EXISTING SELF STORAGE LOCATED ON BLOCK C SHALL REMAIN IN PHASE I AS AN INTERIM USE.

THE SECOND PHASE OF DEVELOPMENT SHALL INCLUDE CONSTRUCTION OF THE HEAVY DUTY SURVEYING INSTRUMENT AL CONSTRUCTION OF THE 8,000 SQ OPEN SPACE ON THE SOUTHERN END OF BLOCK C AND THE SPACING OF THE SELF STORAGE BUILDING, THE 400 CARPORT SPACES.
 (TYPING IN 20-20) SHALL REMAIN AS EXISTING IN PHASE I AND 2 AND WILL NOT BE DEVELOPED UNTIL SOME FUTURE PHASE.



MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan Amendments

CDD Open Space Exhibit



VI. STAFF RECOMMENDATIONS

A. GENERAL

1. Stonebridge Associates, Inc., herein referred to as “the Applicant” shall comply with the following Coordinated Development District (CDD) Approvals: 1) the CDD Conceptual Design Plan, hereafter referred to as CDD Concept Plan, prepared by Christopher Consultants, Inc. and dated 9/15/2020 and revised on 11/03/2020; 2) the conditions contained herein; 3) approved Oakville Triangle & Route 1 Corridor Vision Plan and Urban Design Guidelines & Standards (herein referred to as ‘the Plan’), dated 10/1/2015 and any further amendments; and, 4) the requirements of CDD #24. (P&Z)
2. The Applicant, has submitted documents relating to its application for approval of a CDD Concept Design Plan, and associated approvals for the Oakville Triangle project within the Coordinated Development District (CDD #24). The documents submitted by the Applicant, 9/15/2020 and revised on 11/03/2020, are collectively referred to as the ‘CDD Concept Design Plan (CDP)’ submitted for approval and subject to the following conditions. (P&Z)
3. Each block(s) and/or building(s) shall obtain approval of a Development Special Use Permit (DSUP) and any other applicable approvals. A DSUP may be submitted for a portion of a block when an applicant can provide sufficient information regarding the location, approximate size, type, uses, open space, parking, loading access and additional information as needed for the remainder of the block and adjoining blocks, streets and open space to the satisfaction of the Directors of P&Z, RP&CA and T&ES. (P&Z)
4. The conditions of this approval are binding upon the Applicant, its successors and/or assigns. (P&Z)
5. For the purposes of these conditions, the following items shall be considered accordingly:
 - a. The subject site of the CDP application, hereafter referred to as the “CDP area” is subject to the conditions herein.
 - b. The applicant’s CDD Concept Plan submission defined construction within two broad timeframes, Phases 1 and 2 (referred to as “Phase 1” and “Phase 2” herein). The extents of the respective development and any associated conditions phases shall be as shown in the CDD Concept Plan submission sheet C02.1 ‘Conceptual Design Plan’ “Phasing Plan” (exhibit 1). (P&Z)
6. Notwithstanding any contrary provisions in the Zoning Ordinance, the Oakville Triangle CDD Concept Design Plan (CDD#2020-0003 or any revisions thereof), shall remain valid for 10 years from 1st January, 2021. (P&Z)

MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan

7. The Directors of T&ES, RP&CA and P&Z may require that infrastructure, open space, land uses and other matters adjacent to the subject site deemed necessary to review a preliminary DSUP application also be shown in the application. (P&Z) (T&ES) (RP&CA)
8. The Applicant(s) shall coordinate, to the extent necessary, with other property owners and Applicant(s) within CDD #24 on the design of streets, parks-open spaces, sewer systems and other related infrastructure and construction. (P&Z) (RP&CA) (T&ES)
9. If a dedication required herein results in a use, lot, or structure being no longer in conformity with applicable zoning requirements, such lot or structure shall be treated as a noncomplying use and noncomplying structure as defined in the Zoning Ordinance. (T&ES)(P&Z)

B. CDD CONCEPT DESIGN PLAN

10. The maximum number of residential units shall be limited to the GFA within the CDP or 920 units including both multifamily and townhouse units as indicated. Unless amended, GFA as indicated shall not increase, however, an increase in unit count may be considered as part of the DSUP process pending applicable infrastructure analysis. (P&Z) (T&ES)
11. All new streets planned within the CDP shall be designed and built to be consistent with the street cross-sections provided in the CDD Concept Plan submission dated September 15, 2020 and the Oakville Triangle & Route 1 Vision Plan and Urban Design Standards & Guidelines..
 - a. Sidewalk, vehicular lane, parking lane, and landscape strip dimensions for all new and reconfigured streets within the Oakville Triangle, shall be consistent with the approved street cross-sections standards in the CDD Concept Plan.
 - b. Right-of-way (R.O.W.) dimensions for all new and reconfigured streets within the CDP area shall be consistent with the approved street cross-sections standards in the CDD Concept Plan.
12. Provide public access easements and maintenance agreements for the eastern streetscape of Oakville St. for use by the public for a public sidewalk, as generally shown on the CDD Concept Plan, which allow the City to maintain sidewalks as necessary for all sidewalk areas between the ROW line and the face of buildings. (P&Z) (T&ES)

C. INFRASTRUCTURE & PHASING

13. Within 120 days of approval of the CDP Concept Plan by the City Council or prior to the release of the Phase 1 Infrastructure Final Site Plan, whichever is later, the Applicant shall submit the necessary plans and documentation to

MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan

- dedicate or grant to the City or as otherwise directed by the City, in fee simple or by easement the following:
- a. Dedication of right-of ways;
 - b. Dedication of the area identified as Block E on the CDD Concept Plan. This area remains allowable to count towards the requirements for open space and crown coverage; and
 - c. All public access easements associated with Phase 1. (T&ES)(P&Z)(RP&CA)
14. The Applicant(s) shall be responsible for dedicating all necessary rights-of-way and/or public access easement(s) as required herein and as required by the Oakville Triangle & Route 1 Corridor Vision Plan. Where a public access easement is provided for open space, plazas, streets, and/or sidewalks, the easement(s) shall be a perpetual public access easement for pedestrians and where appropriate, vehicles. (T&ES)(P&Z)(RP&CA)
15. All streets within the CDP area shall be public dedicated streets or public access easements as depicted on the CDD Concept Plans, and as required herein. (T&ES)
16. At a minimum, one north-south connection between Calvert Avenue and Swann Avenue shall be provided within Block D. The type of connection (pedestrian or vehicular) will be determined as part of the development review process for Block D. (P&Z) (T&ES)
17. Phase 1 Infrastructure Final Site Plan: Within 120 days of approval of the CDP by the City Council, the Applicant shall submit for review and approval by the Directors of P&Z, RP&CA and T&ES the necessary plans and documentation for the following:
- a. The Phase 1 Infrastructure Final Site Plan commensurate with area identified as the Preliminary Infrastructure Plan (Minimum Submission Review to ESI will constitute compliance with this requirement). The plan must meet all final site plan requirements as applicable, including, but not limited to the following: Horizontal and vertical profiles for the streets, sanitary sewers, and storm sewers; and shall include all horizontal locations and cross sections for the water lines. The plan shall also include cross sections, construction details, signal design plan for the Swann Ave and Route 1 intersection and the Fannon St. and Route 1 intersection, complete with phasing diagram, layout of connected underground conduit grid within all roadways/street connection throughout site and any other concept design elements identified as necessary by the Director of T&ES;
 - b. Provide a Stormwater Management Master Plan (SWMMP) approach demonstrating compliance with water quality and quantity requirements for each phase.
 - c. Provide conceptual undergrounding plan for power and communication lines;
 - d. Conceptual design analysis of the intersection alignment of the North-South Street connection (Oakville Street) between Calvert Avenue and Raymond Avenue to verify the intersection design as shown on the Infrastructure Site Plan. (P&Z) (T&ES) (RP&CA)

18. Transitions: For roadways, sidewalks, or trails to be provided by the Applicant(s), pursuant to the conditions herein, the Applicant(s) shall coordinate with the adjacent property owner(s) to build any necessary transition(s) from the street cross section pursuant to the Oakville Triangle & Route 1 Corridor Vision Plan to the existing street on the adjacent property owner(s)'s land. In the event the adjacent property owner does not cooperate through granting easements or otherwise, the Applicant(s) shall build the transition on the Applicant(s)'s land. If this occurs and the transition was built on the original Applicant(s)'s property, at the time the adjacent property redevelops, the final street cross-section as depicted in the Oakville Triangle & Route 1 Corridor Vision Plan shall be completed by the new Applicant(s). (T&ES).

D. CDP PHASING PLAN

19. CDP Phasing Plan - The Applicant shall submit a CDP Phasing Plan with the first Phase 1 Infrastructure Final Site Plan submission, and shall update the Phasing Plan concurrently with each subsequent FSP submission. The Phasing Plan shall be subject to the following to the satisfaction of the Directors of P&Z, T&ES and RP&CA:
- a. Provide, for the entire CDP, a general outline of the site and the Applicant's most up-to-date projection of the dates when construction of the different land uses (i.e., medical care facility, office, retail, residential, open space and community facilities) for each block shall commence;
 - b. Provide, for infrastructure identified below in this subparagraph: (1) the general location and layout of the major components of the infrastructure (such components to be determined by the Director of T&ES), and (2) the dates when construction of the infrastructure shall commence (provided, that the projected dates for the commencement of construction of these components shall be consistent with the triggers noted herein). The infrastructure to be provided shall include:
 - i. The street layout;
 - ii. The sanitary sewer system;
 - iii. The stormwater management system;
 - iv. The utility systems to be constructed within the CDP (e.g., electricity, water, gas, phone/communications and cable); and
 - v. The off-site improvements.
 - c. Demonstrate compliance with water quality and quantity requirements. Stormwater quality facility best management practices (BMPs) implemented to meet the state phosphorus reduction and the Alexandria water quality volume default (WQVD) requirements must be installed as designed and functioning properly for each phase of development prior to the completion of each phase being constructed, independent of future improvements in subsequent phases.
 - d. Seek and secure coverage under the General Virginia Pollutant Discharge Elimination System (VPDES) Permit for Discharges of Stormwater from

MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan

Construction Activities for each DSUP prior to commencing land disturbing activities.

- e. Parks/Open Space - Depict and label for each park/open space area required by the conditions herein: (a) the proposed size and location of the park/open space, and (b) the time frame when construction of the improvements to the park/open space is required and/or projected to commence;
- f. Circulation – Provide a circulation plan depicting the temporary pedestrian, bicycle and vehicular circulation during the different construction phases. The plan shall identify temporary sidewalks, fencing around the site and any other features necessary to ensure safe pedestrian and vehicular travel around the site during construction and during the phasing of the development, including methods for constructing the underground parking garages without disturbing pedestrian access from completed portions of the project;
- g. Minimum Performance Standards - The Phasing Plan shall comply with the following criteria:
 - i. Must include a SWMMP which demonstrates water quality is met with the overall condition, with each phase presenting the overall SWMMP and demonstrate compliance during each phase.
 - ii. Stormwater runoff from public roads must be treated in a BMP per the Green Sidewalks Guidelines when technically feasible..
- h. Notwithstanding the above, the Applicant, at its discretion, may submit an updated CDP Phasing Plan from time to time for administrative review and approval by the Directors of P&Z and T&ES; provided, that no such submission shall relieve the Applicant of the requirement that it submit an updated CDP Phasing Plan with each DSUP application that seeks approval of one or more buildings or structures within the CDD.
- i. Updated tabulations for:
 - i. Development sf (GFA) per use and per block/building
 - ii. Open space sf
 - iii. Parking spaces
 - iv. Unit numbersThe updated tabulation shall indicate both what is proposed and cumulatively what has been approved to date. (P&Z) (T&ES) (RP&CA)

E. DEVELOPMENT PHASING

- 20. The applicant shall construct the development in the following phases:
 - a. Development approvals for Blocks A1, A2, B, D, Block C open space, and Mount Jefferson Park Improvements may be constructed concurrently with the Infrastructure Plan subject to approvals by the Planning Commission and City Council. The street layout and supporting infrastructure will be evaluated with each individual DSUP.

MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan

- b. The approximately 31,200 sq. ft. open space on Block C, to the extents as shown on the applicant's Concept Plan Sheet C02.0 (exhibit 2), between Mount Jefferson Park and future residential development on Block C, shall be constructed and certified substantially complete by the landscape architect, in consultation with and advance notice given to the City prior to the issuance of the final certificate of occupancy for Block A1 or B (excluding above and below grade parking and loading areas), whichever building is constructed first. With each Final Site Plan within the CDP area a public art plan shall be provided in accordance with Section X. If public art is proposed to be located within this open space, the public art plan shall depict the general location and schedule of art installation with the open space to ensure the public art is installed during construction of the open space
- c. The at-grade open spaces within the New Road "A" Outlot A and Block E shall be constructed and fully operational prior to City's the Acceptance of Mount Jefferson Park Improvements.
- d. Interim landscaping on the redeveloped portion of C2 constructed in Phase 1 shall be installed to include the trail connection at Fannon and Oakville Street and shall be provided with the Infrastructure DSUP. The final design of the at-grade open space shall be designed and constructed concurrently with the DSUP for redevelopment of Block C as a residential use.
(P&Z)(T&ES)

F. DEVELOPMENT PHASING TRIGGERS

- 21. Development Phasing Triggers - During the development of the CDP area, the following infrastructure shall be provided and/or constructed by the Applicant in accordance with the following:
 - a. **Streets (temporary condition):** The streets (curb to curb), as shown on the Infrastructure Preliminary Site Plan (DSUP 2020-10029), shall be constructed and operational with (at a minimum) base paving as follows:
 - i. Calvert Ave from Route 1 to Oakville St. prior to issuance of first Certificate of Occupancy on Block B or D, whichever is earlier.
 - ii. Calvert Ave from Oakville St. to New Road A prior to issuance of first Certificate of Occupancy on Block B or D, whichever is earlier
 - iii. New Road A prior to issuance of first Certificate of Occupancy on Block B or D, whichever is earlier.
 - iv. Swann Ave from Route 1 to Oakville St., including the completion of any signal modifications for the eastbound approach at Route 1 intersection, prior to issuance of Certificate of Occupancy on Block A1 or B, whichever is earlier.
 - v. Swann Ave from Oakville St. to New Road A prior to issuance of Certificate of Occupancy on Block B or D, whichever is earlier.
 - vi. Oakville St. from Calvert St. to Swann Ave prior to issuance of Certificate of Occupancy on Block B or D, whichever is earlier.

***MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan***

- vii. Oakville St. from Swann Ave to Fannon St. prior to issuance of Certificate of Occupancy on Block A.
 - viii. Fannon St. prior to issuance of first Certificate of Occupancy on Block A2.
 - b. **Streets (permanent condition):** Final condition (including asphalt topping course and striping) for all streets shall occur prior to the approval of the final as-built and release of the performance bond associated with the asphalt improvements as shown on the Phase 1 Infrastructure Final Site Plan. .
 - c. **Streetscapes (temporary condition):** If improvements on a Block, subject to an individual DSUP approval, have not commenced substantial construction in accordance with the Block’s individual DSUP approval, then prior to the approval of the final as-built and release of the performance bond for streetscape improvements as shown on the Phase 1 Infrastructure Final Site Plan, temporary streetscapes within the curb limits of any individual Block awaiting commencement of construction shall be installed in accordance with the approved Phase 1 Infrastructure Final Site Plan and in accordance with the following:
 - i. The raised, 2-way bicycle track shall be completely constructed from Route 1 to Mount Jefferson Park prior to issuance of the Block A1 Certificate of Occupancy.
 - d. **Streetscapes (permanent condition):** Permanent streetscapes, as shown on the individual Block DSUP, shall be installed with each individual Block DSUP. Prior to issuance of the first Certificate of Occupancy for an individual DSUP (not including above or below grade parking), the final streetscape, including street-side bio retention as shown on the Phase 1 Infrastructure Final Site Plan, on all frontages of the Block shall be constructed.
 - e. First phase of transportation improvements –
 - i. The applicant is responsible for the construction of lane modifications, as well as associated modification to existing signal for eastbound Swann Avenue at Route 1 intersection, including from one shared lane to one exclusive left turn lane and one shared-thru/right lanes as shown within the CDD Concept Plan. The improvements shall be operational prior to issuance of first Certificate of Occupancy for development on Block A or B, whichever is earlier (not including above or below grade parking).
 - ii. The proposed signalized intersection at the Fannon Street and Route 1 intersection shall be constructed and operational prior to the Certificate of Occupancy of Block A2. However, construction of the signalized intersection, including lane modifications to Fannon Street and Route 1 shall not be constructed prior to the release of the Final Site Plan for Block A2. (T&ES)
 - iii. Provide \$350,000 to be used for improvements at Route 1/Glebe prior to release of the final site plan for Block A1 or B, whichever is earlier. The amount shall be adjusted annually by CPI-U from 2021.

MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan

- f. The utility undergrounding on Route 1 frontage adjacent to the CDP area shall be completed prior to the issuance of the first Certificate of Occupancy for any building on Block A or B.
(P&Z) (T&ES) (RP&CA)

G. MOUNT JEFFERSON PARK

- 22. The applicant is responsible for all improvements associated with the site plan for Mount Jefferson Park Improvements (DSP2018-0026), subject to the site plan being a logical extension (and not an expansion) of the preliminary plan dated 9/9/15 (as amended). The applicant is responsible for the development of the final site plan, limited remediation, and fulfillment of all associated conditions of approval, except for Section T: Contaminated Land.
 - a. Any modifications to the Final Site Plan for DSP2018-0026 shall be submitted concurrently with the Phase 1 Infrastructure Final Site Plan for the CDP area. The Final Site Plan shall be in substantial conformance with the preliminary plan dated 9/9/15 (as amended). ****
 - b. The improvements shall be:
 - i. underway within a maximum of 12 months of the issuance of a permit to demolish any building or structure in or adjacent to the Park; and
 - c. fully constructed by the applicant and certified substantially complete by the landscape architect, in consultation with and advance notice given to the City prior to the issuance of the first Certificate of Occupancy for any building in the CDP area, or within a maximum 24 months of the issuance of a permit to demolish any building or structure in or adjacent to the Park (whichever is earlier). The applicant will fund the cost of soil remediation work required within the proposed disturbed areas in Mount Jefferson Park limited to the excavation, if required, of non-hazardous contaminated soil and hauling it to a regulated landfill that accepts non-hazardous soils, as required per the Applicant's environmental consultant per the given use. If environmental investigations, performed by the applicant's environmental consultant, find that onsite soils within the proposed disturbed areas contain levels of contamination to be determined hazardous, the developer will not be responsible for disposal of hazardous soil and shall not be obligated to complete the construction of the proposed park improvement's within the area contaminated by hazardous soil until all hazardous soils have been removed from the site.

Any delays to the completion of the Mt Jefferson park improvements due to third party environmental investigations and third party remediation of hazardous materials shall not prevent issuance of Certificates of Occupancy for any building in the CDP area; provided that the applicant has completed all other park improvements not impacted by the third party delays. The time for completion of all work impacted by the delays shall be extended for the same number of days which that the applicant was

MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan

prevented from working on the project.

23. The applicant will fund the cost of all assessment and related remediation work or systems required for any contaminated soils associated with the existing building currently located within the Mount Jefferson Park site, on tax parcels #025-03-0215 and #035.01-0421. (P&Z)(T&ES)(RP&CA)
24. The applicant shall be responsible for maintenance of the project area during construction including plantings and slope/site stabilization during final build-out/completion of work. Maintenance shall be performed consistent with City of Alexandria Landscape Guidelines and the City of Alexandria Zoning Ordinance. (RP&CA)(P&Z)(T&ES)
25. The applicant shall post a performance bond commensurate with City requirements for improvements to public land, specific to DSP2018-0026. The bond shall be posted and approved by the City prior to commencement of ground disturbing activity and remain in place for the full duration of planting warranty period. (RP&CA)
26. The City's Acceptance of the Mount Jefferson Park Improvements site (DSP2018-0026) including its maintenance responsibilities, shall occur upon approval of the final As-Built (including but not limited to demonstration of compliance with associated conditions of approval, as-built plan, required documentation, and any necessary site corrections) and completion of the one-year maintenance bond period after the performance bond for public improvements are released. Final as-built drawings shall be provided as AutoCad Version 2015 files on 5 CD's and six paper copies at a scale commensurate with the approved Final Site Plan. *****(P&Z)(T&ES)(RP&CA)
27. If there are outstanding performance, completion or other bonds relevant to DSP2018-0026 for the benefit of the City and in effect for the property at such time as it may be conveyed or sold to a party other than the applicant, a substitute bond must be provided

by that party or, in the alternative, an assignment or other documentation from the bonding company indicating that the existing bond remains in effect despite the change in ownership may be provided. The bond(s) shall be maintained until such time that all requirements are met and the bond(s) released by the City. (T&ES)
28. The stormwater Best Management Practices (BMPs) required for the Mount Jefferson Park Improvements project DSP2018-0026 shall be maintained by the applicant through the one-year maintenance bond period after the performance bond for public improvements (excluding landscaping) is released. Prior to release of the performance bond, and again prior to release of the maintenance bond for public improvements (excluding landscaping), the design professional shall submit a written sealed certification to the Director of T&ES that the BMPs are:
 - a. Constructed and installed as designed and in accordance with the approved

MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan

Final Site Plan.

- b. Clean and free of debris, soil, and litter by either having been installed or brought into service after the site was stabilized. **** (T&ES)
29. Submit a copy of the Operation and Maintenance Manual to the Stormwater Management Section on digital media for DSP2018-0026 prior to release of the performance bond.
****(T&ES)
30. Prior to release of the performance bond for DSP2018-0026, submit a certification by a qualified professional to the satisfaction of the Director of T&ES that any existing stormwater management facilities adjacent to the project and associated conveyance systems were not adversely affected by construction operations. If maintenance of the facility or systems were required in order to make this certification, provide a description of the maintenance measures performed.
****(T&ES)

H. LANDSCAPE/PEDESTRIAN/STREETSCAPE

31. Develop, provide, install and maintain an integrated Landscape Plan with the final site plan that is coordinated with other associated site conditions, and is updated with each subsequent DSUP submission for approval of blocks/buildings to the satisfaction of the Directors of P&Z and/or RP&CA. At a minimum the Landscape Plan shall:
- a. Meet the requirements in the City's *Landscape Guidelines*
 - b. Meet the requirements in the City's *Green Sidewalks Guidelines* for the sidewalk BMP elements. (P&Z) (T&ES) (RP&CA)
 - c. Or as otherwise approved as part of the DSUP process.

I. OPEN SPACE

32. A total of 40% (approximately 105,966 sf) of the mixed-use development land area (approximately 264,915sf) as shown on Sheet C3.0 title Open Space Exhibit, of the CDD Concept Plan shall be provided as useable open space or rooftop amenity space. A minimum of 20% (52,983) of the mixed-use development land area shall be as ground-level open space and shall be generally consistent with the CDD Concept Plan submission. A minimum 45,500 sf within the CDP area shall be ground-level open space useable and accessible to the public, as generally depicted on the CDD Concept Plan as areas C1, C2, D1 and Outlot E. The townhome development (Block D) shall provide a minimum of 15% ground-level open space and a minimum 10% roof-top open space. Public right-of-ways and access easements between the property line and the western face of buildings on Block A and B shall not be counted as open space and should not be included in the total area calculations.

MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan

33. Roof top open space/amenity space shall be designed as high-quality open space with active and passive uses for residents and building tenants. Roof top open space on office buildings may be accessible to the public if compatible with the building use as determined by the Directors of RP&CA and P&Z in consultation with the Applicant as part of the DSUP process. Roof top open space shall be physically and/or visually accessible. (P&Z)(RP&CA)
34. All ground-level open space, including courtyards, plazas, and private internal courtyards shall be designed as high-quality open space for residents, building tenants and where appropriate, the public. (P&Z)(RP&CA)
35. Ground level open space in Blocks C1 and E as depicted on Sheet C03.0 in the CDD Concept Plan shall be fully open to the public following the hours and guidelines established by the Department of Recreation, Parks and Cultural Activities. Signage for all ground level open spaces with public access easements shall be posted at each site displaying the hours, guidelines and property contact information and shall be designed through the DSUP process to reflect the public nature of the spaces. Guidelines for use of this spaces by the City and/or public shall be developed and agreed upon prior to issuance of first certificate of Occupancy for Block A1 or B. (P&Z)(RP&CA)
36. The total acreage of publicly accessible ground level open space in Blocks C1, C2, D1, and E as depicted on Sheet C3.0 of the CDD Concept Plan is approximately 1.044 acres (45,500 square feet). The applicant shall dedicate perpetual public access easements for each of these areas prior to the issuance of the certificate of occupancy for adjoining development block. (P&Z)(RP&CA)
37. Publicly accessible ground level open spaces shall be maintained by the applicant/owner or its successors, , subject to the terms and conditions to be agreed upon between the City and Applicant as to use of the property for City events. Where publicly accessible open space abuts Mount Jefferson Park, the owner/successor shall review and coordinate maintenance responsibilities and schedules with the Department of Recreation, Parks and Cultural Activities. (P&Z)(RP&CA)
38. There shall be no occupiable buildings constructed in publicly accessible open spaces. (P&Z)(RP&CA)

J. INTERIM USES AND INTERIM CONDITIONS

39. Care should be taken to provide an attractive and welcoming environment during interim phases when a portion of the project has been constructed but adjacent areas within the same Block have been left vacant and with construction of new improvements having been delayed to a subsequent development phase for more than 12 months. Temporary screening should be provided to conceal such unfinished construction and incomplete areas of the project. This screening should

MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan

be appropriately scaled, as reasonably practicable and consistent with customary industry standards, whether it is adjacent to heavily used pedestrian areas or visible only from a distance. Interim conditions associated with phased development may include, but are not limited to, treatment of undeveloped parcels or portions of parcels; and treatment of visible portions of newly constructed structures left unfinished and intended to be covered by future improvements. It is not the intent of Section J to require areas that remain under continuous development to be temporarily screened. Final interim conditions shall be approved to the satisfaction of the Directors of P&Z and T&ES, along the following guidelines:

- a. In the event a portion of a newly constructed building or Block is left in an unfinished condition and construction is interrupted for more than a temporary period of time, plantings or other methods of screening shall be used to screen and buffer views of the temporary conditions (e.g. parking garages, faces of buildings) intended to be covered by future construction. Plant materials shall be fast growing species, primarily evergreen, and appropriate for short-term use. Planting / landscape interim conditions shall be to the approval of the Directors of P&Z, RP&CA and T&ES, along the following guidelines:
 - i. Plantings shall be consistent with the Alexandria Landscape Design Guidelines.
 - ii. Undeveloped parcels (with the exclusion of Block C) shall be enhanced with temporary landscape treatments including:
 - i. Temporary walkways: If pedestrian circulation through undeveloped parcels is needed to link neighborhood pedestrian circulation or link developed parcels with neighborhood circulation, temporary walkways shall be constructed. Walkways shall be constructed of asphalt and be minimum 5' in width.
 - ii. Site shall be graded with gentle slopes and even transitions to offer a safe finished condition.
 - iii. Site shall be seeded with turf type grasses and maintained in a neat, mowed condition.
 - iv. The site shall remain as an open lawn area for public use (where possible).
 - v. Undeveloped parcels shall not be lighted except for streetscape walkways. (P&Z)(T&ES)
40. All interim uses which are considered by the Directors of P&Z and/or T&ES to require screening shall apply the screening and interim conditions above and the following interim uses located on Block A, B and/or D may require a higher quality of screening material. These shall include, but are not limited to:
- a. Interim parking/loading areas, entrances, ramps and garages. Surface parking at highly visible locations may require screening material and installation to an equivalent standard of adjacent buildings and/or extensive landscape screening.;

MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan

- b. Building walls and edge conditions which are not the ultimate finished condition; and,
 - c. Graded and undeveloped lots. . (P&Z)(T&ES)
41. Until such time that Block C is redeveloped pursuant to the CDD Concept Plan as a residential use the existing storage building currently existing on Block C of the CDP area shall be permitted to remain and considered to be a lawfully existing permitted use and the building shall be considered a lawfully existing permitted non-complying structure as defined in the Zoning Ordinance as such the building shall be allowed to be repaired, and re-constructed and re-established in the event of casualty, damage or destruction notwithstanding the provisions of Condition 6. The following improvements to the existing building shall be permitted to the satisfaction of the Director of Planning and Zoning:
- a. Blank walls and walls with minimal fenestration shall be treated with an appropriately scaled finish, which may include but not be limited to, painting and/or installation of additional materials and/or lighting to enhance the exterior appearance of the existing building. Any increase in FAR as a result of the application of exterior materials / panels to the façade (not to exceed 12”) may be permitted..
 - b. Primary building access may remain or be relocated to a location appropriate for the use and not to the detriment of any vehicular access /pedestrian requirements or other infrastructure improvements shown in the CDD Concept Plan.
 - c. A portion of the loading area below the building overhang may be enclosed and/or retrofitted to allow for a permanent management office serving the self-storage building (due to the need for a relocation of the existing management office currently located on Route 1), the location of which shall be appropriate for the use and not to the detriment of any vehicular access / pedestrian requirements or other infrastructure improvements shown in the CDD Concept Plan.
 - d. Temporary uses of a community-serving or retail nature facing Swann Avenue may be permitted.
 - e. Improvements to secure the property consistent with customary industry standards for a self-storage use shall be permitted.
 - f. The existing parking lot located south of the self-storage building shall remain as generally shown on the Infrastructure Preliminary Site Plan.
 - g. Plantings may be installed adjacent to the existing building that are coordinated with and are compatible to the overall design character of the CDP area
 - h. Improvements to the self-storage building on Block C shall be completed prior to the release of any portion of the performance bond associated with the improvements of the Block C open space. (P&Z)

K. USES

42. Any proposed land uses shall be subject to the development levels,

Oakville Triangle Vision Plan & CDD Concept Design Plan

requirements and locations set in the Plan. (P&Z)

*CONCEPTUAL OAKVILLE
DEVELOPMENT SUMMARY TABLE*

Block	Use	Proposed Not to Exceed Gross Floor Area [GFA] (SF) ^{11, 12}
A	Retail ¹⁵	40,000
	Residential (MF)	310,000
	Medical Care Facility	115,000
	Service / BOH / Loading ¹⁴	25,000
	Block A Subtotal GFA	490,000
	Above Grade Garage	75,000
	Below Grade Garage ¹²	EXCLUDED
	Block A Garage Subtotal GFA	75,000
Block A Total	565,000	
B ¹⁰	Retail ¹⁵	25,000
	Residential (MF)	270,000
	Hotel	0
	Service / BOH / Loading ¹⁴	10,000
	Block B Subtotal GFA	305,000
	Above Grade Garage	30,000
	Below Grade Garage ¹²	EXCLUDED
	Block B Garage Subtotal GFA	30,000
Block B Total	335,000	
C ¹⁶	Retail ¹⁵	0
	Residential (MF)	200,000
	Townhouses	0
	Service / BOH / Loading ¹⁴	10,000
	Block C Subtotal GFA	210,000
	Above Grade Garage	10,000
	Below Grade Garage ¹²	EXCLUDED
	Block C Garage Subtotal GFA	10,000
Block C Total	220,000	
D	Retail	0
	Residential (MF)	0
	Townhouses ¹³	250,000
	Service / BOH / Loading ¹⁴	0
	Block D Subtotal GFA	250,000
	Above Grade Garage	0
	Below Grade Garage ¹²	0
	Block D Garage Subtotal GFA	0
Block D Total	250,000	
E	Block E Total	0
	Total (GFA ¹²)	1,370,000

43. GFA may be transferred from one block to another within the CDP with an application for a DSUP; provided the amount of GFA transferred shall not cause GFA in any block to increase or decrease by more than 25% and is in accordance with the CDD Conditions herein. (P&Z)

44. For purposes of these CDD conditions, “Retail” or “Retail Shopping Establishment” are defined to include retail shopping establishments, personal service uses, amusement enterprises, and restaurants, as defined in the Zoning Ordinance, with the exceptions identified below:
 - a) Retail shopping establishments shall not include appliance stores and auto parts stores;
 - b) Personal service uses shall not include appliance repair and rental, contractors’ offices, laundromats, and pawnshops;
 - c) Personal service uses on the ground floor, shall be limited to a frontage less than 30 feet within the required retail areas, unless additional frontage is permitted with an

MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan

approved Special Use Permit;

d) Ground floor uses in the residential units along Calvert Avenue may be utilized for Home Occupations, maker space, retail and restaurant uses as defined by the Zoning Ordinance with the exclusions stated in subparagraph (a) above; and

e) Other similar pedestrian-oriented uses as approved by the Director of P&Z to meet the intent of providing active pedestrian-oriented uses are allowed. (P&Z)

45. No interim uses shall be approved which preclude the layout or function of the approved CDP. (P&Z)
46. For Block 9 (400 Calvert Avenue): the development level afforded to this block represents the floor area of the existing building which is assumed to be adaptively re-used. The floor area does not represent additional square footage afforded to this block. The uses for this block are limited to retail uses as defined herein. (P&Z)
47. Townhouses may be permitted only on Block D.(P&Z)

L. RETAIL OWNERSHIP & MANAGEMENT

48. To ensure a comprehensive and coordinated approach for the retail leasing and maker spaces for the site as depicted in The Oakville- Route 1 Plan shall be operated through a coordinated marketing/merchant's association, which shall be funded by the retail tenants and the property owner(s). The required retail shall be separate ownership and management of the remainder of uses on the site. The Applicant shall be responsible for the initial lease up of the retail for a period of thirty-six (36) months after the initial certificate of occupancy permit for Block A1 or B, whichever is later. The Applicant shall be responsible for selecting and controlling the location and mix of retail.

M. PARKING

49. If parking spaces are proposed to be shared among uses pursuant to Section 8-100 (A) (7) of the Zoning Ordinance to satisfy the required parking requirements of a use within the CDP area, a Shared Parking Plan that summarizes all proposed uses and parking requirements and indicates locations of parking to satisfy the requirements shall be submitted with the DSUP for the CDP area where the shared parking is requested. (P&Z)(T&ES)
 - a. The Applicant shall comply with the following parking requirements for the site: Unbundled Parking: All multifamily residential parking shall be unbundled (i.e., the cost to purchase or lease a parking space is separate from the cost to lease the residential unit). Unbundled parking for all other uses is encouraged and shall be explored as part of the Plan.
 - b. On-Street Parking: The CDP Plan shall include all proposed on-street parking spaces within the CDD Concept Plan. The location and amount of on-street parking spaces may be adjusted as part of the approval of a DSUP, with the exception of on-street parking along New Road "A"

MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan

which may be adjusted at time of the Phase 1 Infrastructure Final Site Plan.

- c. Priority Parking: Priority spaces for carpool/vanpool use shall be provided within all structured parking for the retail uses. (P&Z)(T&ES)
50. All on-street parking controls and restrictions within the project area shall be determined by the City. Any such controls and restrictions which the applicant desires shall be shown on the final site plan.. The City reserves the right to enforce parking meters on private streets containing public access easements. (P&Z)(T&ES)
51. The applicant will be required to dedicate space for two bikeshare stations.
- d. A 15-space bikeshare station to be located at the intersection of Route 1 & Swann Ave. adjacent to Block B. This station is approximately 6’x42’.
 - e. A 15-space bikeshare station at the intersection of Swann Ave & Park Road adjacent to Block D. This station is approximately 6’x42’.
(P&Z)(T&ES)
52. The applicant will be required to provide contributions for the City’s bikeshare initiative. The amount shall be determined and conditioned per block for each of the CDP’s subsequent DSUP applications. These contributions shall not be funded through the required TMP contributions. (P&Z)(T&ES)

N. TRANSPORTATION MANAGEMENT PLAN

53. Transportation Management Plans shall be required for each DSUP in the CDD in compliance with Article XI, Section 11-700 of the City’s Zoning Ordinance. A Transportation Management Plan is required to implement strategies to encourage residents and employees to take public transportation, walk, bike or share a ride, as opposed to being a sole occupant of a vehicle. (T&ES)

O. UTILITIES

54. Transformers/Utilities: All electrical transformers and associated utilities shall not be located within public right-of-way and may be located within the central portion of the blocks, alley(s), and/or screened to the satisfaction of the Director of P&Z or provided in underground vaults which shall comply with all applicable Dominion Virginia Power (DVP) standards. Ventilation grates shall not be located within public open space, public sidewalks or public streets - public right-of-way according to Dominion Virginia Power standards and to the satisfaction of the Directors of P&Z, T&ES and RP&CA. The final location of the transformers and/or vaults shall be approved as part of the preliminary DSUP review for each building/block. (RP&CA) (P&Z) (T&ES)
55. All new utilities serving the CDP, shall be placed underground or appropriately screened at the cost of Applicant. All utilities with the exception of those having a franchise agreement with the City shall be located outside the public right-of-

way; however, no transformers or switch gears shall be placed in the public right-of-way. (T&ES)

56. The location of new underground utilities shall not preclude the location of street trees. (P&Z) (T&ES)

P. ENVIRONMENTAL SUSTAINABILITY

57. Pursue LEED for Neighborhood Development certification or comparable certification for the neighborhood. (P&Z)(T&ES)
58. Green Building: Comply with the City’s Green Building Policy in effect at the time of DSUP approval. Applicants may use LEED, or comparable rating systems of Green Globes and EarthCraft as identified in the Green Building Policy and/or NGBS.
59. Rooftops shall be solar ready to be able to handle the equipment after installation. Pull-wire ready conduit shall be provided for a potential future roof PV system. Space shall be provided for solar related electric panel in or near a building electrical closet. Future installation of solar panels and associated infrastructure, beyond the conduit described in this condition, shall be at the sole discretion of the owner. Block A2 and D shall be excluded from this condition. (P&Z) (T&ES)
60. With each final site plan in the CDP, provide information on the following: how the applicant shall work with the City for recycling and/or reuse of the existing building materials as part of the demolition process, including leftover, unused, and/or discarded building materials. (T&ES)(P&Z)
61. Low Flow Fixtures: The development shall use low flow fixtures and shall be granted a reduction of 35% sanitary sewer flow from the SCAT Regulations and Memorandum to Industry 06-14 dated July 1, 2014 02-07 dated June 1, 2007 for initial developments. (T&ES)

Q. STORMWATER

62. Stormwater Management Master Plan (SWMMP): Each submission of the SWMMP shall include the following:
- f. The applicant shall complete a Virginia Runoff Reduction for the planned and actual phosphorous removals from each DSUP.. The total reductions for each phase must satisfy the regulatory requirements of the most current version of the Virginia Stormwater Management Regulations and the City’s Environmental Management Ordinance..
 - g. In compliance with the preliminary analysis presented in the Preliminary

MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan

Plan, the applicant shall comply with peak flow requirements and prepare a Stormwater Management Plan so that from the site, the post-development peak runoff rate from a two-year 24 hour storm and a ten-year 24 hour storm, considered individually, shall not exceed their respective predevelopment rates for all outfalls considered individually.

- h. Stormwater management and low impact design development (LID) facilities to be included as part of the preliminary DSUP. Each DSUP for Blocks A1, A2, B, C residential building, and D must include a minimum of one LID BMP. *(T&ES)
63. The City of Alexandria's stormwater management regulations regarding water quality are two-fold: 1) state phosphorus removal requirement and 2) Alexandria Water Quality Volume Default. Compliance with the state phosphorus reduction requirement does not relieve the applicant from the Alexandria Water Quality Default requirement. The Alexandria Water Quality Volume Default, as determined by the site's post-development impervious area shall be treated in a Best Management Practice (BMP) facility. Water quality requirements are twofold and compliance is demonstrated independently as follows:
- i. the state phosphorus reduction is calculated using the Virginia Runoff Reduction Method (VRRM) and the requirement for existing impervious area is 10% for land-disturbing activities less than one acre and 20% for land-disturbing activities equal to or greater than one acre; while new impervious surfaces must achieve "no net increase" by meeting a 0.41 lbs/ac/yr loading rate; and
 - j. the Alexandria water quality volume default (WQVD) is calculated as the first ½" of runoff from all of the site's impervious surfaces, and may be met by treating the first ½" of runoff for all of the site's impervious surfaces or paying a fee in lieu to the Water Quality Improvement Fund (WQIF) if approved.
- Each phase of development must demonstrate compliance with water quality requirements in a. and b. above. (T&ES)
64. Maintenance of all private BMPs and water quality practices treating private properties throughout CDD#14 shall be maintained by the Applicant or the Business Improvement District (BID) or comparable entity when established. (T&ES)
65. For all BMPs in private spaces a BMP Maintenance Agreement shall be approved prior to release of each final site plan. Should the private party become a member of the BID or other similar entity, proof of commitment shall be required.(T&ES)
66. Planted BMPs in the right of way shall be designed and installed per the City's Green Sidewalk Design Guidelines or the Virginia BMP Clearinghouse Standards and compliance demonstrated through the VRRM approach. Since the Tree Well design is predicated on the ½" stormwater runoff capture, and the VRRM is based on the 1" capture, only ½ of the total acreage for each tree well can

MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan

be input to the VRRM to demonstrate pollution reduction. (T&ES)

67. Provide BMP narrative and complete pre and post development drainage maps that include areas outside that contribute surface runoff from beyond project boundaries to include adequate topographic information, locations of existing and proposed storm drainage systems affected by the development, all proposed BMPs and a completed Virginia Runoff Reduction Method (VRMM) worksheet showing project compliance. The project must use hydrologic soil group “D” in the spreadsheet unless a soils report from a soil scientist or geotechnical engineer delineates onsite soils otherwise. (T&ES)
68. The stormwater Best Management Practices (BMPs) required for this project shall be constructed and installed under the direct supervision of the design professional or his designated representative. Prior to release of the performance bond, the design professional shall submit a written certification to the Director of T&ES that the BMPs are:
 - k. Constructed and installed as designed and in accordance with the approved Final Site Plan.
 - l. Clean and free of debris, soil, and litter by either having been installed or brought into service after the site was stabilized. **** (T&ES)
69. Surface-installed stormwater Best Management Practice (BMP) measures, i.e. Bio- Retention Filters, Vegetated Swales, etc. that are employed for this site, require installation of descriptive signage to the satisfaction of the Director of T&ES. (T&ES)
70. Submit two originals of the stormwater quality BMP and Stormwater Detention Facilities Maintenance Agreement, to include the BMP Schedule and Guidelines Addendum with the City to be reviewed as part of the Final #2 Plan. The agreement must be executed and recorded with the Land Records Division of Alexandria Circuit Court prior to approval of the final site plan.* (T&ES)
71. With the exception of planted BMPs in the right of way that will be transferred to the City for maintenance, the Applicant shall be responsible for maintaining stormwater Best Management Practices (BMPs), including any permeable pavement installed in the right of way, until activation of the homeowner’s association/master association or business improvement district (HOA/MA/BID), if applicable, or until sale to a private owner; or until bond release for public BMPs installed to treat the ROW. Prior to transferring maintenance responsibility for the BMPs to the HOA/MA/BID or owner, the Applicant shall execute a maintenance service contract with a qualified private contractor for a minimum of three years, and transfer the contract to the HOA/MA/BID or owner. A copy of the contract shall also be placed in the BMP Operation and Maintenance Manual. Prior to release of the performance bond, a copy of the maintenance contract shall be submitted to the City. ****(T&ES)

MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan

72. The Applicant shall be responsible for maintaining the right of way BMPs until acceptance by the City for maintenance. The City shall accept the structural elements of the BMP tree wells constructed and installed in the right of way for maintenance upon passing inspection and termination of the maintenance bond for public improvements. The City shall accept the plantings installed in BMP tree wells located in the right of way upon passing inspection and termination of the maintenance bond for landscaping. (T&ES)
73. If units will be sold as individual residential units and a homeowner's association (HOA) established the following two conditions shall apply:
- m. The Applicant shall furnish the Homeowner's Association with an Owner's Operation and Maintenance Manual for all Best Management Practices (BMPs) used on site. The manual shall include at a minimum: an explanation of the functions and operations of the BMP(s); drawings and diagrams of the BMP(s) and any supporting utilities; catalog cuts on maintenance requirements including any mechanical or electrical equipment; manufacturer contact names and phone numbers; a copy of the executed maintenance service contract; and a copy of the maintenance agreement with the City.
 - n. The Developer shall furnish each home purchaser with a brochure describing the stormwater BMP(s) installed on the site, outlining the responsibilities of the homeowners and the Homeowners Association (HOA) with respect to maintenance requirements. Upon activation of the HOA, the Developer shall furnish five copies of the brochure per unit to the HOA for distribution to subsequent homeowners.
74. Otherwise the following condition applies:
The Developer shall furnish the owners with an Owner's Operation and Maintenance Manual for all Best Management Practices (BMPs) on the project. The manual shall include at a minimum: an explanation of the functions and operations of the BMP(s); drawings and diagrams of the BMP(s) and any supporting utilities; catalog cuts on maintenance requirements including mechanical or electrical equipment; manufacturer contact names and phone numbers; a copy of the executed maintenance service contract; and a copy of the maintenance agreement with the City. (T&ES)
75. **NON_RESIDENTIAL – RETAIL/OFFICE CONDITION:** The Applicant/Owner shall be responsible for installing and maintaining stormwater Best Management Practices (BMPs). The Applicant/Owner shall execute a maintenance service contract with a qualified private contractor for a minimum of three years and develop an Owner's Operation and Maintenance Manual for all Best Management Practices (BMPs) on the project. The manual shall include at a minimum: an explanation of the functions and operations of the BMP(s); drawings and diagrams of the BMP(s) and any supporting utilities; catalog cuts on maintenance requirements including mechanical or electrical equipment; manufacturer contact names and phone numbers; a copy of the executed maintenance service contract;

MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan

and a copy of the maintenance agreement with the City. A copy of the contract shall also be placed in the BMP Operation and Maintenance Manual. Prior to release of the performance bond, a copy of the maintenance contract shall be submitted to the City. ****(T&ES)

76. Submit a copy of the Operation and Maintenance Manual to the T&ES Stormwater Management Section on digital media prior to release of the performance bond.
****(T&ES)
77. Construction inspection checklists and associated photographic documentation must be completed for each stormwater best management practice (BMP) and detention facility during construction. Prior to the release of the performance bond, the applicant must submit all documents required by The City of Alexandria As-Built Stormwater Requirements to the City including as built plans, CAD data, BMP certifications and completed construction inspection checklists. (T&ES)
78. Prior to release of the performance bond, the Applicant is required to submit a certification by a qualified professional to the satisfaction of the Director of T&ES that any existing stormwater management facilities adjacent to the project and associated conveyance systems were not adversely affected by construction operations. If maintenance of the facility or systems were required in order to make this certification, provide a description of the maintenance measures performed. ****(T&ES)

R. WASTEWATER

79. Low flow fixtures, water conservation measures or other facilities or infrastructure to be constructed as part of the DSUP to reduce or manage generation of municipal wastewater. (T&ES)
80. Since the development is conditioned to use low flow fixtures, the applicant is required to prepare a water management master plan as part of the first DSUP submission to reduce potable water use and use of low-flow fixtures. (T&ES)
81. The Preliminary Infrastructure Plan estimates a peak sewer flow of 1.07 MGD using peaking factor of 4.0 and a 35% reduction in sanitary flow for using low flow fixtures. This flow is equivalent to 73 townhome units, 796 multifamily units and 141,300 square feet of office/retail use. With each building DSUP submission, the applicant shall update the table in the infrastructure plan analysis showing the remaining sewer flow and the remaining residential units and retail/office space. If the applicant proposes increases to the number of residential units and square footages for office/retail uses (or comparable uses) than what is shown above additional analysis will be required and/or improvements may be required as part of the DSUP process.

82. The applicant must complete a sanitary sewer adequate outfall analysis, as part of the Infrastructure Plan submission, up to the Alexandria Renew Enterprises wastewater treatment plant in accordance with the requirements of City’s Memorandum to Industry 06-14 dated June 20, 2014. The adequate outfall analysis must include the municipal wastewater flow generated from the proposed development as well as under the build-out conditions. The applicant shall rehabilitate, replace and/or upsize, or construct collector sewers to provide conveyance capacity to serve the proposed development and the ultimate build-out conditions to the satisfaction of the Director of T&ES. If the City requests to provide additional conveyance capacity above and beyond the requirements to serve their development then the City shall provide credits from the sanitary sewer connection fee equal to the incremental cost incurred by the applicant. (T&ES)

S. WATERSHED, WETLANDS, & RPAs

83. Portions of the project site lies within Four Mile Run Watershed thus stormwater quantity controls in these drainage areas shall be designed to demonstrate that post development stormwater runoff does not exceed the existing runoff quantities for the 2-year, 10-year, and 100-year storm events, and must meet the channel protection and flood protection requirements in Section 13- 109(F). (T&ES)
84. The stormwater collection system is located within the Four Mile Run and Potomac River watershed. All on- site stormwater curb inlets and public curb inlets within 50 feet of the property line shall be duly marked using standard City markers, or to the satisfaction of the Director of T&ES. (T&ES)
85. Provide Environmental Site Assessment Notes that clearly delineate, map, describe and/or explain the following environmental features if located on site: individual components of the RPA) as well as the total geographic extent of the RPA, to include the appropriate buffer, intermittent streams and associated buffers; highly erodible and highly permeable soils; steep slopes greater than 15 percent in grade; known areas of contamination; springs, seeps or related features; and a listing of all wetlands permits required by law. (T&ES)

T. CONTAMINATED LAND

86. Indicate whether or not there is any known soil and groundwater contamination present on the plan. The applicant must submit supporting reports for associated environmental investigations or assessments performed to substantiate this determination with each Preliminary DSUP. (T&ES)
87. If environmental site assessments or investigations discover the presence of contamination on site, the final site plan shall not be released, and no construction activity shall take place until the following has been submitted and approved by the Director of T&ES:

MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan

- o. Submit a Site Characterization Report/Extent of Contamination Study detailing the location, applicable contaminants, and the estimated quantity of any contaminated soils and/or groundwater at or in the immediate vicinity of the site.
 - p. Submit a Risk Assessment indicating any risks associated with the contamination.
 - q. Submit a Remediation Plan detailing how any contaminated soils and/or groundwater will be dealt with, including plans to remediate utility corridors. Utility corridors in contaminated soil shall be over excavated by 2 feet and backfilled with “clean” soil. Include description of environmentally sound methods of off-site transport and disposal of contaminated soils and debris (including, but not limited to types of vehicles appropriate for handling specific materials and ensuring vehicle loads are covered).
 - r. Submit a Health and Safety Plan indicating measures to be taken during remediation and/or construction activities to minimize the potential risks to workers, the neighborhood, and the environment.
 - s. The applicant shall screen for PCBs as part of the site characterization if any of the past uses are within the identified high risk category sites for potential sources of residual PCBs, which includes the following SICs: 26&27 (Paper and Allied Products), 30 (Rubber and Misc. Plastics), 33 (Primary Metal Industries), 34 (Fabricated Metal Products), 37 (Transportation Equipment), 49 (Electrical, Gas, and Sanitary Services), 5093 (Scrap Metal Recycling), and 1221&1222 (Bituminous Coal).
88. Should any unanticipated contamination, underground storage tanks, drums or containers be encountered at the site during construction, the Applicant must immediately notify the City of Alexandria Department of Transportation and Environmental Services, Office of Environmental Quality. Should unanticipated conditions warrant, construction within the impacted area shall be stopped until the appropriate environmental reports identified in a. through f. above are submitted and approved at the discretion of the Director of Transportation and Environmental Services. This shall be included as a note on the final site plan. (T&ES)
89. If warranted by a Site Characterization report, design and install a vapor barrier and ventilation system for buildings and parking areas in order to prevent the migration or accumulation of methane or other gases, or conduct a study and provide a report signed by a professional engineer showing that such measures are not required to the satisfaction of Directors of T&ES and Code Administration. If warranted to be installed, the vapor barrier and ventilation system must include a passive ventilation system that can be converted to an active ventilation system if warranted. (T&ES)

U. DISCLOSURE REQUIREMENTS

90. The residential Condominium / Homeowners Association (HOA) documents, if

MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan

applicable, shall incorporate language that requires the following elements and other restrictions deemed necessary by the City Attorney to ensure that the trees proposed to be saved are retained including:

- t. Require property owners to sign a disclosure statement acknowledging the presence and required protection of the trees.
 - u. The trees to be protected as depicted on the approved site plan shall be required to be retained unless otherwise permitted to be removed by the City Arborist due to the health and safety of the tree.
 - v. Any proposal to remove a tree that is designated to be retained on the approved site plan for reasons other than health or safety shall require unanimous approval by the Homeowners Association and a site plan amendment. (P&Z)
91. All residential condominium association covenants shall be reviewed by the Director of P&Z and the City Attorney to ensure inclusion of all the conditions of any associated DSUPs prior to applying for the first certificate of occupancy permit for the project. The association covenants shall include the conditions listed below, which shall be clearly expressed in a separate section of the covenants. The language shall establish and clearly explain that these conditions cannot be changed except by an amendment to this development special use permit approved by City Council.
- w. The principal use of the garage and parking spaces shall be for passenger vehicle parking only; storage which interferes with the use of a parking space for a motor vehicle is not permitted.
 - x. All landscaping and open space areas within the development shall be maintained by the Homeowners' and/or Condominium Owners' Association.
 - y. Exterior building improvements or changes by future residents shall require the approval of the City Council, except for minor changes to the facades, as determined by the Director of P&Z.
 - z. The applicant shall develop a noise control by-law aimed at controlling noise levels in the proposed development and resolving noise issues between neighboring occupants, and disclose this by-law to all involved at the time of sale or lease agreement.
 - aa. Stormwater facility BMPs must be inspected and adequately maintained as designed to ensure proper functioning.
 - bb. Where applicable, ground level open space on Blocks C and D with public access easements shall be fully open to the public following the hours and guidelines agreed upon between the Applicant and the City.
 - cc. The specific language of the disclosure statement to be utilized shall be provided to the City for approval prior to release of the first certificate of occupancy permit.

***(P&Z) (T&ES)

92. If environmental site assessments or investigations discover the presence of onsite

MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan

contamination, the applicant or its agent shall furnish each prospective buyer with a statement disclosing the prior history of the Oakville Triangle site, including previous environmental conditions and on-going remediation measures. Disclosures shall be made to the satisfaction of the Director of Transportation and Environmental Services. (T&ES)

93. Notify prospective buyers, in its marketing materials and homeowner documents, that New Road “A” is a private street with public access easement and shall not be maintained by the City of Alexandria; and that the sanitary and storm sewers located within the site are private and shall be maintained privately. (T&ES)

V. CODE REQUIREMENTS

94. Provide all calculations and computations related to demonstration of meeting the state phosphorus requirements and the Alexandria WQVD. (T&ES)
95. Provide typical details of all stormwater BMP facilities, to include the Tree Well BMPs. (T&ES)
96. A complete Stormwater Pollution Prevention Plan (SWPPP) Book must be provided with the Final #1 submittal for each individual DSUP. For each individual DSUP, the stormwater management (SWM) plan and the erosion and sediment control (E&SC) plan must be approved prior to the SWPPP being deemed approved and processed to receive coverage under the VPDES Construction General Permit.
97. All land-disturbing activities commencing under each plan must seek and secure coverage under the VPDES Construction General Permit. (T&ES)
98. A Notice of Termination to discontinue discharges under the VPDES Construction General Permit must be submitted and approved for the applicable portion of the CDD. (T&ES)
99. Include a solid waste management plan incorporating reduction, reuse, recycling, recovery (composting, etc.) and proper disposal as priorities per the requirements of Article H to Title 5 (Ordinance No 4438) of the Alexandria City Code. (T&ES)

Per the requirements of the City of Alexandria Zoning Ordinance (AZO) Article XIII, Environmental Management Ordinance, the applicant shall comply with the water quality and quantity requirements and provide channel protection and flood protection in accordance with these requirements. (T&ES)

W. CONTRIBUTION(S)

100. The Applicant(s) shall make a monetary contribution to a dedicated Oakville –

MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan

Route 1 Implementation Fund to be established by the City to account for the developer contributions as required herein and as may be necessary for other properties within CDD# 24 (hereinafter “Developer Contributions”). Developer contributions shall be provided for all New Development within the CDD Concept Plan area as delineated on the Plan dated 9/15/2020 and revised on 11/03/2020; prepared by Christopher Consultants, consistent with the intent of the Oakville - Route 1 Small Area Plan and as required herein. The Developer Contributions shall be paid prior to the release of the first Certificate of Occupancy for Block A1 or B, unless otherwise specified herein. (P&Z)

101. The Developer Contribution as required herein are as of January 1, 2016, and shall escalate annually hereafter on January 1 of each year hereafter starting on January 1, 2017 in accordance with increases in prior years in the CPI-U, which shall be the Consumer Price Index for all urban consumers (CPI-U), 1982-1984=100 (not seasonally adjusted) as reported by the United States Department of Labor, Bureau of Labor Statistics. The resulting adjusted Developer Contribution per square foot of New Development shall be in effect for that calendar year. Interest earned on any funds deposited by any Applicant(s) will be invested by the City as per Code of Virginia investment laws for short-term investments. Interest accrued shall remain in the fund to be utilized to implement the Oakville – Route 1 Small Area Plan improvements depicted on Tables 3 and 4 of the REZ#2015-0004 and MPA#2015-0007 case or as amended by this approval and for ultimate inclusion in the Oakville-Route 1 Small Area Plan.

The developer contributions shall be used to implement the Public Benefits as generally depicted within Tables 3 and 4 of the REZ#2015-0004 and MPA#2015-0007 case or as amended by this approval and for ultimate inclusion in the Oakville-Route 1 Small Area Plan.

All other improvements, as required by the CDD zoning, CDD Concept Plan, these Conditions, the Oakville – Route 1 Small Area Plan, and as required as part of the DSUP process, shall be the sole responsibility of the Applicant(s). (P&Z)

102. The Applicant(s) will provide an inception-to-date update on the amount of Developer Contributions received and status of improvements as required herein, as part of the preliminary DSUP process. (P&Z)

X. PUBLIC ART

103. The Applicant shall provide public art in the CDP area consistent with the City’s approved Public Art Policy approved in October 2012. The Applicant shall work with staff to determine if the Applicant will provide public art on site or provide an equivalent contribution to be used for public art within the CDP area.. If public art will be the selected option the Applicant shall provide the following information with each Final Site Plan submittal:
- dd. Outline goals and approach to public art within the site;

MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan

- ee. Identify locations, such as gateways, corridors, historically significant sites, highly visible sites, open space, and others, for opportunities for public art;
 - ff. Describe forms of public art desired, such as stand-alone pieces or integration into the architecture, landscape, open space and/or public streetscape;
 - gg. The process for the selection of the artist or artwork;
 - hh. Maintenance considerations.
 - ii. On a per DSUP basis, the applicant's Public Art Contribution shall be set at the 2020 public art rate of \$0.30 per gross square foot of development up to a maximum amount of \$75,000, or equivalent contribution to be used for public art within the CDP area for each DSUP building. For clarity, it is anticipated that there will be five separate DSUPs subject to this condition within the CDP area: Blocks A1, A2, B, C and D.
(P&Z)(Arts)(RP&CA)
104. With the first Final Site Plan submission for either Block A1 or B (whichever is earlier), the applicant may submit a draft of a consolidated and coordinated public art plan outlining locations for public art throughout the CDP. The public art plan shall be approved by RPCA prior to the release of the First Final Site Plan.
- a. All public art locations will be on private property and accessible to the public.
 - b. The Applicant may install onsite public art either as separate pieces on a block by block basis or install such art in identified site within the CDP area. For instance, all onsite Public Art could be installed in an area designated as a public open space. If a public open space is determined to be the location for consolidated public art, funding for installation of the public art shall be provided to coordinate the public art with the open space design.
 - c. The public art plan will identify timing for each piece of public art.
(P&Z)(Arts)(RP&CA)

Y. COMMUNITY FACILITIES

105. A community meeting room or resident amenity area no less than 1,400 sq. ft. within a building in Block A1 or B in the CDP area may be used as a 'community facility' to facilitate community meetings, subject to terms of this condition. The space shall be made available at no cost to local community and non-profit organizations in addition to Alexandria City government agencies at least 12 times per year for a total annual minimum of 48 hours to the satisfaction of the Director of P&Z. The space shall be made available during the hours of 8:00 a.m. to 10:00 p.m. on weekdays and 8:00 a.m. to 6:00 p.m. on Saturdays on a space-available basis, upon request. As part of this requirement, this space may also be made available to uses complying with section 5-502(H) (i.e. uses which foster art, history and cultural awareness), or a library kiosk, and open for the enjoyment of the larger community. The condominium association or building management has

MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan

the authority to require refundable security deposits or institute alternative requirements for non-City government agencies, with the exception of charging user fees, to protect against damage and/or to cover clean- up costs. (P&Z)

106. An outdoor unconditioned community area/gathering space shall be integrated into the design of the Block C open space. At a minimum, the area shall be designed to include a shade element and seating and wired for electrical power. The City reserves the right to program ten (10) events/year for the City and/or community. The final design of the space shall be coordinated with the City to the satisfaction of the Directors of P&Z and RP&CA. (P&Z) (RP&CA)

Z. AFFORDABLE HOUSING

107. The Applicant shall provide a total of 65 affordable set-aside rental units comprising 63 studios and 2 two-bedroom units in accordance with the approved Affordable Housing Plan, dated September 9, 2020. (Housing)
108. Rents payable for the set-aside units shall not exceed the maximum rents allowed under the Federal Low Income Housing Tax Credit program for households with incomes at or below 60% of the Washington D.C. Metropolitan Area Median Family Income for a period of 35 years from the date of initial occupancy of each affordable unit. The applicant shall re-certify the incomes of resident households annually. (Housing)
109. Once an income-eligible household moves into a unit, that unit will be considered an affordable unit until the household's income increases to more than 140% of the then- current income limit. At that time, the over-income household shall be allowed to remain, but the next available unit of comparable size (i.e., with the same number of bedrooms, den space, and/or approximate square footage) must be made available to a qualified household. Once the comparable unit is rented, the rent of the over-income unit may then be increased to market rate in accordance with any lease restrictions. If a comparable unit within the building does not exist, the over-income tenant must be given notice and required to vacate the unit and replaced with an income-eligible household. (Housing)
110. Households receiving Housing Choice Voucher assistance will not be denied admission on the basis of receiving such assistance. A household will be considered income qualified if the amount of rent it can pay based on income, together with the voucher payment, is sufficient to cover the applicable rent. (Housing)
111. The applicant agrees that residents of set-aside units shall have access to all amenities offered on the entire Development. (Housing)
112. The set-aside units shall be of the same size and floor plan and with the same finishes as other similar units in the Development. Concentrations of set-aside

MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan

units within each building will be avoided. (Housing)

113. The set-aside units shall be distributed across the residential rental buildings in the project on Blocks A, B, and C in accordance with the approved Affordable Housing Plan (Housing)
114. If the market rents are less than anticipated, the tax credit rents will continue to be used as the affordable rents; however, in the event the differential between the market rents and the affordable rents falls below \$150, the affordable rents shall be reduced to maintain a differential of at least \$150 at all times. (Housing)
115. Residents of the set-aside units shall have access to leased parking at the same rates and terms as offered to other residents. (Housing)
116. The applicant shall provide the City with access to the necessary records and information to enable annual monitoring for compliance with the above conditions for the 35-year affordability period. (Housing)
117. Amendments to the approved Affordable Housing Plan impacting the phasing and/or number, type, size (number of bedrooms), and level and length of affordability of the affordable units must be submitted to the Alexandria Housing Affordability Advisory Committee for consideration, and require final approval from the City Manager. (Housing)

AA. ARCHAEOLOGY

118. Hire a professional consultant to work with staff and the landscape designers to incorporate and interpret elements of the historical character and archaeological findings into the design of the open space and to prepare interpretive elements, which shall be erected as part of the development project. The site plan shall indicate themes and locations of interpretive elements. Prior to release of the final site plan, the consultant shall provide text and graphics for the signage subject to approval by the Office of Historic Alexandria/Alexandria Archaeology and the Directors of P&Z and/or RP&CA.* (Arch)(P&Z)(RP&CA)
119. Hire an archaeological consultant to complete a Documentary Study and an Archaeological Evaluation. If significant resources are discovered, the consultant shall complete a Resource Management Plan, as outlined in the City of Alexandria Archaeological Standards. Preservation measures presented in the Resource Management Plan, as approved by the City Archaeologist, will be implemented. (Archaeology)
120. The Final Site Plan, Grading Plan, or any other permits involving ground disturbing activities (such as coring, grading, filling, vegetation removal, undergrounding utilities, pile driving, landscaping and other excavations as defined in Section 2-151 of the Zoning Ordinance) shall not be released until the City archaeologist confirms that all archaeological fieldwork has been completed

MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan

or that an approved Resource Management Plan is in place to recover significant resources in concert with construction activities. * (Archaeology)

121. The final Certificate of Occupancy for the later to occur between Block A1 or B, shall not be issued for this property until interpretive elements have been constructed, interpretive markers have been erected, and the final archaeological report has been received and approved by the City Archaeologist.*** (Archaeology)
122. Call Alexandria Archaeology immediately (703-746-4399) if any buried structural remains (wall foundations, wells, privies, cisterns, etc.) or concentrations of artifacts are discovered during development. Work must cease in the area of the discovery until a City archaeologist comes to the site and records the finds. The language noted above shall be included on all final site plan sheets involving any ground disturbing activities. (Archaeology)
123. The applicant shall not allow any metal detection and/or artifact collection to be conducted on the property, unless authorized by Alexandria Archaeology. Failure to comply shall result in project delays. The language noted above shall be included on all final site plan sheets involving any ground disturbing activities. (Archaeology)

Archaeology Findings

- F-1. The property known as the Oakville Triangle is located on acreage that once belonged to the Swann family in the nineteenth century. William T. Swann and his wife Frances Alexander Swann established a small plantation which they called Oakville (reputedly a reference to the mature oak trees that dotted the plantation) in the early nineteenth century, prior to William's death in 1823. Frances died at Oakville in 1856 and the property passed to her son Thomas Swann and his wife Helen Chapman Swann. Not long after the Civil War, the Swanns relocated to a new home at Mount Auburn (overlooking what is now the 3000 block of Mount Vernon Avenue), but retained ownership of the 15 acre Oakville property. Remnants of the plantation continued to stand into the 1920s, even after a railroad spur was built across the property around 1900.
- F-2. According to historic maps and aerial photographs, the core area of Oakville plantation was located between Calvert Ave. and Swann Ave. where warehouse facilities now stand. Given the likelihood that the current buildings were built on slab foundations, subsurface archaeological evidence of Oakville may be preserved on the property.
- F-3. If this project is a federal undertaking or involves the use of any federal funding, the applicant shall comply with federal preservation laws, in particular Section 106 of the National Historic Preservation Act of 1966. The applicant will coordinate with the Virginia Department of Historic Resources and the federal agency involved in the project, as well as with Alexandria Archaeology.

Code

124. All required archaeological preservation measures shall be completed in compliance with Section 11-411 of the Zoning Ordinance. (Archaeology)

PWS Comments:

125. City standard pavement materials, shall be constructed to the City of Alexandria's standards and specifications. Standard pavement materials are defined as minimum of VDOT A-3 concrete (curb and gutter, and sidewalk), VDOT A-4 concrete (bus pads, dumpster pads) and asphalt pavement section made up of SM-9.5A (surface), BM-25.0A (base) and 21B (subbase). Alternative materials, such as pavers and bricks (with the exception of those approved in the City's standard specifications), or any other material will not be accepted within the right-of-way. Material thickness shall be in accordance with the pavement standard as defined in note C-17 of this document, VDOT Standards and Specifications and the City of Alexandria Standards and Specifications. Concrete and asphalt materials that are used within the public right-of-way shall have no artificial and/or manipulated color other than their natural occurring color (grey/white and black), and only excludes striping and other pre-approved areas by the Director of Transportation of Environmental Services.
126. All bracing, temporary forms and structures shall be removed prior to backfill and compaction around the foundations and utilities. Upon removing temporary construction measures, backfill material shall be compacted to no less than 95% compaction based on soils based on Unified (ASTM D 2488) soil classification and the Atterberg tests (liquid and plastic limits) shall be conducted. Independent geotechnical engineer shall perform and verify these tests, and provide results within ten (10) calendar days after completion to the City. If the soil compaction and/or temporary bracing is not found to be within the requirements, the Owner shall be in compliance prior final acceptance.
127. Prior to and after construction, the Owner shall use closed circuit television (CCTV) to inspect all storm and sanitary sewer manholes and pipes that are being connected to. Storm lines shall be inspected from the point of connection to the point of outfall / daylighting to a natural channel. Sanitary sewers shall be inspected from the point of connection to a trunk main. Based on the inspection of the post construction, the City may require additional cleaning and/or correction to the systems based on construction activities. All records for CCTV shall be provided within ten (10) calendar days of the inspection to the Owner.
128. Public and/or Private utilities that are service the building(s) or other improvements, shall not be placed under any BMP structure (pervious pavement, rain gardens, tree wells, etc.). Utilities shall have a minimum of ten (10) feet horizontally on each side of the BMP for clearance, to the extent feasible (PC). Additional clearance may be required between utilities and BMP's for future

maintenance requirements which shall be approved by the respected utility provider and for City owned utilities by the Director of Transportation and Environmental Services.

AlexRenew Comments:

129. Applicant to include the following notes on the DSUP plans and the plans issued for construction:
- a. Contractor shall ensure all discharges are in accordance with City of Alexandria Code Title 5, Chapter 6, Article B.
 - b. Dewatering and other construction related discharge limits to the sewer system are regulated by AlexRenew Pretreatment. Contractor is required to contact AlexRenew's Pretreatment Coordinator at 703-549-3381 x2020.

BB. FIRE DEPARTMENT

130. The applicant shall provide a separate Fire Service Plan which illustrates **where applicable**: a) emergency ingress/egress routes to the site; b) one fire department connection (FDC) for buildings under 5 stories or 55 feet or two sufficiently remote FDC's for buildings over 5 stories or 55 feet; c) all existing and proposed fire hydrants where fire hydrants are located between forty (40) and one hundred (100) feet of each required FDC; d) on site fire hydrants spaced with a maximum distance of three hundred (300) feet between hydrants and the most remote point of vehicular access on site; e) emergency vehicle easements (EVE) around the building with a minimum width of twenty-two (22) feet; f) the location and size of the separate fire line(s) for the building fire service connection and fire hydrants.

- a) Access routes are shown on plans.
- b) Due to height of buildings, two fire department connections are required on each building.
- c) Hydrants that are shown on plans are within required distance but additional hydrants will be necessary once second FDC location is determined.
- d) Additional hydrants will be required to meet this specification. Location of second FDC should be considered when placing the additional hydrants.
- e) All roads appear to be public streets and there do not appear to be and fire lanes.
- f) No fire service lines have been shown on plans. (AFD)

CC. GIS

Findings

131. Street level addressing and Road Naming: Currently a new road is identified as “Park Road”. This name is an existing road within the City of Alexandria. All road names need to be approved before assignment. Refer to Planning and Zoning, GIS Division for all road names and street level addressing. Moe Abu-Rabi, 703-746-3823. (GIS)
132. Unit Numbering Plan: The applicant is proposing a mixed use, with residential project, where a Unit Numbering Plan for the multifamily residential portion is needed. The units within should apply the principal of: first floor unit doors receiving "100" series numbering, second floor units receiving "200" series numbering, etc., and not to include alphas or fractions. Units such as stacked multistory townhomes, where the above unit is accessed on the 3rd story, and with no second level doorway, shall receive “300” series numbering. This will help GIS in determining the appropriate unit numbering scheme for the project, as well as satisfy police and fire concerns. (GIS)

DD. Transportation & Environmental Services

Findings

133. Parking for the residential and commercial uses shall be consistent with the requirements of the Zoning Ordinance in effect at the time of approval by City Council and/or Planning Commission. Parking for the medical care facilities shall be no greater than 3 spaces per 1,000 square feet. (P&Z) (T&ES)
134. The applicant will be required to provide contributions to the City for the purchase and installation of parking meters for metered blocks with each individual Block DSUP. (T&ES)

**MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan**

EXHIBITS

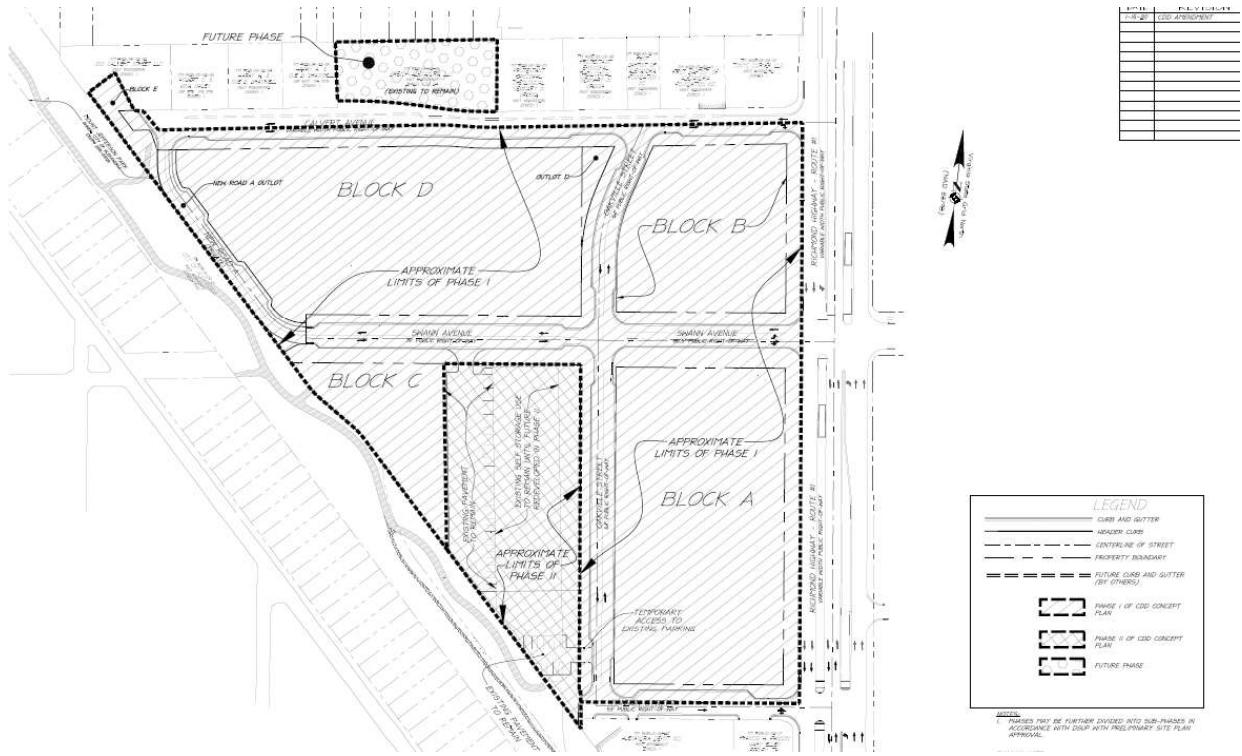
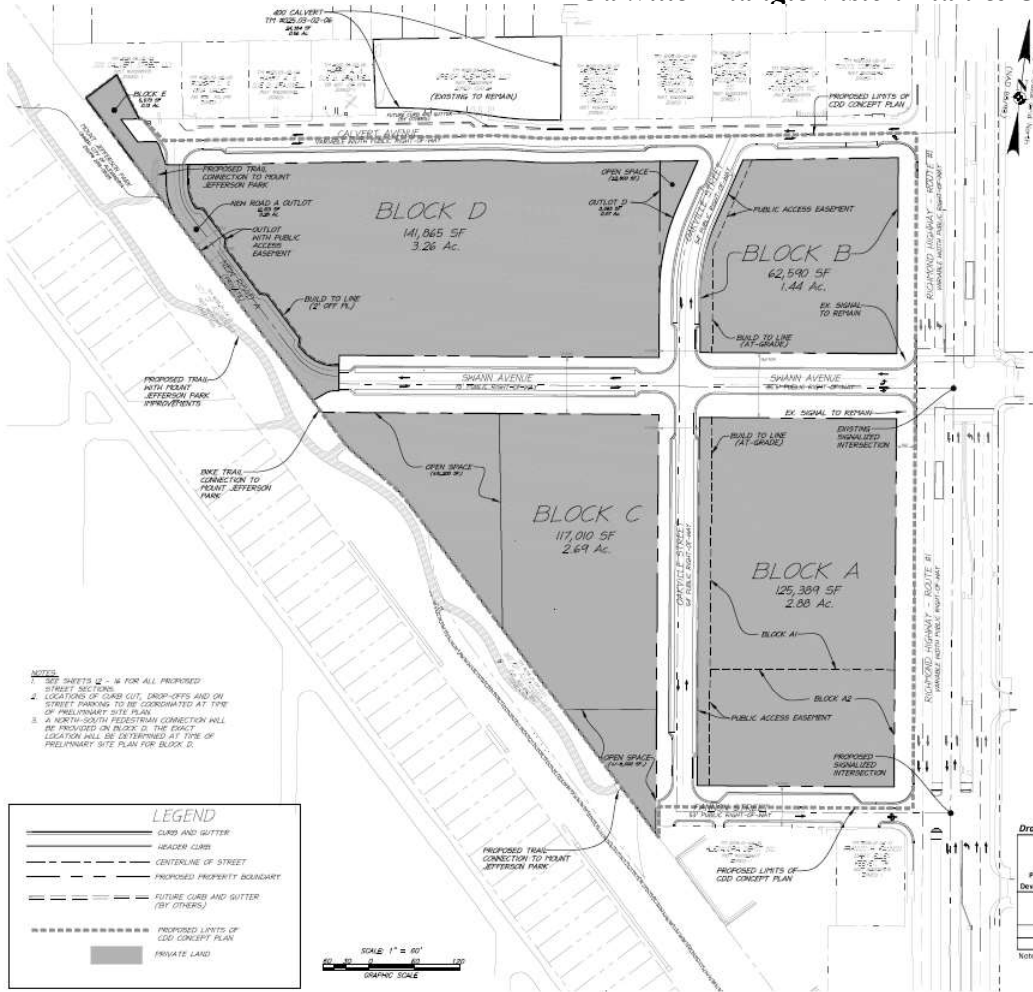


Exhibit 1. Preliminary Phasing Plan (from C02.1 dated 9/15/2020 and revised 11/03/2020)

MPA2020-00003, CDD2020-00003, ZTA2020-00006 Oakville Triangle Vision Plan & CDD Concept Design Plan



CONCEPTUAL OAKVILLE DEVELOPMENT SUMMARY TABLE

Block	Use	Proposed Net to Exceed Gross Floor Area (GFA) (%)
A	Residential (MF)	41,000
	Residential (MF)	130,000
	Residential (MF) for City	111,000
	Service / Retail / Loading (R)	25,000
	Block A Subtotal GFA	297,000
B	Service / Retail / Loading (R)	25,000
	Block B Subtotal GFA	31,000
	Choice Goods Storage	31,000
	Residential (MF)	29,000
	Block B Total	116,000
C	Residential (MF)	0
	Residential (MF)	200,000
	Townhomes	0
	Service / Retail / Loading (R)	25,000
	Block C Subtotal GFA	225,000
D	Choice Goods Storage	31,000
	Residential (MF)	0
	Block D Subtotal GFA	0
	Choice Goods Storage	0
	Block D Total	31,000
E	Residential (MF)	0
	Residential (MF)	200,000
	Townhomes	0
	Service / Retail / Loading (R)	0
	Block E Subtotal GFA	200,000
Total GFA (%)		1,830,000

Block	GFA (sq ft)	% of Total
A	297,000	16.2%
B	116,000	6.3%
C	225,000	12.3%
D	31,000	1.7%
E	200,000	11.0%
Total	1,830,000	100%

- Notes:
- GFA may be transferred from one block to another with an application for a GSA; provided, the amount of GFA transferred shall not cause GFA in any block to increase or decrease by more than 20% and in accordance with the CDD Staff Recommendations.
 - Open Floor Area (OFA) - as defined in Oakville Triangle & Douie I Corridor Vision Plan and Urban Design Standards and Guidelines.
 - May include traditional free sample lots and/or neighborhood units.
 - Service / Retail / Loading accounts for above ground transformer space, loading/truck docks and associated storage space, and garage extract / mechanical space.
 - Residential - as defined in Oakville Triangle & Douie I Corridor Vision Plan and Urban Design Standards and Guidelines.
 - Existing off-storage buildings to remain in as-is in Phase 1.

Draft Affordable Housing Phasing Plan (By Units)

Phase of Development	Use	Total # Units ¹	% of Total Residential GFA by Phase	% of Affordable Units Provided in Each Phase	Total Affordable Units to be Provided by Phase ²
1	Block A: Residential - Multifamily	300	36%	74%	48
1	Block B: Residential - Multifamily	260	28%	10%	0
1	Block D: Residential - Townhomes	90	10%	10%	17
2	Block C: Residential - Multifamily	240	26%	16%	0
Total		900	100%	100%	65

- Notes:
- All affordable units to be provided in Multifamily Residential Buildings.
 - Required units at time of Phase 2 development may be provided via conversion of existing market units located in Blocks A or B.
 - Total number of units provided in each building as shown on this chart is as expected at time of CDD plan submission. Actual number may vary.

Exhibit 2. Conceptual Design Plan (from C02.0 dated 9/15/2020 and revised on 11/03/2020)

VII. ATTACHMENTS

1. Master Plan Resolution MPA#2020-00003
2. Affordable Housing Plan Submitted 9.9.2020
3. List of Master Plan Changes
4. Oakville Triangle & Route 1 Corridor Vision Plan 2020 Markup

ATTACHMENT #1

RESOLUTION NO. **MPA 2020-00003**

WHEREAS, under the Provisions of Section 9.05 of the City Charter, the Planning Commission may adopt amendments to the Master Plan of the City of Alexandria and submit to the City Council such revisions in said plans as changing conditions may make necessary; and

WHEREAS, the proposed amendments will amend the **Potomac West Small Area Plan** chapter of the 1992 Master Plan;

WHEREAS, the Department of Planning and Zoning has analyzed the proposed revisions and presented its recommendations to the Planning Commission; and

WHEREAS, a duly advertised public hearing on the proposed amendment was held on **December 1, 2020** with all public testimony and written comment considered; and

WHEREAS, the Planning Commission finds that:

1. The proposed amendments are necessary and desirable to guide and accomplish the coordinated, adjusted and harmonious development of the **Potomac West Small Area Plan** section of the City; and
2. The proposed amendments are generally consistent with the overall goals and objectives of the 1992 Master Plan and with the specific goals and objectives set forth in the **Potomac West Small Area Plan** section of the 1992 Master Plan; and
3. The proposed amendments show the Planning Commission's long-range recommendations for the general development of the **Potomac West Small Area Plan**; and
4. Based on the foregoing findings and all other facts and circumstances of which the Planning Commission may properly take notice in making and adopting a master plan for the City of Alexandria, adoption of the amendments to the **Potomac West Small Area Plan** chapter of 1992 Master Plan will, in accordance with present and probably future needs and resources, best promote the health, safety, morals, order, convenience, prosperity and general welfare of the residents of the City;

NOW, THEREFORE, BE IT RESOLVED by the Planning Commission of the City of Alexandria that:

MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan Amendments

1. The following amendments to the **Potomac West Small Area Plan** through the **Oakville Triangle & Route 1 West Corridor Vision & Urban Design Guidelines Overlay** are hereby adopted in their entirety amending the Potomac West Small Area Plan chapter of the 1992 Master Plan of the City of Alexandria, Virginia in accordance with Section 9.05 of the Charter of the City of Alexandria, Virginia:

a. Updates to text as provided below. *Deleted text is shown with a ~~strikethrough~~. Revised or additional text is underlined.*

P.9:

- Amend the vision statement, as follows: The Plan envisions a future Oakville Triangle/Route 1 Corridor that is compatible with the fabric of existing neighborhoods, features an enhanced Mount Jefferson Park/Trail, and a blend of new and existing open spaces and land uses unified by an attractive urban streetscape. High quality architecture and urban design celebrate the area’s industrial heritage, and new development includes a diversity of residential, office, hotel, medical care facility, and regional and neighborhood-serving retail uses, as well as an emerging “maker” economy. New development capitalizes on nearby transit by concentrating height at transit stops and in close proximity to ~~the future~~ Potomac Yard Metrorail Station, while ensuring compatibility with surrounding neighborhoods. As an inclusive neighborhood, the area provides housing options affordable to a range of incomes and welcoming to different household types. Residents, workers, and visitors walk and bike within the Plan area and to adjoining neighborhoods.

P.11:

- Amend the second column of text, as follows: New development will be required to provide usable ground level public open spaces, including, but not limited to, ~~three new public open spaces~~ a new .7 acre central public open space within Oakville Triangle to build on and visually connect to the Mount Jefferson Park, as well as expansion of the existing Ruby Tucker Park to create a larger public open space in the northern portion of the Plan area. A ~~new hardscape plaza~~ new central open space in Oakville Triangle will provide a public gathering space for daily enjoyment and special events for residents and visitors, new and old. In addition, larger projects will provide rooftop amenity space to ensure adequate opportunity for new residents’ access to the outdoors, designed to be compatible with and to minimize light and noise impacts on the adjoining neighborhoods.

P.14:

- Amend Recommendation 1.13, as follows: A pedestrian and bike connection between Swann Avenue and Stewart Avenue to provide connectivity between the neighborhoods and the future Potomac Yard Metrorail station. ~~The bike lane within the central urban plaza within the Oakville Triangle site will be designed as a shared space.~~
- Amend Recommendation 1.17, as follows: ~~An additional signalized pedestrian crossing across Route 1 between East Custis Avenue and East Glebe Road to improve pedestrian~~

MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan Amendments

~~access between Oakville Triangle and Del Ray/Lynhaven and the future Potomac Yard Metrorail station. An additional signalized crossing across Route 1 at Fannon Street, including pedestrian and intersection improvements, to improve pedestrian access between Oakville Triangle and Del Ray/Lynhaven and the Potomac Yard Metrorail station and provide efficient access to the medical care facility.~~

P.17:

- Amend Recommendation 1.13, as follows: A pedestrian and bike connection between Swann Avenue and Stewart Avenue to provide connectivity between the neighborhoods and the future Potomac Yard Metrorail station. ~~The bike lane within the central urban plaza within the Oakville Triangle site will be designed as a shared space.~~

P.18:

- Amend Recommendation 1.17, as follows: ~~An additional signalized pedestrian crossing across Route 1 between East Custis Avenue and East Glebe Road to improve pedestrian access between Oakville Triangle and Del Ray/Lynhaven and the future Potomac Yard Metrorail station.~~ An additional signalized crossing across Route 1 at Fannon Street, including pedestrian and intersection improvements, to improve pedestrian access between Oakville Triangle and Del Ray/Lynhaven and the Potomac Yard Metrorail station and provide efficient access to the medical care facility.

P.19:

- Add as a new bullet: Installation of traffic signal, pedestrian, and intersection improvements at Fannon Street and Route 1.

P.22:

- Amend Note 1, as follows: 1. Gross Floor Area (GFA) is defined as the sum of all gross horizontal areas under a roof or roofs. These areas are measured from the exterior faces of walls or from the centerline of party walls. Elevator and stair bulkheads, multi-story atriums and similar volumetric construction, not involving floor space are excluded. Below grade parking structure is excluded. See notes K and L on page 23 for blocks 4-7.
- Amend Note 4, as follows: 4. For lots identified in the Plan with maker spaces on all four street-facing facades, if not feasible to provide maker spaces on all four sides, a minimum of 3 street-facing facades shall be used to provide maker spaces.
- Amend Note 10, as follows: 10. Conversion rate from GFA sf to dwelling unit = 940 sf The final number of units to be determined at time of DSUP.
- Add Note 12, as follows: 12. Hotel use may be permitted within the Plan area. The final location shall be determined during the DSUP process.

*MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan Amendments*

P.23:

- Amend Note a, as follows: a. Based on heights in the approved Oakville Triangle & Route 1 Corridor Vision Plan, as amended October 2015. If greater heights are allowed, maximums may be increased to the approval of PC & CC.
- Amend Note m, as follows: m. Development (sf) based on Preliminary CDD Concept Plan ~~CDD2014-0002 submission 10/29/15~~ as amended.
- Amend Note q, as follows: q. ~~Townhouses shall be liner units or as generally depicted per the approved CDD Concept Plan CDD2014-0002~~ Final design and configuration of townhouses will be determined during the DSUP process.
- Amend Note r, as follows: r. As per all blocks, required retail identified in the plan at ground floor locations is required. Non-ground floor retail may be converted to an alternate use through the DSUP process.

P.25:

- Amend the first column header, as follows: PHASE 1* - OPERATIONAL PRIOR TO 800,000 SQ. FT. (NET WITHOUT PARKING)
- Amend the first note in the first column, as follows: 1 - Signal modifications at Route 1 and E. Reed Ave* (with North Potomac Yard first building)
- Add a note to the first column, as follows: 8 - New signalized intersection improvements and pedestrian crossings at Route 1/Fannon Street
- Amend the fifth note in the second column, as follows: 8 - Pedestrian crossing across Route 1 ~~(at Fannon St. or Raymond Ave)~~
- Delete the first note in the third column, as follows: 9 - ~~Improvements at Route 1/E. Glebe (Phase 2)~~

P.26:

- Amend the last row of text in the chart of Non-Transportation Improvements, as follows: Open Space: Consideration in the Plan's phasing ensures that sufficient, consolidated, high quality open space is provided based on planned development. On-site open space is required for the Oakville site consistent with the Plan. Significant on-site open spaces ~~(e.g. Swann Plaza)~~ are required to be provided with adjacent development blocks.

P.27:

- Amend the first row of text in the chart of Non-Transportation Improvements, as updated. Timing for initiation and completion of park improvements is specified in the CDP for the Oakville Triangle site. Improvements shall be fully constructed ~~and accepted by the City~~ in accordance with CDD conditions prior to issuance of first Certificate of Occupancy for any building in the CDP area, or within a maximum 24 months of issuance of permit to demolish any building or structure in or adjacent to Park (whichever is earlier).

*MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan Amendments*

P.32:

- Amend recommendation 2.5, as follows: Provide a ~~central public urban plaza~~ new central public open space within Oakville Triangle that includes usable amenities that help to meet the recreational needs of new residents.
- Amend recommendation 2.6, as follows: Provide ~~five~~ four new ground level public green spaces adjacent to Mount Jefferson Park.

P.37:

- Amend the column of text, as follows: Establishing the street grid will visually unify this approximately 15-acre district. The defining elements of this neighborhood will be the adjoining Mount Jefferson Park frontage and the centrally located ~~public urban plaza~~ ± 31,200 sq. ft. public open space. The building height and setback of the buildings have been designed to step down to the adjoining Mount Jefferson Park and single-family homes to the west. The centrally located ~~±21,000 sq. ft.~~ public open space will need to be configured to accommodate events, ~~and programming, and social gatherings.~~ Swann Avenue is intended to provide retail shopping through the provision of a significant amount of ground floor retail. Calvert Avenue is intended to be an area where some of the existing neighborhood-serving or “maker uses” could be located. Calvert Avenue is also intended to be more “industrial” in design and character. Park Road is intended to be smaller scale 30’-45’ tall townhouses with front porches and architectural elements and materials compatible with Del Ray. The district is intended to be a mix of building types and uses ranging from townhouses, multi-family, office, hotel buildings, medical care facility, and retail and maker space.

P.51:

- Amend Standard 41, as follows: The intersection of Glebe Road and Route 1 will need to be reconfigured as generally depicted within the attached cross-sections. ~~The timing and phasing of the improvements will be established as part of the CDD zoning for the Plan area.~~ Funding will be proposed as part of the Oakville Plan CDD for the improvements.
- Amend Standard 4.5iii, as follows: A maximum of one curb cut per block face shall be permitted on each side of the street. To the extent possible, curb cuts should be aligned with curb cuts on the opposite side of the street. Offset curbs may be permitted, where it is deemed necessary. Additional curb cuts may be considered as part of the DSUP process.

P.56:

- Amend the text under Active Uses in the third column, as follows: Active uses are specified on the ground level or frontages of many blocks in the Plan area to ensure a vibrant public realm. For purposes of the Design Standards, active uses shall be residential, office, medical care facility, retail, maker, and/or hotel uses.

*MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan Amendments*

P.57:

- Amend the header in the first column of text, as follows: ~~PRIMARY-RETAIL:~~
- Amend the first column of text, as follows: For purposes of the Design Standards, primary retail is intended as retail uses and ~~restaurants~~ personal service uses. Final uses and requirements are specified with the CDD zoning ~~for the Plan area and on pg 58.~~
- Delete the header and second column of text, as follows: ~~SECONDARY RETAIL:~~
- ~~For purposes of the Design Standards, secondary retail is a combination of retail, maker-uses and personal service uses. Final uses and requirements are specified with the CDD-zoning for the Plan area and on pg 58.~~

P.58:

- Amend the header in the first column of text, as follows: ~~PRIMARY-RETAIL~~
- Amend the Intent text in the first column, as follows: Intent: For the purposes of the Oakville Triangle/Route 1 Corridor Plan Area, the areas designated for “~~primary-retail~~” are intended to achieve an experiential environment, where shopping, dining, and other establishments engage pedestrians and activate the street front with attractive, unique storefronts. Generally, this includes restaurants, personal service uses, and retail as defined in the CDD. ~~As shown on the Land Uses Map, the area designated for primary-retail is Swann Avenue within Oakville Triangle, with some additional primary retail-designated for north and south of Swann on Oakville Street and Route 1. Examples of Primary Retail include restaurants, coffee shops, bakeries, gourmet food, home and fashion retail, and interactive establishments such as cooking or other experiential schools.~~
- Amend the Allowed Uses text in the first column of text, as follows: Allowed Uses: For the purposes of the Plan area, Primary Retail is defined to include:
 - Restaurants
 - Personal service uses
 - Retail shopping establishments (~~excluding uses detailed below~~)
- Amend the Excluded Uses in the second column of text, as follows: ~~Excluded Uses: For the purposes of the Plan, the following uses are considered incompatible with the intent of Primary Retail:-~~
 - ~~Excluded retail shopping establishments: Appliances, Auto parts without service or installation on premises, Drugstores, lawn and garden supplies, Variety Goods~~
 - ~~Personal service establishments~~
- Delete the header and the second column of text, as follows: ~~SECONDARY RETAIL~~
- ~~Intent: For the purposes of the Oakville Triangle/Route 1 Corridor Plan Area, the areas designated for “secondary retail” are intended to achieve a neighborhood serving and personal services retail environment, where frequent shopping, repair, or service needs can be met. The main area designated for secondary retail is on Calvert Avenue, where there is also a significant degree of overlap with Maker Space Use (Figure 7).~~

*MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan Amendments*

- Delete the text in the second and third columns, as follows: ~~Allowed Uses: For the purposes of the Plan area, Secondary Retail is defined to include:~~
 - ~~Retail~~
 - ~~Personal service uses (excluding uses detailed below) Amusement enterprises~~
 - ~~Restaurants~~

~~Additional Uses: Other uses consistent with the neighborhood-serving intent of the Plan herein, may be allowed at the discretion of the Director of Planning and Zoning.~~

~~Excluded Uses: For the purposes of the Plan area, Secondary Retail may not include the following:~~

- ~~Retail shopping establishments shall not include appliance stores and auto parts stores~~
- ~~Personal service uses shall not include appliance repair and rental, contractors' offices, laundromats, and pawnshops~~

P.66:

- Delete the second and third bullet in the second column of text, as follows:
 - ~~Five additional open spaces adjacent to Mount Jefferson Park, ranging in size from ±4,400 sq. ft. to ± 21,000 sq. ft.~~
 - ~~: A ± 21,000 sq. ft. central urban square within the Oakville Triangle.~~
- Add fourth bullet in the second column of text, as follows: A central .7 acre public park in Oakville Triangle.
- Amend the second bullet in the third column of text, as follows: Mixed-use projects (with ground floor retail - commercial) will include a minimum of 15% 20% ground level open space and 25% roof top amenity space as part of redevelopment with a total of 40% on-site open space to include rooftop-amenity space as part of redevelopment.

P.67:

- Amend the header in the first column of text, as follows: OAKVILLE URBAN PLAZA CENTRAL OPEN SPACE
- Amend the text in the first and second columns, as follows: The central urban plaza will be hardscape with appropriate plantings, shade options, and lighting. High quality materials and finishes, as well as the inclusion of public art or other focal features, ensure this space will be a great amenity. The plaza will be a shared space for pedestrians and bikes. The central .7 acre open space is intended to be a combination of multiple outdoor "Rooms" for multigenerational programing, including but not limited to an outdoor meeting space, lawn area, children's play area and sustainability features and public art. The materiality of the park will also incorporate the industrial heritage of the site through materials such as wood, stone, weathered metal, etc.

P.69:

- Amend text, as follows: These open spaces will range from front yards and courtyards to a new public open spaces and an urban plaza on Swann Avenue.

*MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan Amendments*

P.70:

- Amend Standard 4.18, as follows: The ~~five~~ new open spaces adjacent to Mount Jefferson Park shall be constructed as generally depicted in Figure 30, as part of the redevelopment adjacent to the Mount Jefferson Park. The at grade trail (lower trail) extension on the northern portion shall be constructed by the adjoining property owner.
- Amend Standard 4.19, as follows: The central open space within Oakville Triangle shall ~~be hardscape with~~ have appropriate plantings, shade options, and lighting and shall contain high-quality materials and finishes, as well as the inclusion of public art or other focal features. The open space/~~plaza~~ shall be accessible to the public through the provision of a public access easement.
- Amend Standard 4.23, as follows: New mixed-use projects (with ground floor retail-commercial) shall provide a minimum of ~~15%~~ 20% ground level open space ~~and 25% roof-top amenity space.~~ with a total of 40% on-site open space to include rooftop-amenity space as part of redevelopment.

P.74:

- Amend Standard 5.2, as follows: The streetwall shall generally be parallel to the street and at a minimum height as required herein. The streetwall for each block frontage shall be a minimum of 75% for residential, medical care facility, and office and hotel. For residential buildings where courtyards are provided, the streetwall shall be a minimum of 50%. A courtyard shall be limited to a maximum of two block frontage. Ground floor retail and maker uses shall provide a minimum of 85% continual streetwall.

P.81:

- Amend Standard 5.25, as follows: A minimum of 50% of each group of townhouses for each project shall provide a front porch. The porches shall comply with the applicable requirements herein. Final location of porches will be determined during the DSUP process.

P.99:

- Amend the title of Figure 41, as follows: Figure 41: Ground Floor Uses: ~~Primary, Secondary~~ Retail, ~~and~~ Maker Spaces

P.113:

- Amend Standard 8.3, as follows: All streets within the Plan are intended to be public streets, dedicated to the City or private with public access easements. ~~unless otherwise noted the property line is assumed to be at the edge of the right-of-way (ROW) as defined herein.~~

P.121:

- Amend the Typology header text, as follows: TYPOLOGY: ~~SHARED STREET~~ NEIGHBORHOOD RESIDENTIAL

*MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan Amendments*

P. 130:

- Amend recommendation 1.13, as follows: A pedestrian and bike connection between Swann Avenue and Stewart Avenue to provide connectivity between the neighborhoods and the future Potomac Yard Metrorail station. ~~The bike lane within the central urban plaza within the Oakville Triangle site will be designed as a shared space.~~
- Amend recommendation 1.17, as follows: ~~An additional signalized pedestrian crossing across Route 1 between East Custis Avenue and East Glebe Road to improve pedestrian access between Oakville Triangle and Del Ray/ Lynhaven and the future Potomac Yard Metrorail station.~~ An additional signalized crossing across Route 1 at Fannon Street, including pedestrian and intersection improvements, to improve pedestrian access between Oakville Triangle and Del Ray/Lynhaven and the Potomac Yard Metrorail station and provide efficient access to the medical care facility.

P.131:

- Amend Standard 2.5, as follows: Provide a new central public urban plaza open space within Oakville Triangle that includes usable amenities that help to meet the recreational needs of new residents.
- Amend Standard 2.6, as follows: Provide ~~five~~ four new ground level public green spaces adjacent to Mount Jefferson Park.

P. 132:

- Amend Standard 4.1, as follows: The intersection of Glebe Road and Route 1 will need to be reconfigured as generally depicted within the attached cross-sections. ~~The timing and phasing of the improvements will be established as part of the CDD zoning for the Plan area. Funding will be proposed as part of the Oakville Plan CDD for the improvements.~~
- Amend Standard 4.5iii, as follows: A maximum of one curb cut per block face shall be permitted on each side of the street. To the extent possible, curb cuts should be aligned with curb cuts on the opposite side of the street. Offset curbs may be permitted, where it is deemed necessary. Additional curb cuts may be considered as part of the DSUP process.

P.133:

- Amend Standard 4.18, as follows: The ~~five~~ new open spaces adjacent to Mount Jefferson Park shall be constructed as generally depicted in Figure 30, as part of the redevelopment adjacent to the Mount Jefferson Park. The at grade trail (lower trail) extension on the northern portion shall be constructed by the adjoining property owner.
- Amend Standard 4.19, as follows: The central open space within Oakville Triangle shall ~~be hardscape with~~ have appropriate plantings, shade options, and lighting and shall contain high-quality materials and finishes, as well as the inclusion of public art or other focal features. The open space/~~plaza~~ shall be accessible to the public through the provision of a public access easement.

*MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan Amendments*

P.134:

- Amend Standard 4.23, as follows: New mixed-use projects (with ground floor retail-commercial) shall provide a minimum of ~~15%~~ 20% ground level open space ~~and 25% roof-top amenity space~~ with a total of 40% on-site open space to include rooftop-amenity space as part of redevelopment.
- Amend Standard 4.34, as follows: As part of the new multi-family, office, medical care facility, or hotel buildings, explore providing a community meeting space within each building.

P.135:

- Amend Standard 5.2, as follows: The streetwall shall generally be parallel to the street and at a minimum height as required herein. The streetwall for each block frontage shall be a minimum of 75% for residential, medical care facility, and office and hotel. For residential buildings where courtyards are provided, the streetwall shall be a minimum of 50%. A courtyard shall be limited to a maximum of two block frontage. Ground floor retail and maker uses shall provide a minimum of 85% continual streetwall.

P.137:

- Amend Standard 5.25, as follows: A minimum of 50% of each group of townhouses for each project shall provide a front porch. The porches shall comply with the applicable requirements herein. Final location of porches will be determined during the DSUP process.

P.146:

- Amend Standard 8.3, as follows: All streets within the Plan are intended to be public streets, dedicated to the City or private with public access easements. ~~unless otherwise noted the property line is assumed to be at the edge of the right-of-way (ROW) as defined herein.~~

b. Updates to Figures, Tables and Charts as listed below and attached:

- Figure 3 & 18: Illustrative Plan
- Figure 7: Oakville Triangle District
- Figure 19: Framework Streets
- Figure 25: Land Uses
- Figure 27: Maximum Building Heights
- Figure 28: Minimum Building Heights
- Figure 29: Open Spaces (Public, Ground level and Roof-top)
- Figure 30: Open Spaces adjacent to Mount Jefferson Park
- Figure 41: Ground Floor Uses: ~~Primary, Secondary~~ Retail, and Maker Spaces
- Figure 47: New and Reconfigured Streets
- Street Cross-Sections: Pages 117 - 125
- P.24: Transportation Improvements Table

***MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan Amendments***

- Chart 1: Public Benefits Phasing
- Chart 2: Development Summary Table

See following pages.

*MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan Amendments*

Figures 3 & 18: Illustrative Plan

Approved 2015 Plan as amended



Proposed 2020 Amendment

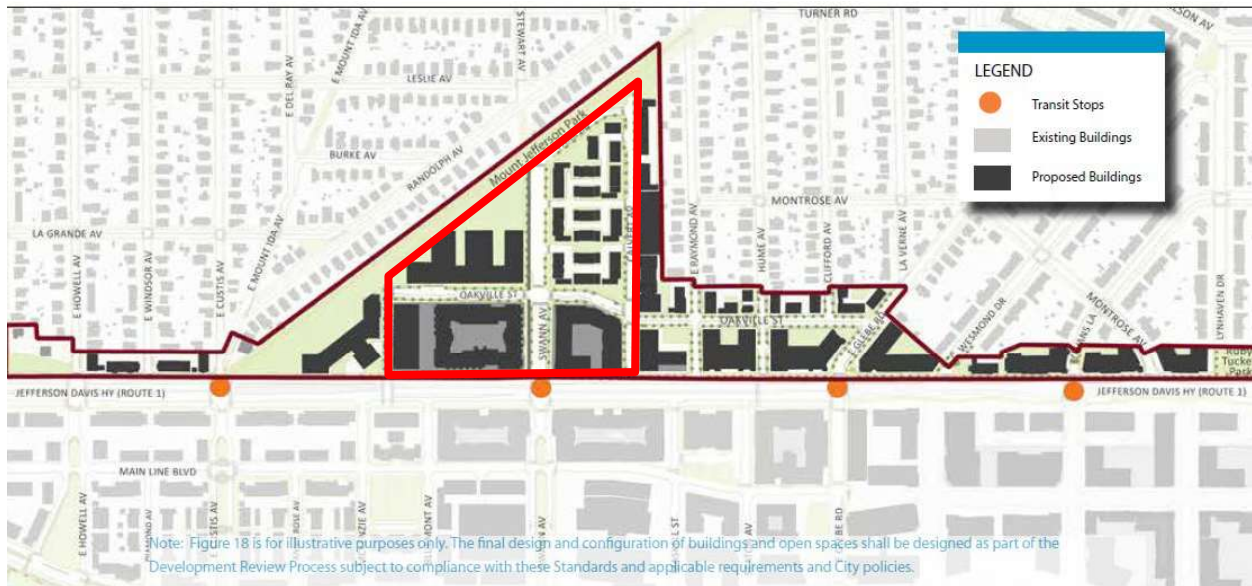
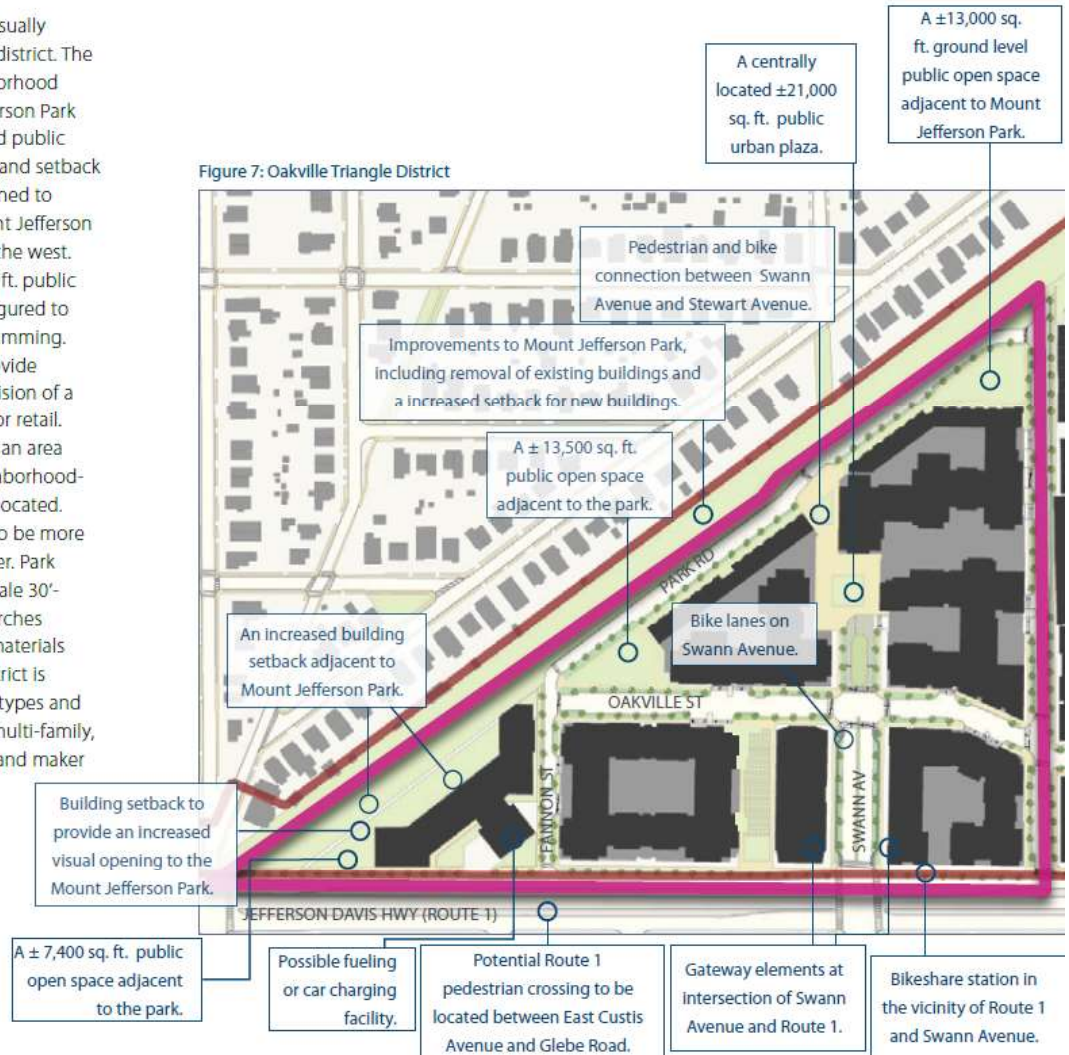


Figure 7: Oakville Triangle District

Approved 2015 Plan as amended

B. DISTRICT 2 - OAKVILLE TRIANGLE

Establishing the street grid will visually unify this approximately 15-acre district. The defining elements of this neighborhood will be the adjoining Mount Jefferson Park frontage and the centrally located public urban plaza. The building height and setback of the buildings have been designed to step down to the adjoining Mount Jefferson Park and single-family homes to the west. The centrally located $\pm 21,000$ sq. ft. public open space will need to be configured to accommodate events and programming. Swann Avenue is intended to provide retail shopping through the provision of a significant amount of ground floor retail. Calvert Avenue is intended to be an area where some of the existing neighborhood-serving or "maker uses" could be located. Calvert Avenue is also intended to be more "industrial" in design and character. Park Road is intended to be smaller scale 30'-45' tall townhouses with front porches and architectural elements and materials compatible with Del Ray. The district is intended to be a mix of building types and uses ranging from townhouses, multi-family, office, hotel buildings, and retail and maker space.

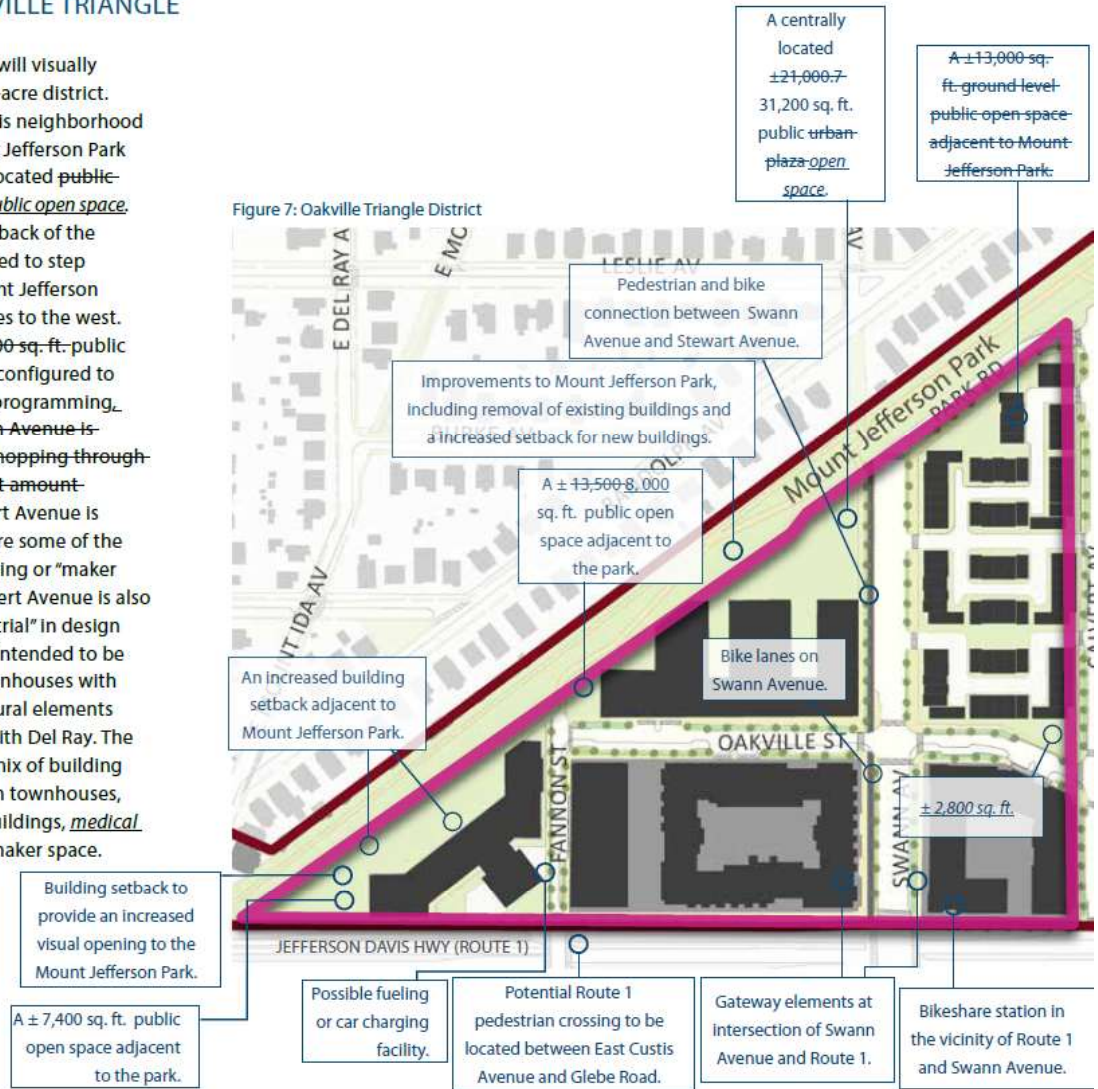


MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan Amendments

Proposed 2020 Amendment

B. DISTRICT 2 - OAKVILLE TRIANGLE

Establishing the street grid will visually unify this approximately 15-acre district. The defining elements of this neighborhood will be the adjoining Mount Jefferson Park frontage and the centrally located public urban plaza $\pm 31,200$ sq. ft. public open space. The building height and setback of the buildings have been designed to step down to the adjoining Mount Jefferson Park and single-family homes to the west. The centrally located $\pm 21,000$ sq. ft. public open space will need to be configured to accommodate events, and programming, and social gatherings. Swann Avenue is intended to provide retail shopping through the provision of a significant amount of ground floor retail. Calvert Avenue is intended to be an area where some of the existing neighborhood-serving or "maker uses" could be located. Calvert Avenue is also intended to be more "industrial" in design and character. Park Road is intended to be smaller scale 30'-45' tall townhouses with front porches and architectural elements and materials compatible with Del Ray. The district is intended to be a mix of building types and uses ranging from townhouses, multi-family, office, hotel buildings, medical care facility, and retail and maker space.



MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan Amendments

Figure 19: Framework Streets

Approved 2015 Plan as amended



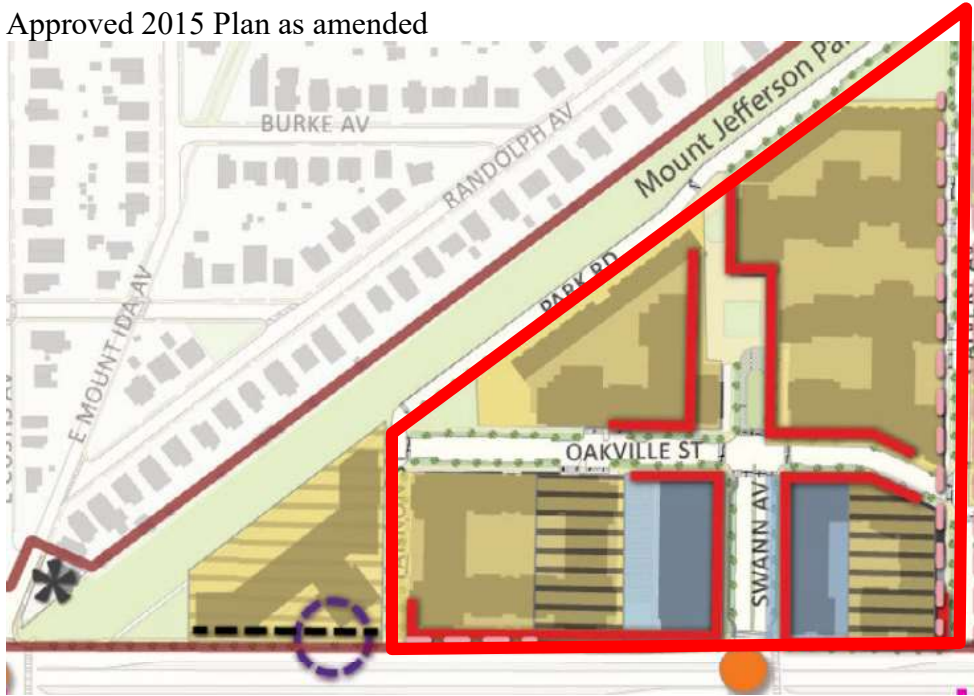
Proposed 2020 Amendment



Figure 25: Land Uses

Note: Only portion of the figure is shown below.

Approved 2015 Plan as amended

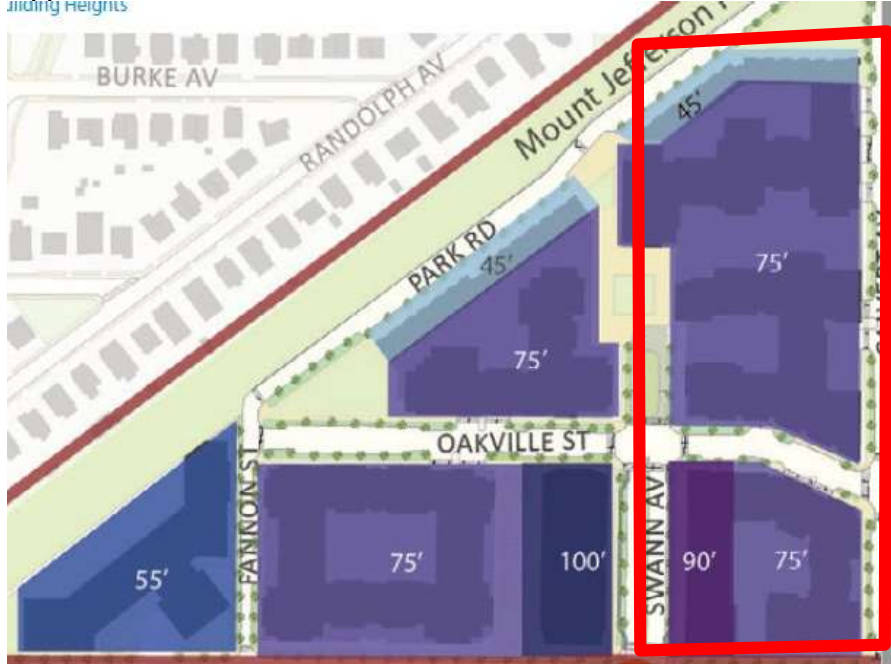


Proposed 2020 Amendments



Figure 27: Maximum Building Heights
Note: Only portion of the figure is shown below

Approved 2015 Plan as amended



Proposed 2020 Amendment



Figure 28: Minimum Building Heights

*MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan Amendments*

Note: Only portion of the figure is shown below

Approved 2015 Plan as amended



Proposed 2020 Height Amendments



Figure 29: Open Spaces (Public, Ground level and Roof-top)

*MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan Amendments*

Approved 2015 Plan as amended



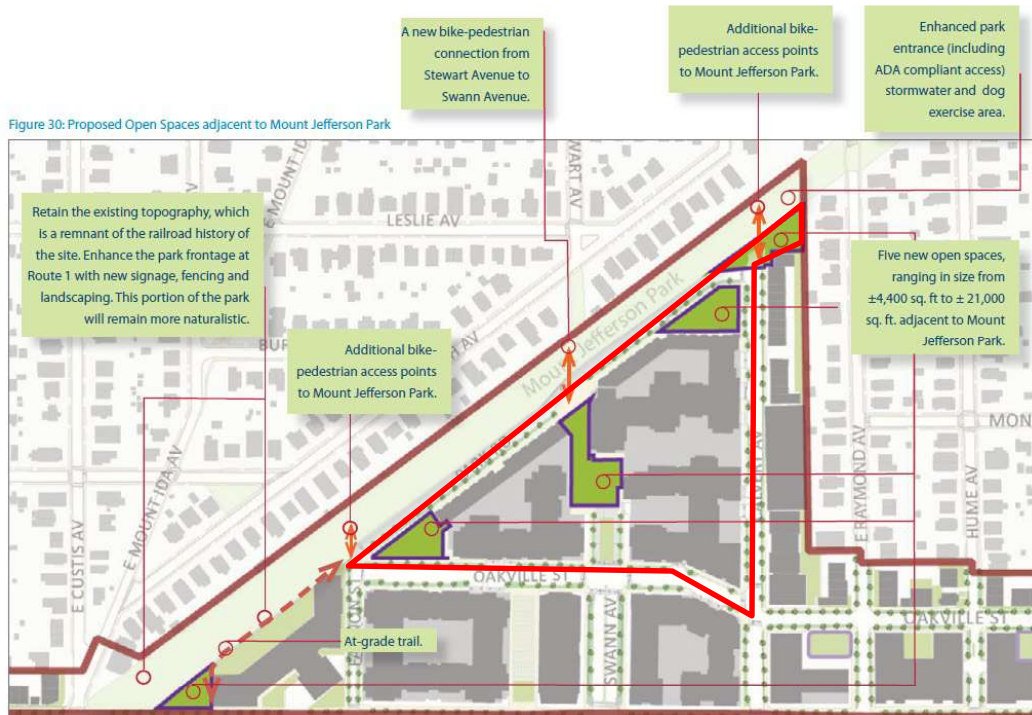
Proposed 2020 Amendment



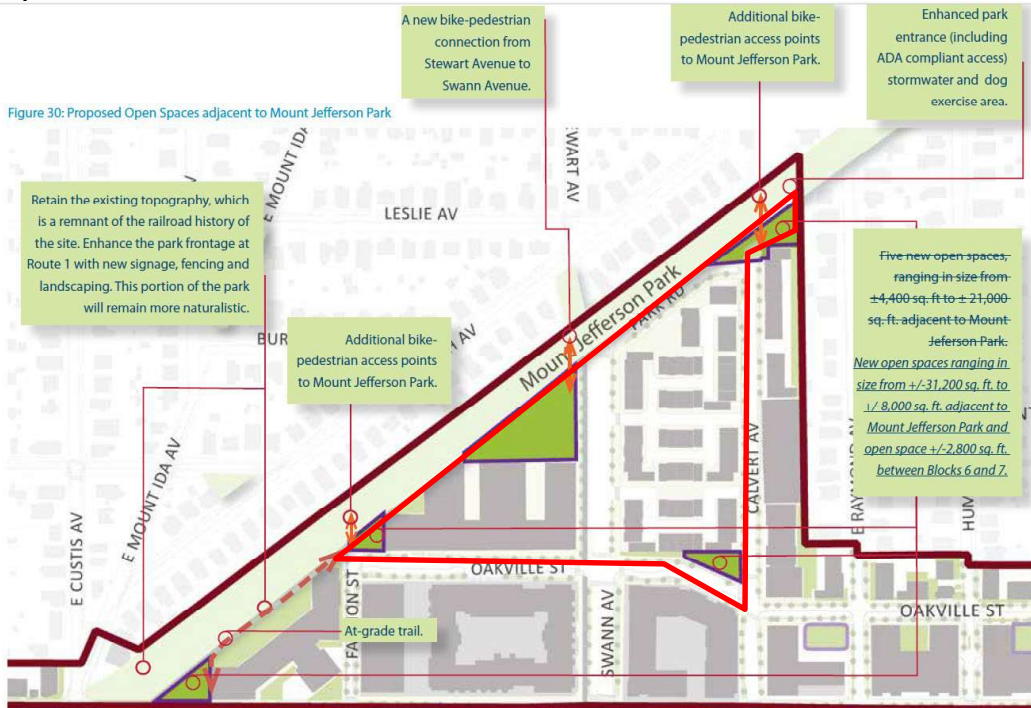
MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan Amendments

Figure 30: Open Spaces adjacent to Mount Jefferson Park

Approved 2015 Plan as amended



Proposed 2020 Amendment



MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan Amendments

Figure 41: Ground Floor Uses: Primary, Secondary Retail, and Maker Spaces

Approved 2015 Plan as amended



Proposed 2020 Amendment



Figure 47: New and Reconfigured Streets
Note: Only portion of the figure is shown below

Approved 2015 Plan as amended

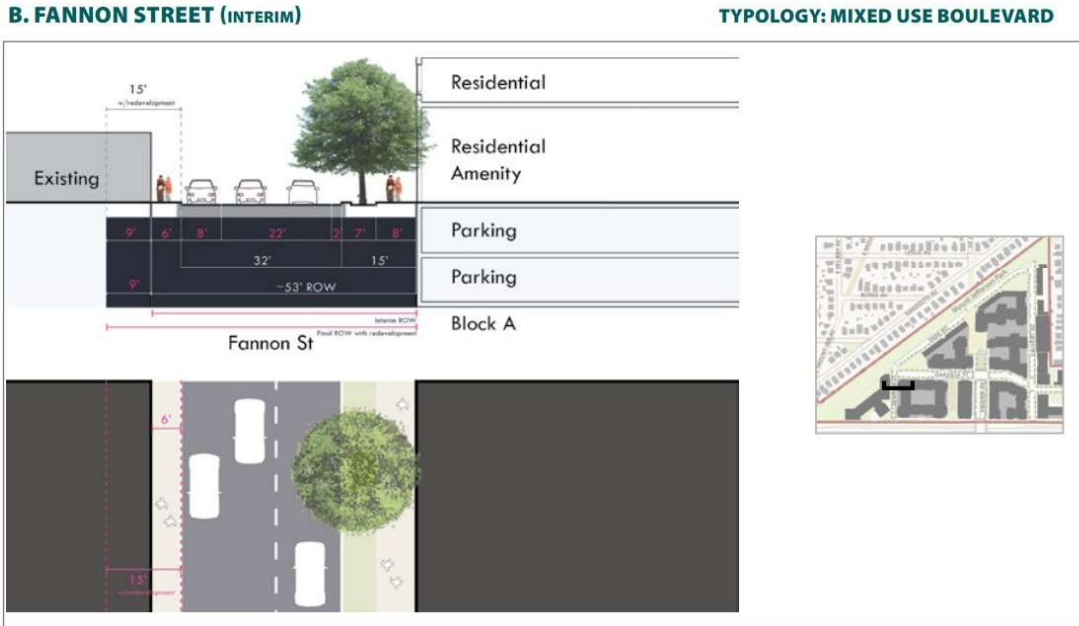


Proposed 2020 Amendment

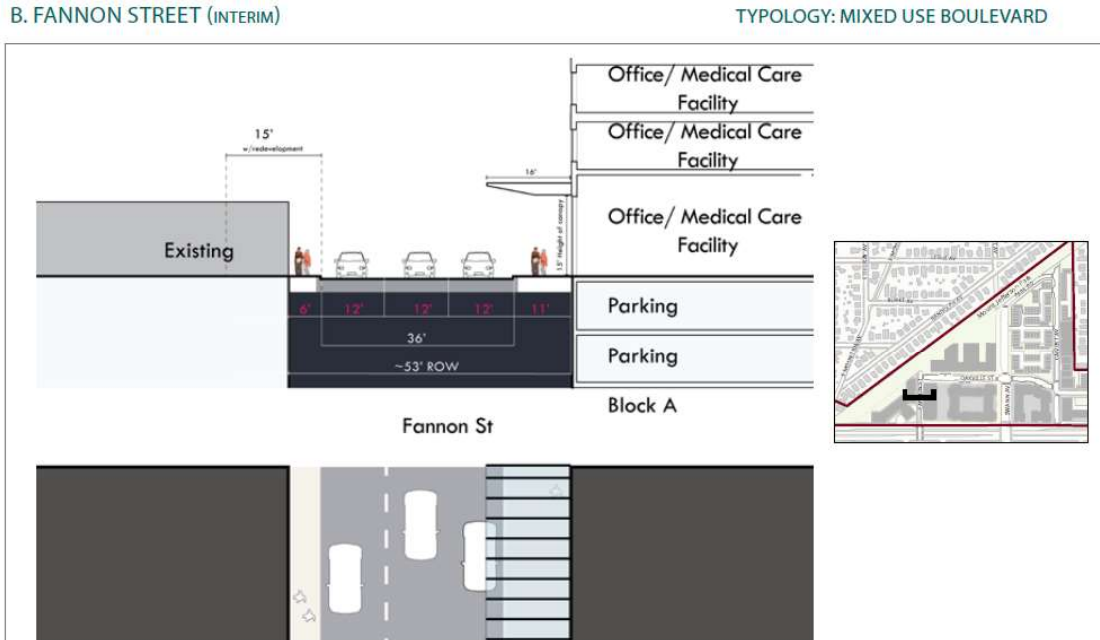


Street Cross Sections: Pages 117 – 125

p.117 Fannon Street (Interim) (2015)



p.117 Fannon Street (Interim) (Proposed)

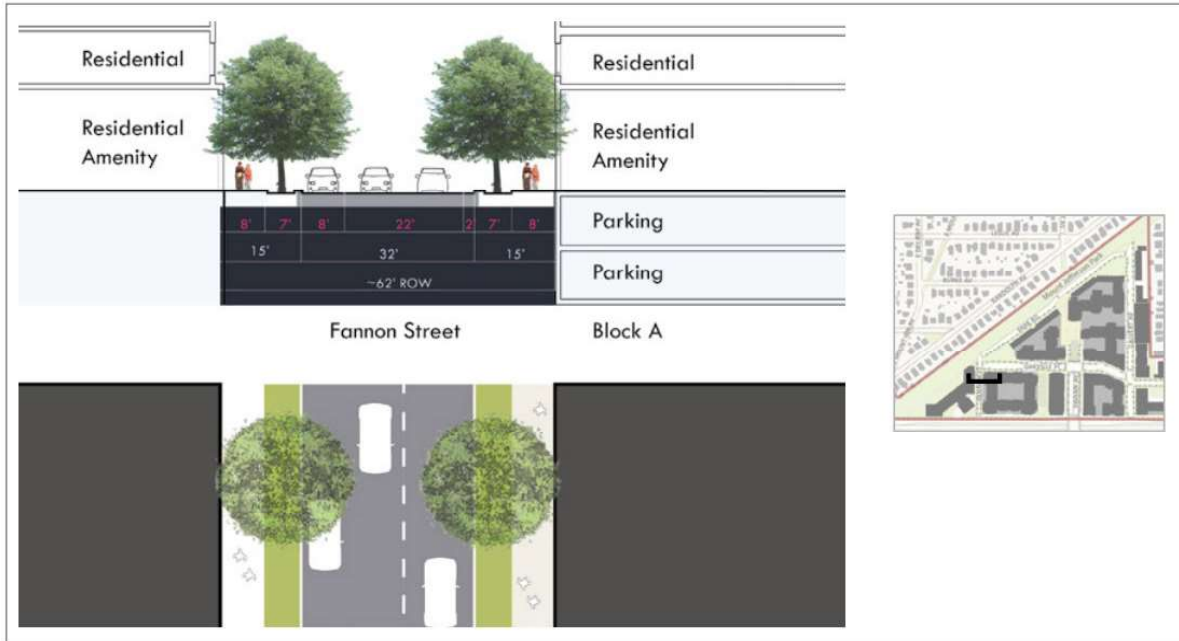


MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan Amendments

p. 118 Fannon Street (with Redevelopment) (2015)

FANNON STREET (WITH REDEVELOPMENT)

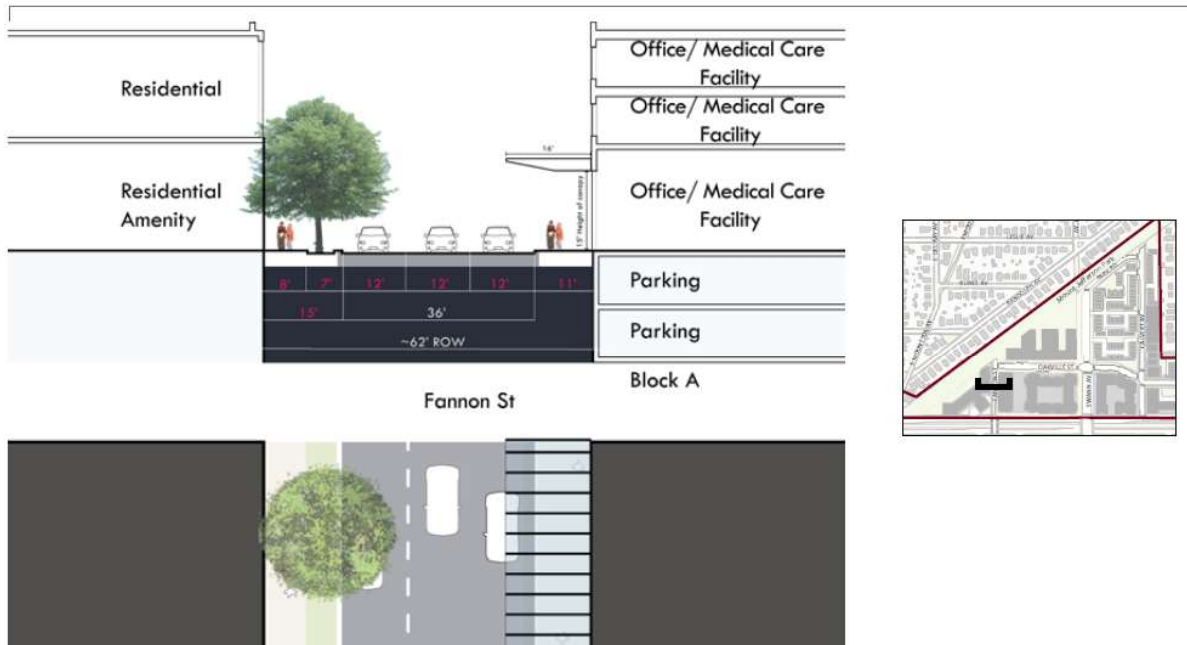
TYPOLGY: MIXED USE BOULEVARD



p.118 (Proposed)

FANNON STREET (WITH REDEVELOPMENT)

TYPOLGY: MIXED USE BOULEVARD

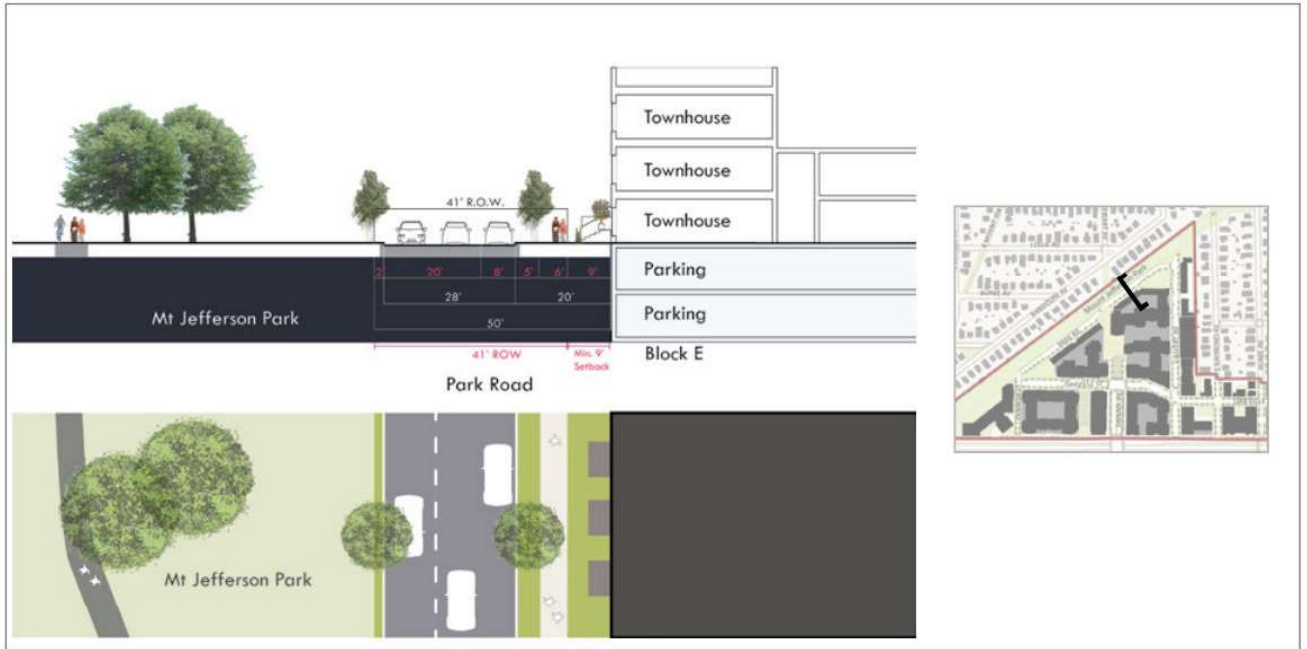


*MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan Amendments*

p. 119 (2015)

C. PARK ROAD

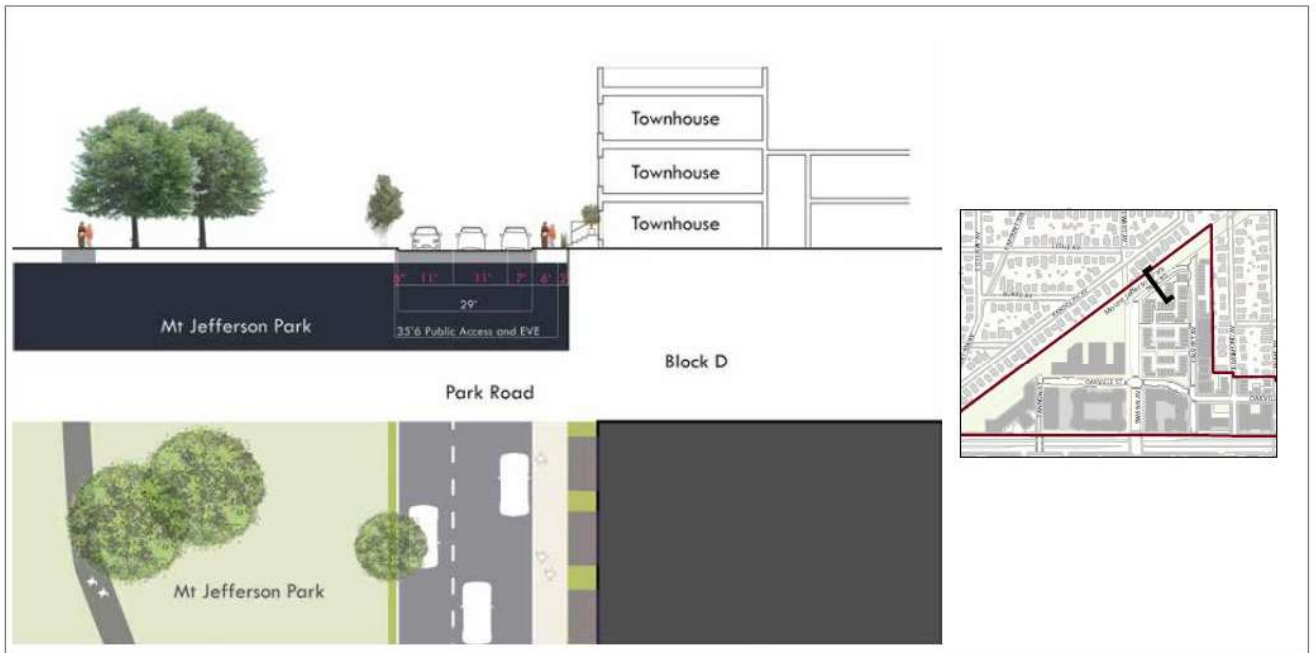
TYOLOGY: PARK ROAD



p.119 (Proposed)

C. PARK ROAD

TYOLOGY: PARK ROAD

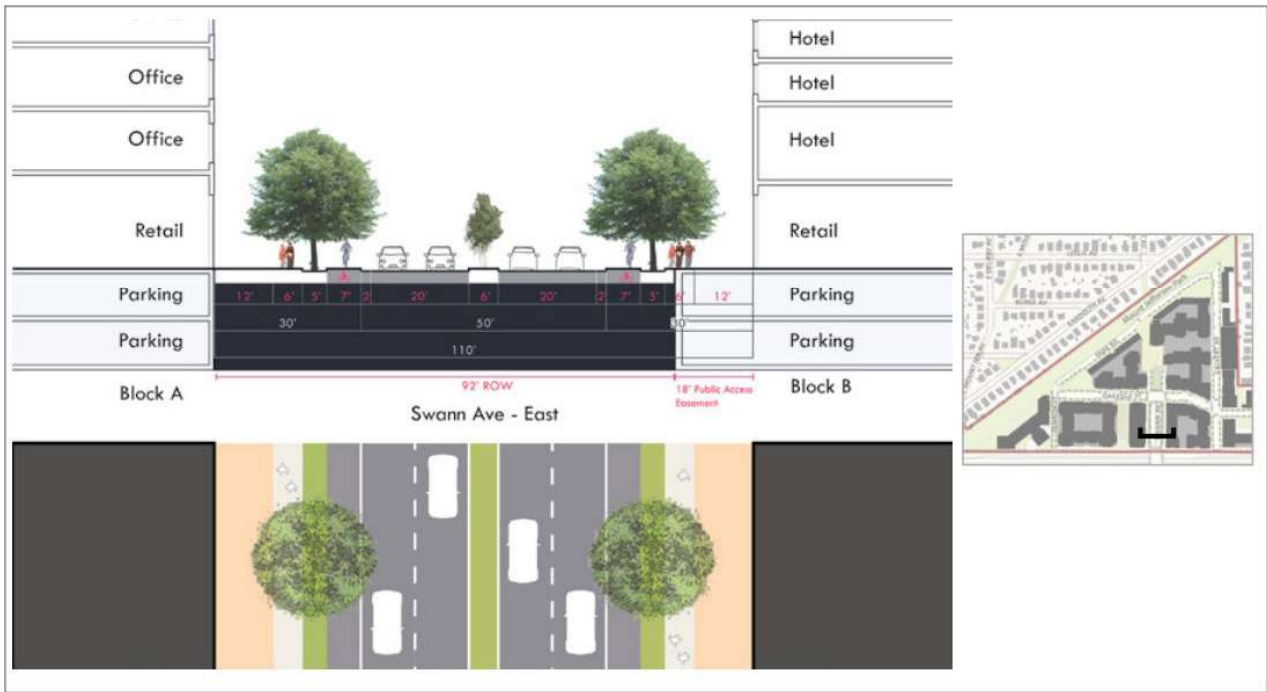


**MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan Amendments**

p. 120 (2015)

D. SWANN AVENUE (EAST)

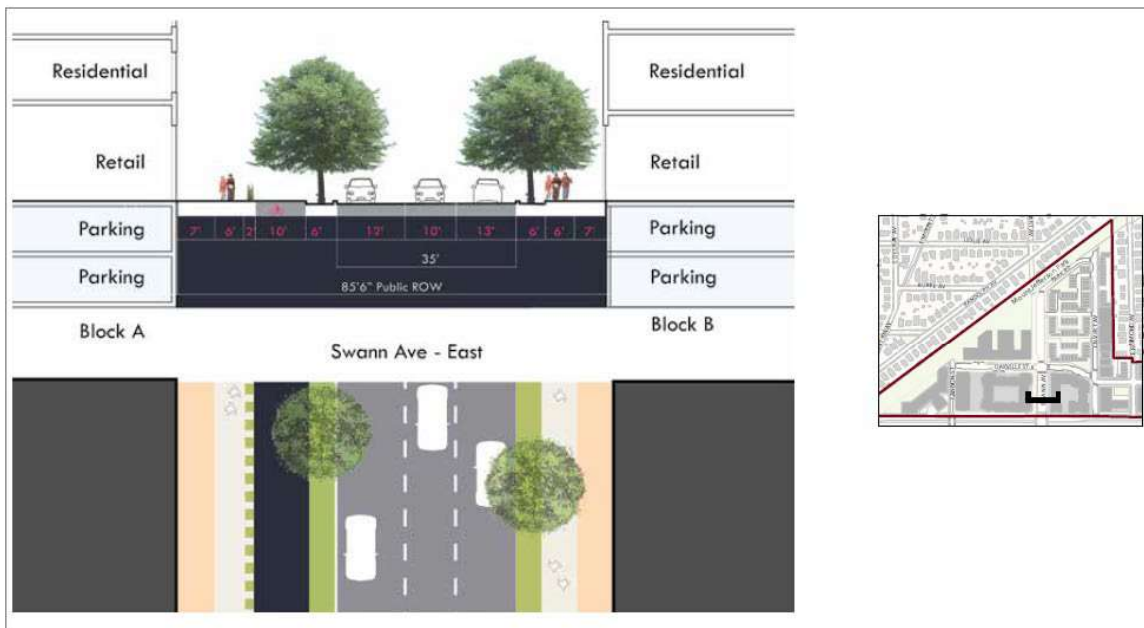
TYPOLGY: MIXED USE BOULEVARD



p. 120 (Proposed)

D. SWANN AVENUE (EAST)

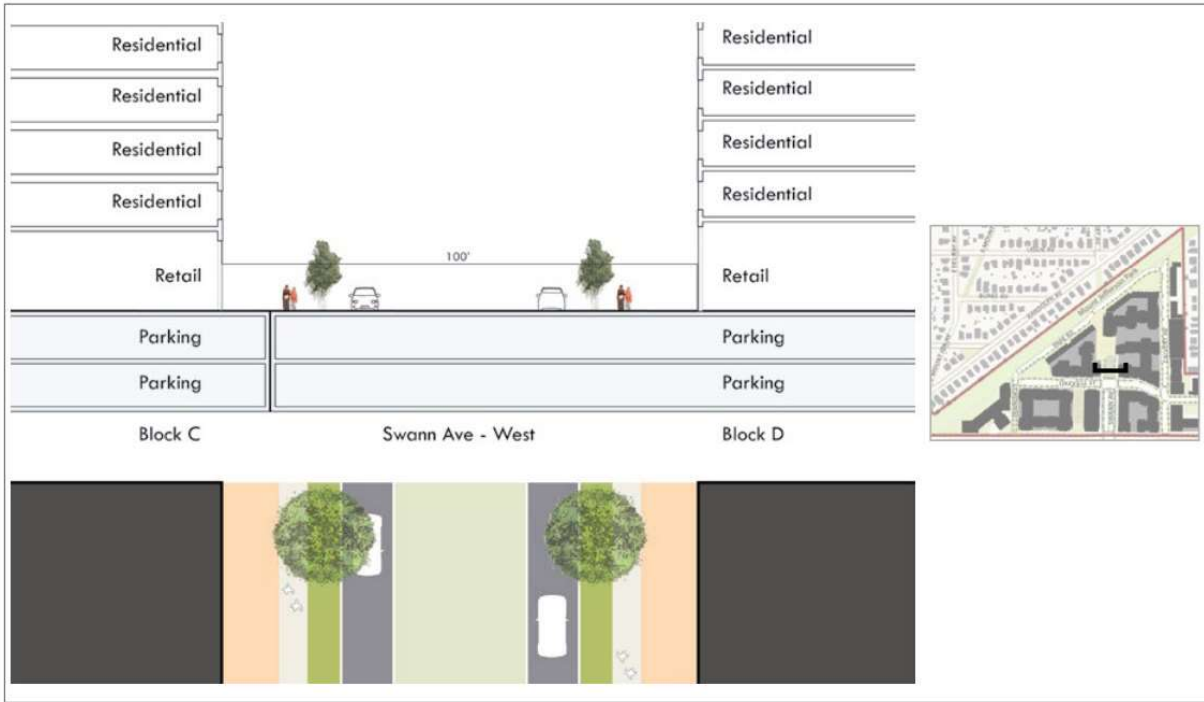
TYPOLGY: MIXED USE BOULEVARD



p.121 (2015)

SWANN AVENUE (WEST)

TYPOLOGY: SHARED STREET

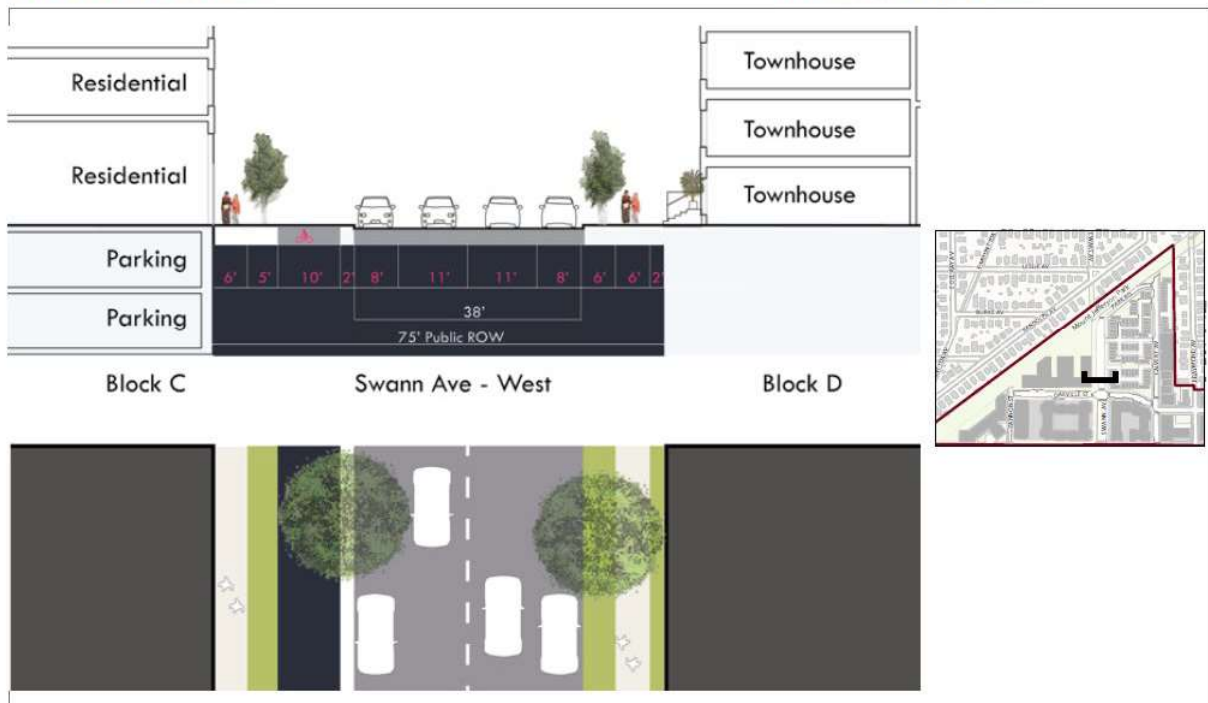


p.121 (Proposed)

NEIGHBORHOOD RESIDENTIAL

SWANN AVENUE (WEST)

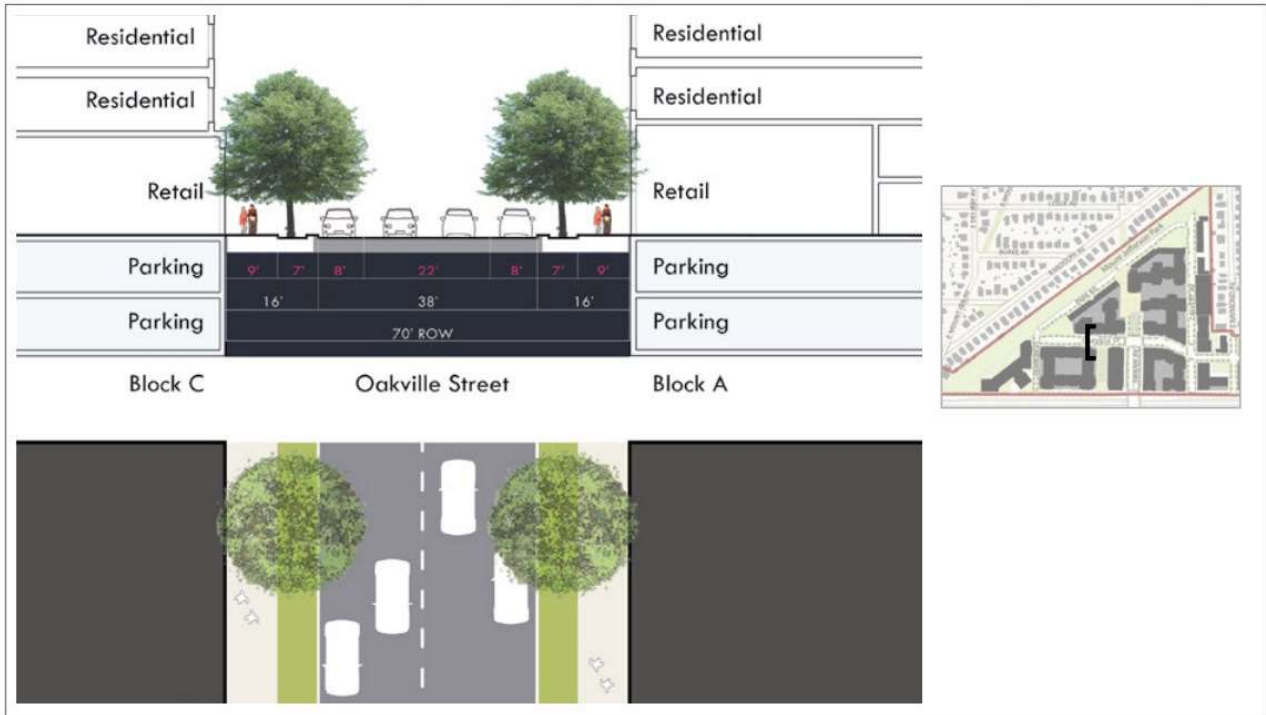
TYPOLOGY: SHARED STREET



**MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan Amendments**

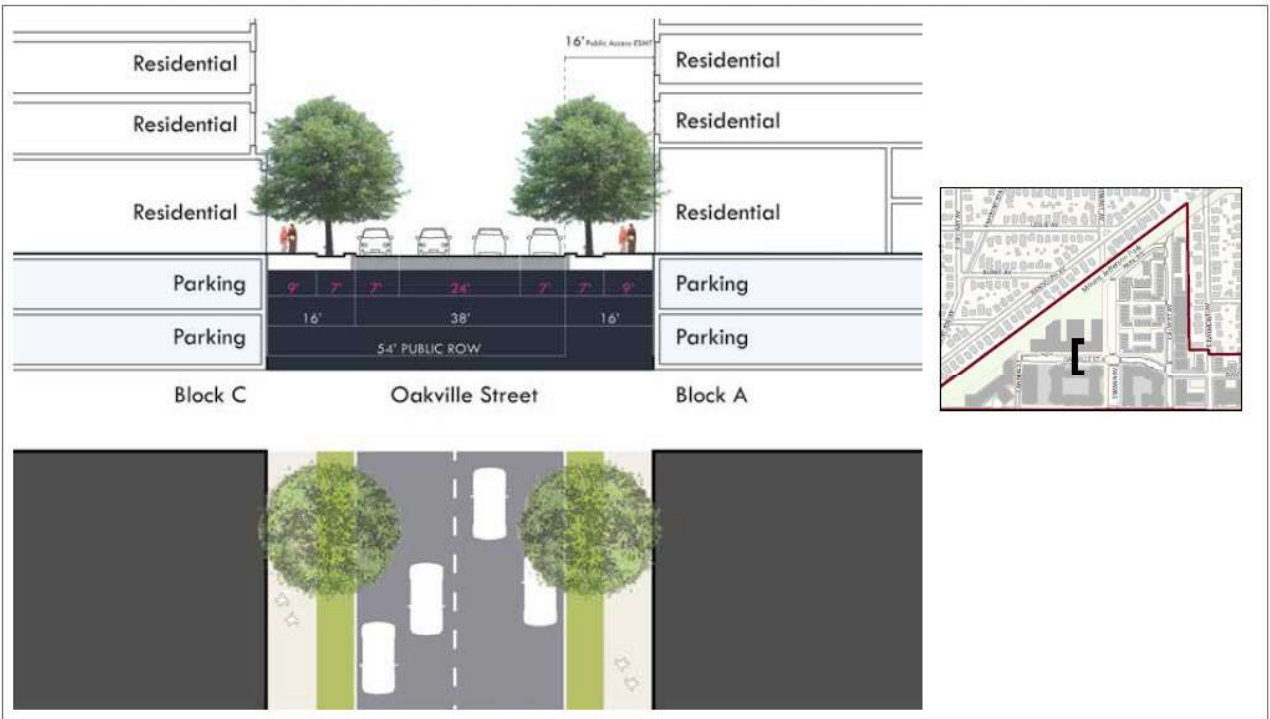
p.122 (2015)

E. OAKVILLE STREET (FANNON STREET TO CALVERT AVENUE) TYPOLOGY: MIXED USE BOULEVARD



p.122 (Proposed)

E. OAKVILLE STREET (FANNON STREET TO CALVERT AVENUE) TYPOLOGY: MIXED USE BOULEVARD



p.123 (2015)

F. CALVERT AVENUE (WEST - INTERIM)

TYOLOGY: MAIN STREET



p. 123 (Proposed)

F. CALVERT AVENUE (WEST - INTERIM)

TYOLOGY: MAIN STREET



p.124 (2015)

CALVERT AVENUE (WEST - WITH REDEVELOPMENT)

TYPOLOGY: MAIN STREET



p. 124 (Proposed)

CALVERT AVENUE (WEST - WITH REDEVELOPMENT)

TYPOLOGY: MAIN STREET



p.125 (2015)

CALVERT AVENUE (EAST)

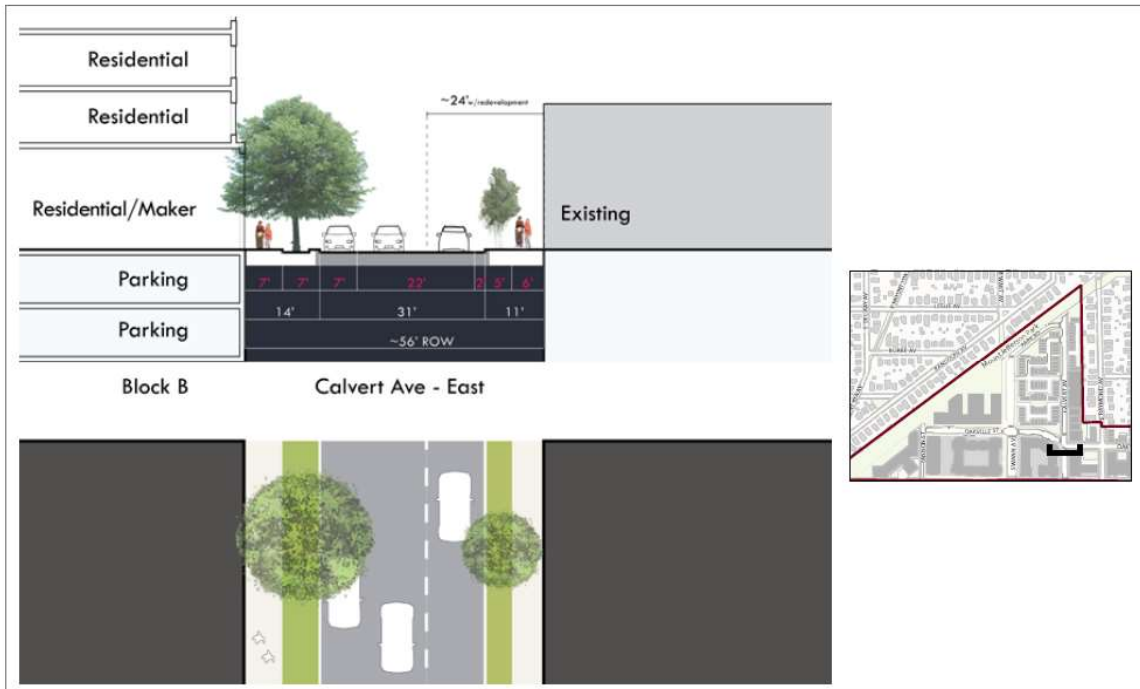
TYOLOGY: MIXED USE BOULEVARD



p. 125 (Proposed)

CALVERT AVENUE (EAST)

TYOLOGY: MIXED USE BOULEVARD



MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan Amendments

p. 24. Transportation Improvements (2015)

PHASE	DESCRIPTION	RESPONSIBILITY/ FUNDING	REQUIRED TIMING
1	Construction of lane modifications eastbound Swann Ave. at Route 1, as specified in Oakville Triangle CDP	Developer of Oakville Triangle	Operational prior to issuance of first Certificate of Occupancy (CO) for development within the Oakville Triangle CDP area (not including above or below grade parking).
1	Initial work on Phase 1 improvements at Route 1/E. Glebe Rd. (eastbound right turn modified to allow overlap right turns), as specified in Oakville Triangle CDP	Developer of Oakville Triangle to provide \$200,000	Prior to release of the final site plan for the first block within the Oakville Triangle CDP area.
1	Route 1 / E. Reed signal modifications (Address through Potomac Yard North SAP Amendment)	Developer contributions and /or incremental tax	Prior to 800,000 sf of development (net of parking)
2	Phase 1 improvements at Route 1/E. Glebe Rd.: Signal phasing and construction from Route 1 to new North-South Road	Developer of Oakville Triangle to provide \$1,400,000	Earlier of: (1) 1 year after issuance of final CO for completion of Phase I (Block A1, B & D) or (2) prior to first final site plan submission for fourth block in the project.
2	Route 1/E. Reed intersection improvements: Westbound lanes modified from exclusive left turn lane and shared through/right lane to exclusive left, through and right turn lanes	Developer contributions and /or incremental tax	Prior to issuance of CO at 1.6 million sf (Net without parking)
2	New signal at Montrose Ave./ Route 1.	Developer contributions and /or incremental tax	Prior to issuance of CO at 1.6 million sf (Net without parking)
2	Improvements on Montrose Avenue and at the intersection of Montrose Avenue at East Glebe Road / Ashby Street	Developer contributions and /or incremental tax	Prior to issuance of CO at 1.6 million sf (Net without parking)
2	New Route 1 pedestrian crossing/signal between East Custis Avenue and East Glebe Road.	Developer contributions and /or incremental tax	Prior to issuance of CO at 2 million sf (Net without parking)
3	Route 1 / E. Glebe Intersection improvements (Phase 2 from the new North-South Road to LaVerne Avenue) or comparable improvement – in consultation with property owners.	Developer contributions and/ or incremental tax	Prior to issuance of CO at 2 million sf (Net without parking)
3	Modification of lane configuration at Route 1/Custis Ave.	Developer contributions and/ or incremental tax	Prior to issuance of CO at 2 million sf (Net without parking)

p. 24 Transportation Improvements (Proposed)

PHASE	DESCRIPTION	RESPONSIBILITY/ FUNDING	REQUIRED TIMING
1	Construction of lane modifications eastbound Swann Ave. at Route 1, as specified in Oakville Triangle CDP	Developer of Oakville Triangle	Operational prior to issuance of first Certificate of Occupancy (CO) for <u>blocks 4A or Z</u> development within the Oakville Triangle CDP area (not including above or below grade parking).
1	Initial work on Phase 1 improvements at Route 1/E. Glebe Rd. (eastbound right turn modified to allow overlap right turns), as specified in Oakville Triangle CDP	Developer of Oakville Triangle to provide \$200,000 <u>\$350,000</u>	Prior to release of the final site plan for blocks 4A or 7 the first block within the Oakville Triangle CDP area.
1	Route 1 / E. Reed signal modifications (Address through Potomac Yard North SAP Amendment)	Developer contributions and /or incremental tax	Prior to 800,000 sf of development <u>With North Potomac Yard first building</u> (net of parking)
1	<u>New signalized intersection improvements and pedestrian crossings at Route 1/Fannon Street</u>	<u>Developer of Oakville Triangle</u>	<u>Operational prior to issuance of Certificate of Occupancy (CO) for the Block 4B.</u>
2	Phase 1 improvements at Route 1/E. Glebe Rd.: Signal phasing and construction from Route 1 to new North-South Road	Developer of Oakville Triangle to provide \$1,400,000	Earlier of: (1) 1 year after issuance of final CO for completion of Phase I (Block A1, B & D) or (2) prior to first final site plan submission for fourth block in the project.
2	Route 1/E. Reed intersection improvements: Westbound lanes modified from exclusive left turn lane and shared through/right lane to exclusive left, through and right turn lanes	Developer contributions and /or incremental tax	Prior to issuance of CO at 1.6 million sf (Net without parking)
2	New signal at Montrose Ave./ Route 1.	Developer contributions and /or incremental tax	Prior to issuance of CO at 1.6 million sf (Net without parking)
2	Improvements on Montrose Avenue and at the intersection of Montrose Avenue at East Glebe Road / Ashby Street	Developer contributions and /or incremental tax	Prior to issuance of CO at 1.6 million sf (Net without parking)
2	New Route 1 pedestrian crossing/signal between East Custis Avenue and East Glebe Road.	Developer contributions and /or incremental tax	Prior to issuance of CO at 2 million sf (Net without parking)
3	Route 1 / E. Glebe Intersection improvements (Phase 2 from the new North-South Road to LaVerne Avenue) or comparable improvement – in consultation with property owners.	Developer contributions and/ or incremental tax	Prior to issuance of CO at 2 million sf (Net without parking)
3	Modification of lane configuration at Route 1/Custis Ave.	Developer contributions and/ or incremental tax	Prior to issuance of CO at 2 million sf (Net without parking)

MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan Amendments

Chart 1: Public Benefits Phasing (2015)

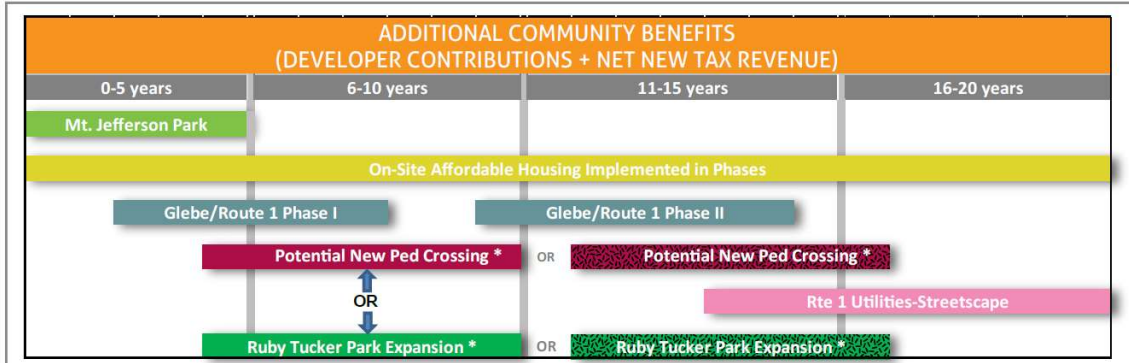
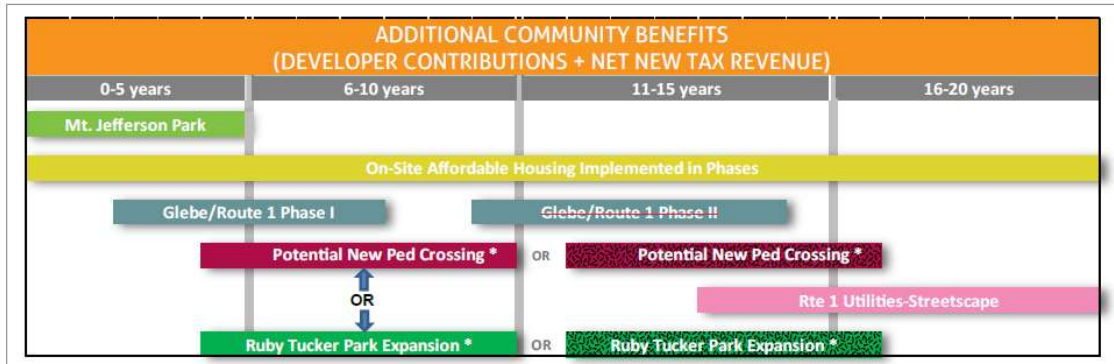


Chart 1: Public Benefits Phasing Proposed (2020)



MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan Amendments

Chart 2: Development Summary Table (2015)

District #	Block #	Applicable notes (see notes page below)	Residential				Office	Hotel	Retail and Maker		Maximum Total (sq. ft.)
			1a		1b		2	3	4	5	
			Multifamily Residential (max. GFA)	Multifamily Residential (max. units)	Townhouse (max. GFA) Includes garages. See Note 1	Townhouse (max. units)	Office (max. GFA)	Max. GFA	Retail (Primary / Secondary) (max. GFA)	Retail (Maker Space Required) (max. GFA) See Note c	
1	1	f	0	0	24,800	10	0	0	0	-	24,800
	2	f	0	0	24,800	10	0	0	0	-	24,800
	3	a b f	140,500	149	0	0	0	0	0	9,500	150,000
2	4	b k l m n	307,000	326	0	0	135,200	0	46,000	0	488,200
	5	k l m q	162,600	172	35,100	14	0	0	31,000	0	228,700
	6	k l m q r	377,400	399	31,000	14	0	0	81,700	0	490,100
	7	b k l m p	117,100	125	0	0	0	145,300	35,200	0	297,600
	8		0	0	44,900	18	0	0	0	0	44,900
3	9	d	0	0	0	0	0	0	0	21,500	21,500
	10		66,800	71	14,800	5	0	0	0	6,200	87,800
	11		75,600	80	0	0	0	0	0	19,200	94,800
	12		0	0	30,100	13	0	0	0	0	30,100
	13		80,000	85	0	0	0	0	0	27,900	107,900
	14		0	0	21,800	9	0	0	0	0	21,800
	15		0	0	28,000	11	0	0	0	0	28,000
	16	b	134,900	143	0	0	0	0	0	29,800	164,700
4	17	b	120,000	127	0	0	0	0	0	6,100	126,100
	18	f	0	0	14,800	5	0	0	0	-	14,800
	19		79,700	85	0	0	0	0	0	8,600	88,300
	20		68,200	72	0	0	0	0	0	5,900	74,100
	21		61,700	65	0	0	0	0	0	5,600	67,300
CDD Total			1,791,500	1,900	270,100	109	135,200	145,300	193,900	140,300	2,676,300

Chart 2: Development Summary Table (Proposed 2020 Amendment)

District #	Block #	Applicable notes (see notes page below)	Residential				Office	Hotel	Retail and Maker		Maximum Total (sq. ft.)
			1a		1b		2	3	4	5	
			Multifamily Residential (max. GFA)	Multifamily Residential (max. units)	Townhouse (max. GFA) Includes garages. See Note 1	Townhouse (max. units)	Office, Medical Care Facility (max. GFA)	Max. GFA	Retail (Primary / Secondary) (max. GFA)	Retail (Maker Space Required) (max. GFA) See Note c	
1	1	f	0	0	24,800	10	0	0	0	-	24,800
	2	f	0	0	24,800	10	0	0	0	-	24,800
	3	a b f	140,500	149	0	0	0	0	0	9,500	150,000
2	4	b k l m n	307,000 310,000	326 330	0	0	135,200 115,000	0	46,000 40,000	0	488,200 465,000
	5	k l m q	162,600 200,000	172 240	35,100 0	14 14	0	0	31,000 0	0	228,700 200,000
	6	k l m q r	377,400 0	399 0	31,000 250,000	14 90	0	0	81,700 0	0	490,100 250,000
	7	b k l m p	117,100 270,000	125 125	0	0	0	145,300	35,200 25,000	0	297,600 295,000
	8		0	0	44,900	18	0	0	0	0	44,900
3	9	d	0	0	0	0	0	0	0	21,500	21,500
	10		66,800	71	14,800	5	0	0	0	6,200	87,800
	11		75,600	80	0	0	0	0	0	19,200	94,800
	12		0	0	30,100	13	0	0	0	0	30,100
	13		80,000	85	0	0	0	0	0	27,900	107,900
	14		0	0	21,800	9	0	0	0	0	21,800
	15		0	0	28,000	11	0	0	0	0	28,000
	16	b	134,900	143	0	0	0	0	0	29,800	164,700
4	17	b	120,000	127	0	0	0	0	0	6,100	126,100
	18	f	0	0	14,800	5	0	0	0	-	14,800
	19		79,700	85	0	0	0	0	0	8,600	88,300
	20		68,200	72	0	0	0	0	0	5,900	74,100
	21		61,700	65	0	0	0	0	0	5,600	67,300
CDD Total			1,607,400 1,791,500	1,707 1,900	454,000 270,100	171 109	115,000 135,200	145,300	65,000 193,900	140,300	2,381,700 2,676,300

**MPA2020-00003, CDD2020-00003, ZTA2020-00006
Oakville Triangle Vision Plan & CDD Concept Design Plan Amendments**

2. This resolution shall be signed by the Chairman of the Planning Commission and attested by its secretary, and a true copy of this resolution forwarded and certified to the City Council.

ADOPTED the **1st** day of December 2020.



Chair, Alexandria Planning Commission

ATTEST: Karl W. Moritz

Karl Moritz, Secretary

ATTACHMENT #2

Affordable Housing Plan Oakville Triangle September 9, 2020

1.1 Oakville – 2610 Richmond Highway, 420 Swann Ave. 300 Swann Ave., 405 Swann Ave. 400 Fannon St., 2500 Oakville St., 2514 Richmond Highway, and 2412 Richmond Highway.

1.2 CDD 2020-0003

1.3 CDD Development shall consist of approximately:

- 65,000 GSF of Retail Use
- 780,000 GSF of Residential Use
- 90 Townhomes
- 115,000 of Medical Care Use
- 45,000 GSF of Service Use
- 155,000 GSF of Above Grade Parking

1.4 Special Use Permits/Zoning Modifications/Waivers include:

- CDD Concept Plan Special Use Permit
- Development Special Use Permit With Site Plan
- Transportation Management Plan
- Vacation of City Right of Way

1.5 The project is located within the Oakville Triangle/Route 1 Corridor. CDD 2020-003 shall include the 65 on-site affordable dwelling units as previously approved by Planning Commission and Council which is consistent with the CDD 2014-002 approval.

2.1 65 on-site affordable dwelling units consisting of 63 studio units and two (2) two-bedroom units offered for rent at 60% AMI affordability for a period of 35 years.

2.2 Affordable units shall be located in the multifamily residential rental buildings located on Blocks A, B, and C of the CDD plan. Specific unit allocations between buildings shall initially be determined at the time of DSUP submission for each individual block and may be transferred between blocks after completion of any construction. Affordable units are not specifically designated units within the structures.

2.3 Residents of affordable units shall have equal access to all amenities available to residents of market-rate units.

2.4 The project does not require demolition of any existing affordable units.

2.5 Tenant relocation plan is not applicable for this project.

2.6 In accordance with table in Section 2.8 below, it is anticipated that approximately 48 affordable units will be delivered in Phase 1 of the project consisting of 46 studio units and 2 two-bedroom units. The balance shall be delivered in Phase 2 consisting of the remaining 17 studio units. Phase 1 shall include Blocks A and B. Phase 2 shall include Block C. Final unit counts shall be determined at the time of DSUP submission for each individual block. All affordable units are to be in the multi-family rental product.

2.7 The project will not be subject to any additional voluntary contributions in addition to the provision of affordable units.

2.8 Conceptual Affordable Housing Unit Delivery Table

Phase of Development	Use	Total # Units ³	% of Total Residential Units by Phase	% of Affordable Units Provided in Each Phase	Total Affordable Units to Be Provided by Phase ¹
1	Block A: Residential - Multifamily	330	36%		
1	Block B: Residential - Multifamily	260	28%	74%	48
1	Block D: Residential - Townhome	90	10%		
2	Block C: Residential - Multifamily ²	240	26%	26%	17
	Total	920	100%	100%	65

Notes

- 1 All affordable units to be provided in Multifamily Residential Buildings. Required units at time of Phase 2 development may be provided via conversion of
- 2 existing market rate units located in Blocks A or B. Total number of units provided in each building as shown on this chart is as
- 3 expected at time of CDD plan submission. Actual number may vary.

ATTACHMENT #3

Oakville Triangle and Route 1 Corridor Vision Plan and Design Standards and Guidelines

List of Master Plan Changes

P.9:

- Amend the vision statement, as follows: The Plan envisions a future Oakville Triangle/Route 1 Corridor that is compatible with the fabric of existing neighborhoods, features an enhanced Mount Jefferson Park/Trail, and a blend of new and existing open spaces and land uses unified by an attractive urban streetscape. High quality architecture and urban design celebrate the area’s industrial heritage, and new development includes a diversity of residential, office, hotel, medical care facility, and regional and neighborhood-serving retail uses, as well as an emerging “maker” economy. New development capitalizes on nearby transit by concentrating height at transit stops and in close proximity to ~~the future~~ Potomac Yard Metrorail Station, while ensuring compatibility with surrounding neighborhoods. As an inclusive neighborhood, the area provides housing options affordable to a range of incomes and welcoming to different household types. Residents, workers, and visitors walk and bike within the Plan area and to adjoining neighborhoods.

P.11:

- Amend the second column of text, as follows: New development will be required to provide usable ground level public open spaces, including, but not limited to, ~~three new public open spaces~~ a new .7 acre central public open space within Oakville Triangle to build on and visually connect to the Mount Jefferson Park, as well as expansion of the existing Ruby Tucker Park to create a larger public open space in the northern portion of the Plan area. ~~A new hardcape plaza~~ new central open space in Oakville Triangle will provide a public gathering space for daily enjoyment and special events for residents and visitors, new and old. In addition, larger projects will provide rooftop amenity space to ensure adequate opportunity for new residents’ access to the outdoors, designed to be compatible with and to minimize light and noise impacts on the adjoining neighborhoods.

P.14:

- Amend Recommendation 1.13, as follows: A pedestrian and bike connection between Swann Avenue and Stewart Avenue to provide connectivity between the neighborhoods and the future Potomac Yard Metrorail station. ~~The bike lane within the central urban plaza within the Oakville Triangle site will be designed as a shared space.~~
- Amend Recommendation 1.17, as follows: ~~An additional signalized pedestrian crossing across Route 1 between East Custis Avenue and East Glebe Road to improve pedestrian access between Oakville Triangle and Del Ray/Lynhaven and the future Potomac Yard Metrorail station.~~ An additional signalized crossing across Route 1 at Fannon Street, including pedestrian and intersection improvements, to improve pedestrian access between Oakville Triangle and Del Ray/Lynhaven and the Potomac Yard Metrorail station and provide efficient access to the medical care facility.

P.17:

- Amend Recommendation 1.13, as follows: A pedestrian and bike connection between Swann Avenue and Stewart Avenue to provide connectivity between the neighborhoods and the future Potomac Yard Metrorail station. ~~The bike lane within the central urban plaza within the Oakville Triangle site will be designed as a shared space.~~

P.18:

- Amend Recommendation 1.17, as follows: ~~An additional signalized pedestrian crossing across Route 1 between East Custis Avenue and East Glebe Road to improve pedestrian access between Oakville Triangle and Del Ray/Lynhaven and the future Potomac Yard Metrorail station.~~ An additional signalized crossing across Route 1 at Fannon Street, including pedestrian and intersection improvements, to improve pedestrian access between Oakville Triangle and Del Ray/Lynhaven and the Potomac Yard Metrorail station and provide efficient access to the medical care facility.

P.19:

- Add as a new bullet: Installation of traffic signal, pedestrian, and intersection improvements at Fannon Street and Route 1.

P.22:

- Amend Note 1, as follows: 1. Gross Floor Area (GFA) is defined as the sum of all gross horizontal areas under a roof or roofs. These areas are measured from the exterior faces of walls or from the centerline of party walls. Elevator and stair bulkheads, multi-story atriums and similar volumetric construction, not involving floor space are excluded. Below grade parking structure is excluded. See notes K and L on page 23 for blocks 4-7.
- Amend Note 4, as follows: 4. For lots identified in the Plan with maker spaces on all four street-facing facades, if not feasible to provide maker spaces on all four sides, a minimum of 3 street-facing facades shall be used to provide maker spaces.
- Amend Note 10, as follows: 10. Conversion rate from GFA sf to dwelling unit = 940 sf The final number of units to be determined at time of DSUP.
- Add Note 12, as follows: 12. Hotel use may be permitted within the Plan area. The final location shall be determined during the DSUP process.

P.23:

- Amend Note a, as follows: a. Based on heights in the approved Oakville Triangle & Route 1 Corridor Vision Plan, as amended October 2015. If greater heights are allowed, maximums may be increased to the approval of PC & CC.
- Amend Note m, as follows: m. Development (sf) based on Preliminary CDD Concept Plan ~~CDD2014-0002-submission-10/29/15~~ as amended.

- Amend Note q, as follows: q. ~~Townhouses shall be liner units or as generally depicted per the approved CDD Concept Plan CDD2014-0002~~ Final design and configuration of townhouses will be determined during the DSUP process.
- Amend Note r, as follows: r. As per all blocks, required retail identified in the plan at ground floor locations is required. Non-ground floor retail may be converted to an alternate use through the DSUP process.

P.25:

- Amend the first column header, as follows: PHASE 1* - OPERATIONAL PRIOR TO 800,000 SQ. FT. (NET WITHOUT PARKING)
- Amend the first note in the first column, as follows: 1 - Signal modifications at Route 1 and E. Reed Ave* (with North Potomac Yard first building)
- Add a note to the first column, as follows: 8 - New signalized intersection improvements and pedestrian crossings at Route 1/Fannon Street
- Amend the fifth note in the second column, as follows: 8 - Pedestrian crossing across Route 1 ~~(at Fannon St. or Raymond Ave)~~
- Delete the first note in the third column, as follows: 9 - ~~Improvements at Route 1/ E. Glebe (Phase 2)~~

P.26:

- Amend the last row of text in the chart of Non-Transportation Improvements, as follows: Open Space: Consideration in the Plan's phasing ensures that sufficient, consolidated, high quality open space is provided based on planned development. On-site open space is required for the Oakville site consistent with the Plan. Significant on-site open spaces ~~(e.g. Swann Plaza)~~ are required to be provided with adjacent development blocks.

P.27:

- Amend the first row of text in the chart of Non-Transportation Improvements, as updated. Timing for initiation and completion of park improvements is specified in the CDP for the Oakville Triangle site. Improvements shall be fully constructed ~~and accepted by the City~~ in accordance with CDD conditions prior to issuance of first Certificate of Occupancy for any building in the CDP area, or within a maximum 24 months of issuance of permit to demolish any building or structure in or adjacent to Park (whichever is earlier).

P.32:

- Amend recommendation 2.5, as follows: Provide a ~~central public urban plaza~~ new central public open space within Oakville Triangle that includes usable amenities that help to meet the recreational needs of new residents.
- Amend recommendation 2.6, as follows: Provide ~~five~~ four new ground level public green spaces adjacent to Mount Jefferson Park.

P.37:

- Amend the column of text, as follows: Establishing the street grid will visually unify this approximately 15-acre district. The defining elements of this neighborhood will be the adjoining Mount Jefferson Park frontage and the centrally located ~~public urban plaza ± 31,200 sq. ft. public open space~~. The building height and setback of the buildings have been designed to step down to the adjoining Mount Jefferson Park and single-family homes to the west. The centrally located ~~±21,000 sq. ft. public open space~~ will need to be configured to accommodate events, ~~and programming, and social gatherings~~. ~~Swann Avenue is intended to provide retail shopping through the provision of a significant amount of ground floor retail~~. Calvert Avenue is intended to be an area where some of the existing neighborhood-serving or “maker uses” could be located. Calvert Avenue is also intended to be more “industrial” in design and character. Park Road is intended to be smaller scale 30’-45’ tall townhouses with front porches and architectural elements and materials compatible with Del Ray. The district is intended to be a mix of building types and uses ranging from townhouses, multi-family, office, hotel buildings, medical care facility, and retail and maker space.

P.51:

- Amend Standard 41, as follows: The intersection of Glebe Road and Route 1 will need to be reconfigured as generally depicted within the attached cross-sections. ~~The timing and phasing of the improvements will be established as part of the CDD zoning for the Plan area~~. Funding will be proposed as part of the Oakville Plan CDD for the improvements.
- Amend Standard 4.5iii, as follows: A maximum of one curb cut per block face shall be permitted on each side of the street. To the extent possible, curb cuts should be aligned with curb cuts on the opposite side of the street. Offset curbs may be permitted, where it is deemed necessary. Additional curb cuts may be considered as part of the DSUP process.

P.56:

- Amend the text under Active Uses in the third column, as follows: Active uses are specified on the ground level or frontages of many blocks in the Plan area to ensure a vibrant public realm. For purposes of the Design Standards, active uses shall be residential, office, medical care facility, retail, maker, and/or hotel uses.

P.57:

- Amend the header in the first column of text, as follows: ~~PRIMARY RETAIL:~~
- Amend the first column of text, as follows: For purposes of the Design Standards, primary retail is intended as retail uses and ~~restaurants~~ personal service uses. Final uses and requirements are specified with the CDD zoning ~~for the Plan area and on pg 58~~.
- Delete the header and second column of text, as follows: ~~SECONDARY RETAIL:~~

- For purposes of the Design Standards, ~~secondary retail is a combination of retail, maker uses and personal service uses. Final uses and requirements are specified with the CDD zoning for the Plan area and on pg 58.~~

P.58:

- Amend the header in the first column of text, as follows: ~~PRIMARY RETAIL~~
- Amend the Intent text in the first column, as follows: Intent: For the purposes of the Oakville Triangle/Route 1 Corridor Plan Area, the areas designated for “~~primary retail~~” are intended to achieve an experiential environment, where shopping, dining, and other establishments engage pedestrians and activate the street front with attractive, unique storefronts. Generally, this includes restaurants, personal service uses, and retail as defined in the CDD. ~~As shown on the Land Uses Map, the area designated for primary retail is Swann Avenue within Oakville Triangle, with some additional primary retail designated for north and south of Swann on Oakville Street and Route 1. Examples of Primary Retail include restaurants, coffee shops, bakeries, gourmet food, home and fashion retail, and interactive establishments such as cooking or other experiential schools.~~
- Amend the Allowed Uses text in the first column of text, as follows: Allowed Uses: For the purposes of the Plan area, Primary Retail is defined to include:
 - Restaurants
 - Personal service uses
 - Retail shopping establishments ~~(excluding uses detailed below)~~
- Amend the Excluded Uses in the second column of text, as follows: ~~Excluded Uses: For the purposes of the Plan, the following uses are considered incompatible with the intent of Primary Retail:~~
 - ~~Excluded retail shopping establishments: Appliances, Auto parts without service or installation on premises, Drugstores, lawn and garden supplies, Variety Goods~~
 - ~~Personal service establishments~~
- Delete the header and the second column of text, as follows: ~~SECONDARY RETAIL~~
- ~~Intent: For the purposes of the Oakville Triangle/Route 1 Corridor Plan Area, the areas designated for “secondary retail” are intended to achieve a neighborhood-serving and personal services retail environment, where frequent shopping, repair, or service needs can be met. The main area designated for secondary retail is on Calvert Avenue, where there is also a significant degree of overlap with Maker Space Use (Figure 7).~~
- Delete the text in the second and third columns, as follows: ~~Allowed Uses: For the purposes of the Plan area, Secondary Retail is defined to include:~~
 - ~~Retail~~
 - ~~Personal service uses (excluding uses detailed below) Amusement enterprises~~
 - ~~Restaurants~~

~~Additional Uses: Other uses consistent with the neighborhood-serving intent of the Plan herein, may be allowed at the discretion of the Director of Planning and Zoning.~~

~~Excluded Uses: For the purposes of the Plan area, Secondary Retail may not include the following:~~

- ~~○ Retail shopping establishments shall not include appliance stores and auto parts stores~~
- ~~○ Personal service uses shall not include appliance repair and rental, contractors' offices, laundromats, and pawnshops~~

P.66:

- Delete the second and third bullet in the second column of text, as follows:
 - ~~Five additional open spaces adjacent to Mount Jefferson Park, ranging in size from ±4,400 sq. ft. to ± 21,000 sq. ft.~~
 - ~~: A ± 21,000 sq. ft. central urban square within the Oakville Triangle.~~
- Add fourth bullet in the second column of text, as follows: A central .7 acre public park in Oakville Triangle.
- Amend the second bullet in the third column of text, as follows: Mixed-use projects (with ground floor retail - commercial) will include a minimum of 15% 20% ground level open space and 25% roof-top amenity space as part of redevelopment with a total of 40% on-site open space to include rooftop-amenity space as part of redevelopment.

P.67:

- Amend the header in the first column of text, as follows: OAKVILLE URBAN PLAZA CENTRAL OPEN SPACE
- Amend the text in the first and second columns, as follows: The central urban plaza will be hardscape with appropriate plantings, shade options, and lighting. High-quality materials and finishes, as well as the inclusion of public art or other focal features, ensure this space will be a great amenity. The plaza will be a shared space for pedestrians and bikes. The central .7 acre open space is intended to be a combination of multiple outdoor "Rooms" for multigenerational programing, including but not limited to an outdoor meeting space, lawn area, children's play area and sustainability features and public art. The materiality of the park will also incorporate the industrial heritage of the site through materials such as wood, stone, weathered metal, etc.

P.69:

- Amend text, as follows: These open spaces will range from front yards and courtyards to a new public open spaces and an urban plaza on Swann Avenue.

P.70:

- Amend Standard 4.18, as follows: ~~The five new open spaces adjacent to Mount Jefferson Park shall be constructed as generally depicted in Figure 30, as part of the redevelopment adjacent to the Mount Jefferson Park. The at grade trail (lower trail) extension on the northern portion shall be constructed by the adjoining property owner.~~

- Amend Standard 4.19, as follows: The central open space within Oakville Triangle shall ~~be hardscape with~~ have appropriate plantings, shade options, and lighting and shall contain high-quality materials and finishes, as well as the inclusion of public art or other focal features. The open space/~~plaza~~ shall be accessible to the public through the provision of a public access easement.
- Amend Standard 4.23, as follows: New mixed-use projects (with ground floor retail-commercial) shall provide a minimum of ~~15%~~ 20% ground level open space ~~and 25% roof-top amenity space.~~ with a total of 40% on-site open space to include rooftop-amenity space as part of redevelopment.

P.74:

- Amend Standard 5.2, as follows: The streetwall shall generally be parallel to the street and at a minimum height as required herein. The streetwall for each block frontage shall be a minimum of 75% for residential, medical care facility, and office and hotel. For residential buildings where courtyards are provided, the streetwall shall be a minimum of 50%. A courtyard shall be limited to a maximum of two block frontage. Ground floor retail and maker uses shall provide a minimum of 85% continual streetwall.

P.81:

- Amend Standard 5.25, as follows: A minimum of 50% of each group of townhouses for each project shall provide a front porch. The porches shall comply with the applicable requirements herein. Final location of porches will be determined during the DSUP process.

P.99:

- Amend the title of Figure 41, as follows: Figure 41: Ground Floor Uses: ~~Primary, Secondary~~ Retail, and Maker Spaces

P.113:

- Amend Standard 8.3, as follows: All streets within the Plan are intended to be public streets, dedicated to the City or private with public access easements. ~~unless otherwise noted the property line is assumed to be at the edge of the right-of-way (ROW) as defined herein.~~

P.121:

- Amend the Typology header text, as follows: TYPOLOGY: ~~SHARED STREET~~ NEIGHBORHOOD RESIDENTIAL

P. 130:

- Amend recommendation 1.13, as follows: A pedestrian and bike connection between Swann Avenue and Stewart Avenue to provide connectivity between the neighborhoods

and the future Potomac Yard Metrorail station. ~~The bike lane within the central urban plaza within the Oakville Triangle site will be designed as a shared space.~~

- Amend recommendation 1.17, as follows: ~~An additional signalized pedestrian crossing across Route 1 between East Custis Avenue and East Glebe Road to improve pedestrian access between Oakville Triangle and Del Ray/ Lynhaven and the future Potomac Yard Metrorail station.~~ An additional signalized crossing across Route 1 at Fannon Street, including pedestrian and intersection improvements, to improve pedestrian access between Oakville Triangle and Del Ray/Lynhaven and the Potomac Yard Metrorail station and provide efficient access to the medical care facility.

P.131:

- Amend Standard 2.5, as follows: Provide a new central public urban plaza open space within Oakville Triangle that includes usable amenities that help to meet the recreational needs of new residents.
- Amend Standard 2.6, as follows: Provide ~~five~~ four new ground level public green spaces adjacent to Mount Jefferson Park.

P. 132:

- Amend Standard 4.1, as follows: The intersection of Glebe Road and Route 1 will need to be reconfigured as generally depicted within the attached cross-sections. ~~The timing and phasing of the improvements will be established as part of the CDD zoning for the Plan area.~~ Funding will be proposed as part of the Oakville Plan CDD for the improvements.
- Amend Standard 4.5iii, as follows: A maximum of one curb cut per block face shall be permitted on each side of the street. To the extent possible, curb cuts should be aligned with curb cuts on the opposite side of the street. Offset curbs may be permitted, where it is deemed necessary. Additional curb cuts may be considered as part of the DSUP process.

P.133:

- Amend Standard 4.18, as follows: The ~~five~~ new open spaces adjacent to Mount Jefferson Park shall be constructed as generally depicted in Figure 30, as part of the redevelopment adjacent to the Mount Jefferson Park. The at grade trail (lower trail) extension on the northern portion shall be constructed by the adjoining property owner.
- Amend Standard 4.19, as follows: The central open space within Oakville Triangle shall ~~be hardscape with~~ have appropriate plantings, shade options, and lighting and shall contain high-quality materials and finishes, as well as the inclusion of public art or other focal features. The open space/~~plaza~~ shall be accessible to the public through the provision of a public access easement.

P.134:

- Amend Standard 4.23, as follows: New mixed-use projects (with ground floor retail-commercial) shall provide a minimum of ~~15%~~ 20% ground level open space ~~and 25% roof-top amenity space.~~ with a total of 40% on-site open space to include rooftop-amenity space as part of redevelopment.
- Amend Standard 4.34, as follows: As part of the new multi-family, office, medical care facility, or hotel buildings, explore providing a community meeting space within each building.

P.135:

- Amend Standard 5.2, as follows: The streetwall shall generally be parallel to the street and at a minimum height as required herein. The streetwall for each block frontage shall be a minimum of 75% for residential, medical care facility, and office and hotel. For residential buildings where courtyards are provided, the streetwall shall be a minimum of 50%. A courtyard shall be limited to a maximum of two block frontage. Ground floor retail and maker uses shall provide a minimum of 85% continual streetwall.

P.137:

- Amend Standard 5.25, as follows: A minimum of 50% of each group of townhouses for each project shall provide a front porch. The porches shall comply with the applicable requirements herein. Final location of porches will be determined during the DSUP process.

P.146:

- Amend Standard 8.3, as follows: All streets within the Plan are intended to be public streets, dedicated to the City or private with public access easements. ~~unless otherwise noted the property line is assumed to be at the edge of the right-of-way (ROW) as defined herein.~~

Amended Figures:

- Figure 3 & 18: Illustrative Plan
- Figure 7: Oakville Triangle District
- Figure 19: Framework Streets
- Figure 25: Land Uses
- Figure 27: Maximum Building Heights
- Figure 28: Minimum Building Heights
- Figure 29: Open Spaces (Public, Ground level and Roof-top)
- Figure 30: Open Spaces adjacent to Mount Jefferson Park
- Figure 41: Ground Floor Uses: ~~Primary, Secondary~~ Retail, and Maker Spaces
- Figure 47: New and Reconfigured Streets
- Street Cross-Sections: Pages 117 - 125
- P.24: Transportation Improvements Table
- Chart 1: Public Benefits Phasing
- Chart 2: Development Summary Table

Figures 3 & 18: Illustrative Plan

Approved 2015 Plan as amended



Proposed 2020 Amendment

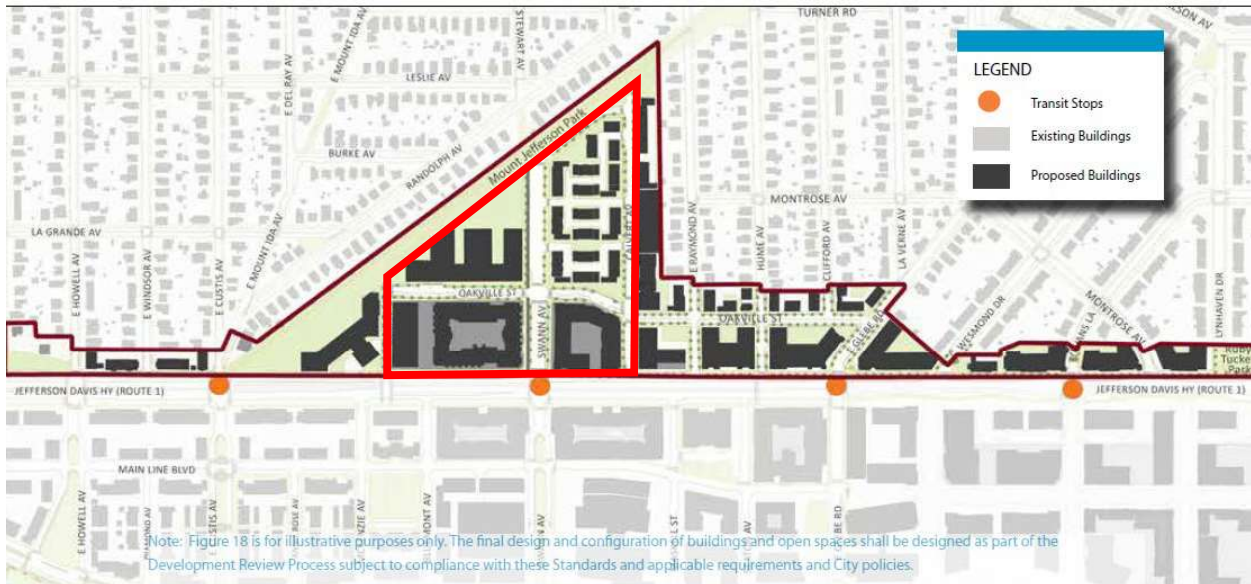
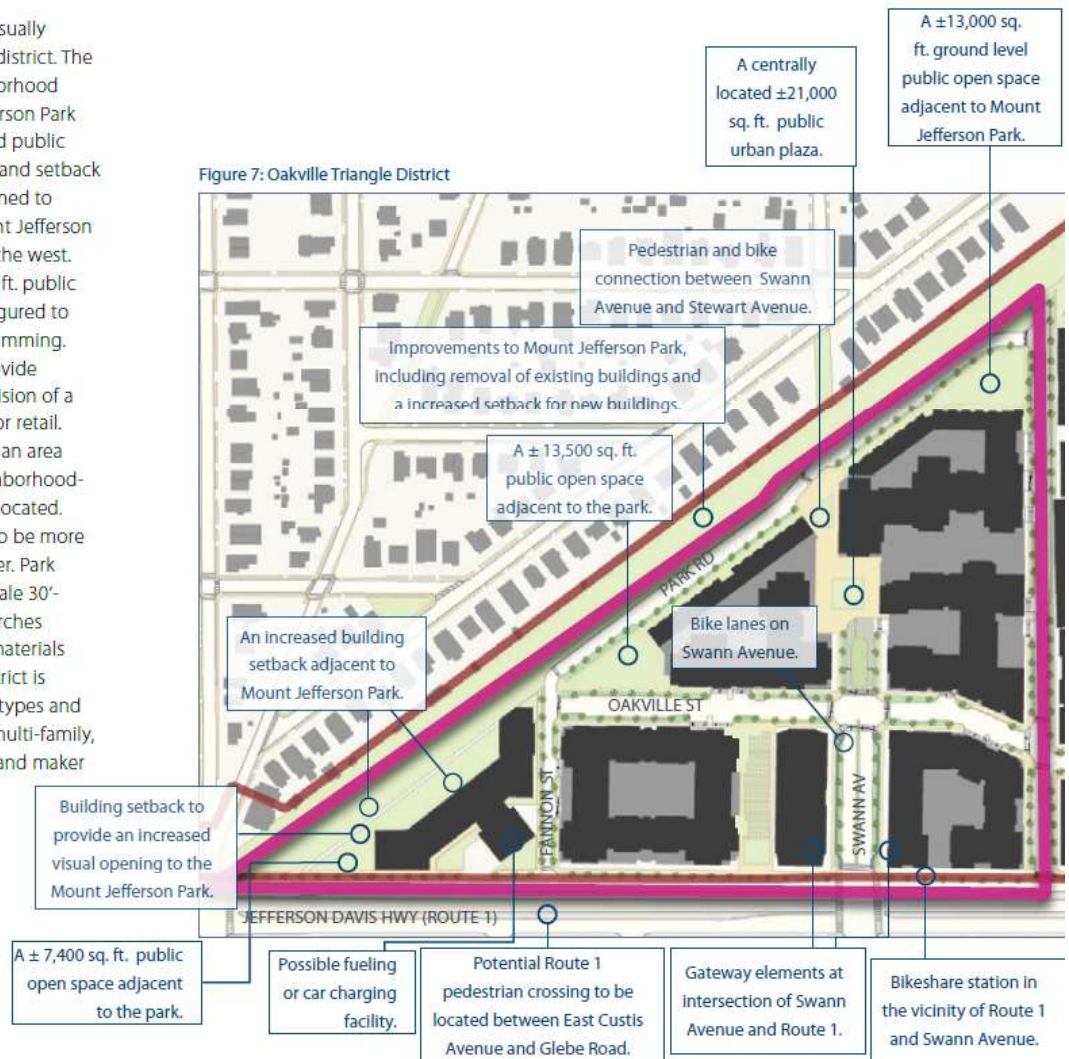


Figure 7: Oakville Triangle District

Approved 2015 Plan as amended

B. DISTRICT 2 - OAKVILLE TRIANGLE

Establishing the street grid will visually unify this approximately 15-acre district. The defining elements of this neighborhood will be the adjoining Mount Jefferson Park frontage and the centrally located public urban plaza. The building height and setback of the buildings have been designed to step down to the adjoining Mount Jefferson Park and single-family homes to the west. The centrally located $\pm 21,000$ sq. ft. public open space will need to be configured to accommodate events and programming. Swann Avenue is intended to provide retail shopping through the provision of a significant amount of ground floor retail. Calvert Avenue is intended to be an area where some of the existing neighborhood-serving or "maker uses" could be located. Calvert Avenue is also intended to be more "industrial" in design and character. Park Road is intended to be smaller scale 30'-45' tall townhouses with front porches and architectural elements and materials compatible with Del Ray. The district is intended to be a mix of building types and uses ranging from townhouses, multi-family, office, hotel buildings, and retail and maker space.



Proposed 2020 Amendment

B. DISTRICT 2 - OAKVILLE TRIANGLE

Establishing the street grid will visually unify this approximately 15-acre district. The defining elements of this neighborhood will be the adjoining Mount Jefferson Park frontage and the centrally located public urban plaza $\pm 31,200$ sq. ft. public open space. The building height and setback of the buildings have been designed to step down to the adjoining Mount Jefferson Park and single family homes to the west. The centrally located $\pm 21,000$ sq. ft. public open space will need to be configured to accommodate events, and programming, and social gatherings. Swann Avenue is intended to provide retail shopping through the provision of a significant amount of ground floor retail. Calvert Avenue is intended to be an area where some of the existing neighborhood-serving or "maker uses" could be located. Calvert Avenue is also intended to be more "industrial" in design and character. Park Road is intended to be smaller scale 30'-45' tall townhouses with front porches and architectural elements and materials compatible with Del Ray. The district is intended to be a mix of building types and uses ranging from townhouses, multi-family, office, hotel buildings, *medical care facility*, and retail and maker space.

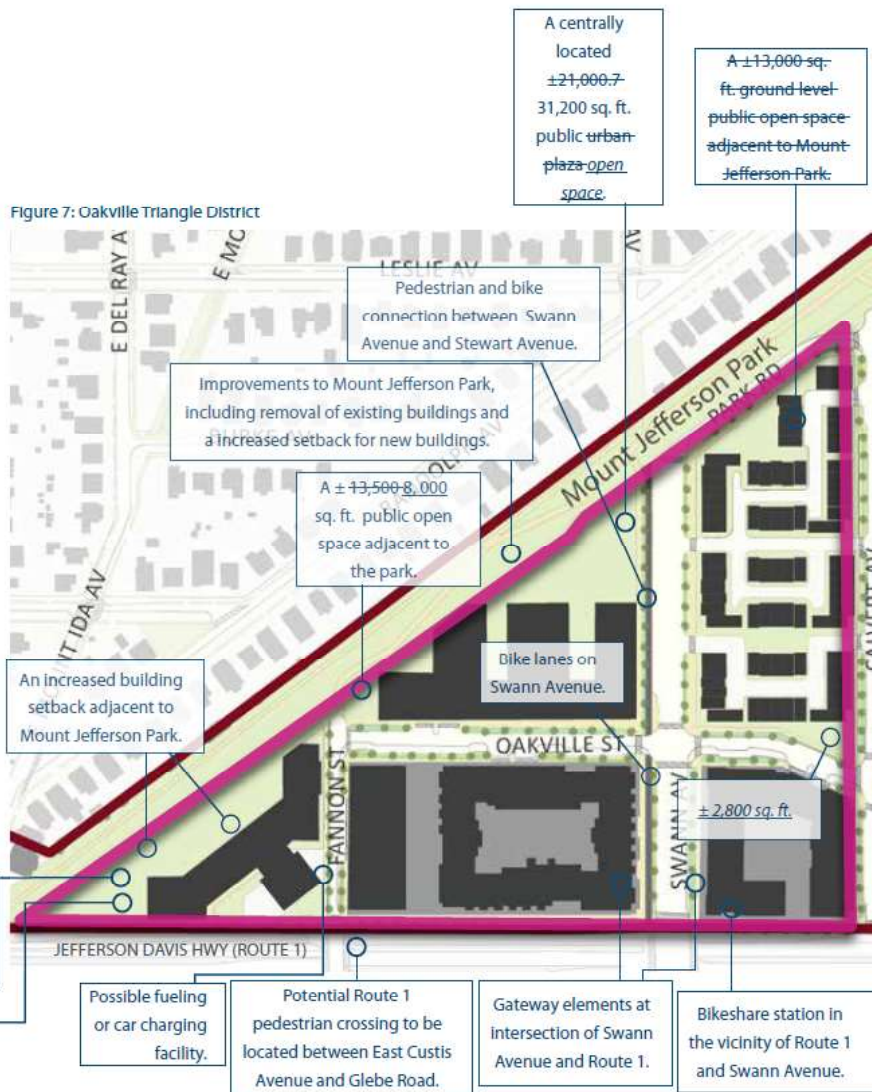


Figure 19: Framework Streets

Approved 2015 Plan as amended



Proposed 2020 Amendment



Figure 25: Land Uses

Note: Only portion of the figure is shown below.

Approved 2015 Plan as amended



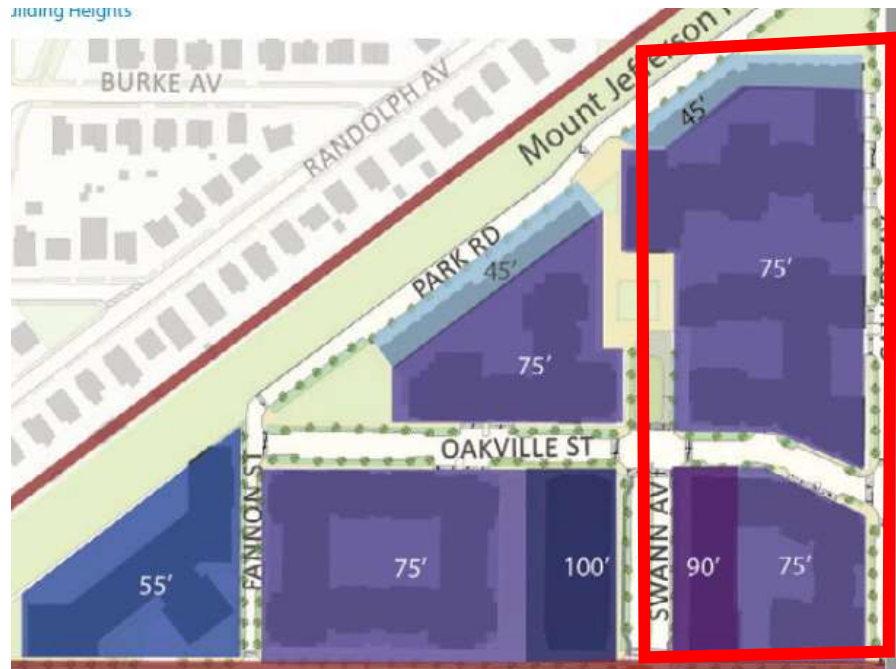
Proposed 2020 Amendments



Figure 27: Maximum Building Heights

Note: Only portion of the figure is shown below

Approved 2015 Plan as amended



Proposed 2020 Amendment



Figure 28: Minimum Building Heights

Note: Only portion of the figure is shown below

Approved 2015 Plan as amended



Proposed 2020 Height Amendments



Figure 29: Open Spaces (Public, Ground level and Roof-top)

Approved 2015 Plan as amended

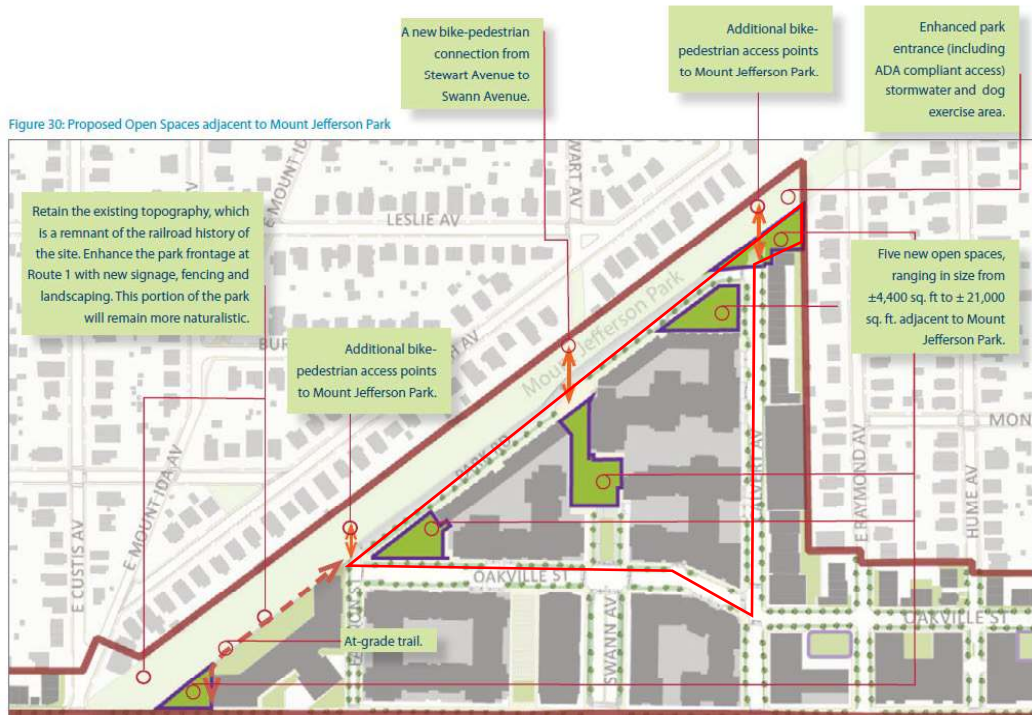


Proposed 2020 Amendment



Figure 30: Open Spaces adjacent to Mount Jefferson Park

Approved 2015 Plan as amended



Proposed 2020 Amendment

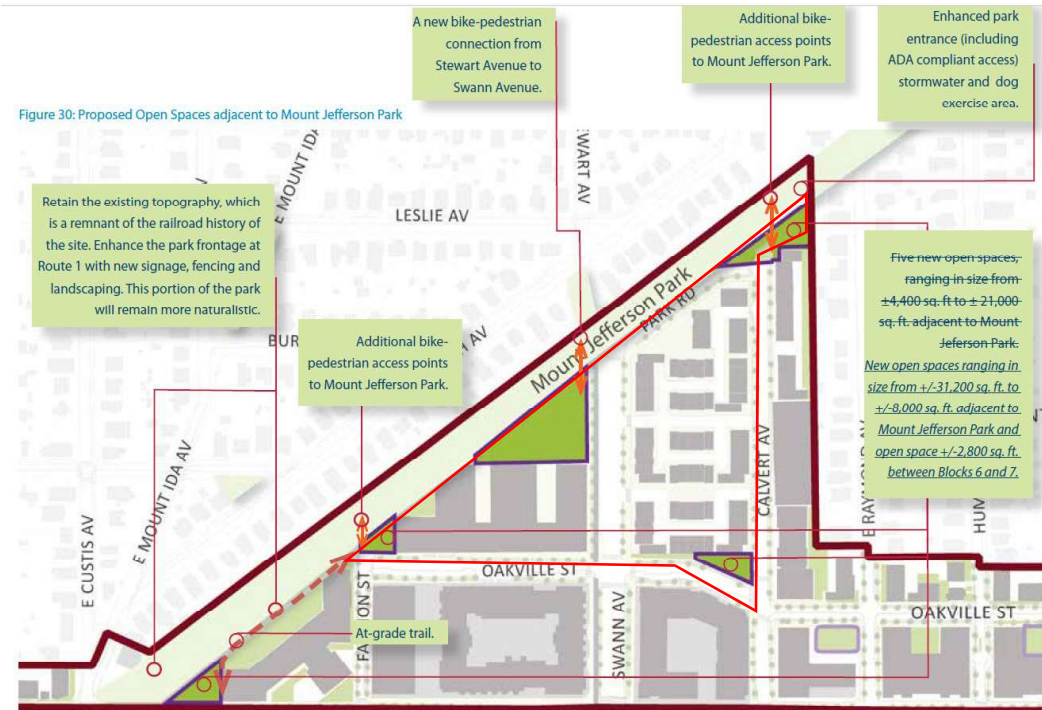


Figure 41: Ground Floor Uses: Primary, Secondary Retail, and Maker Spaces

Approved 2015 Plan as amended



Proposed 2020 Amendment



Figure 47: New and Reconfigured Streets

Note: Only portion of the figure is shown below

Approved 2015 Plan as amended

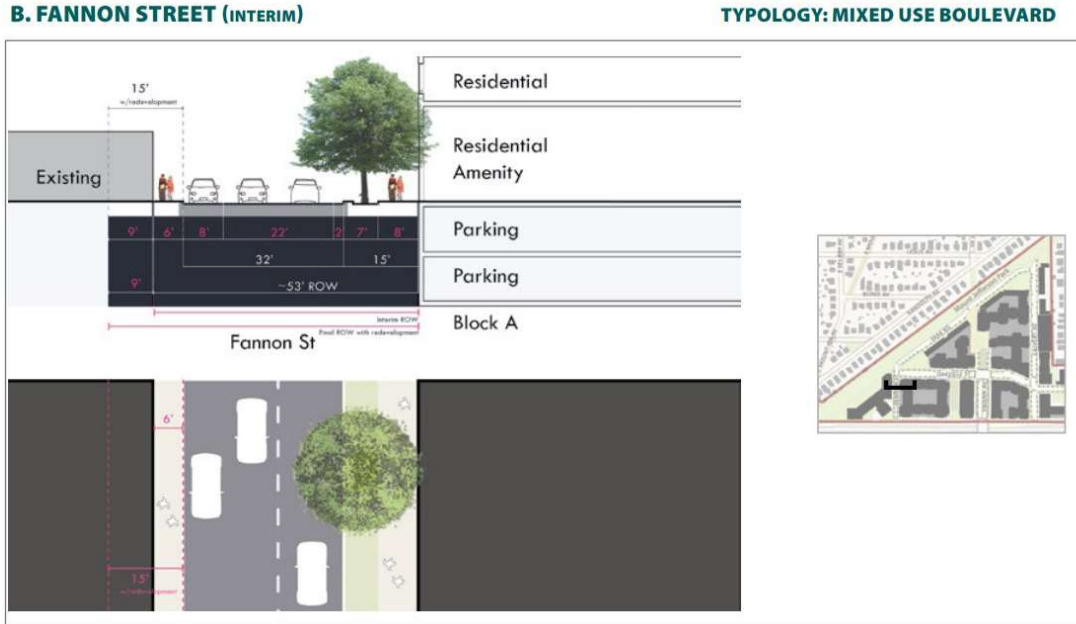


Proposed 2020 Amendment

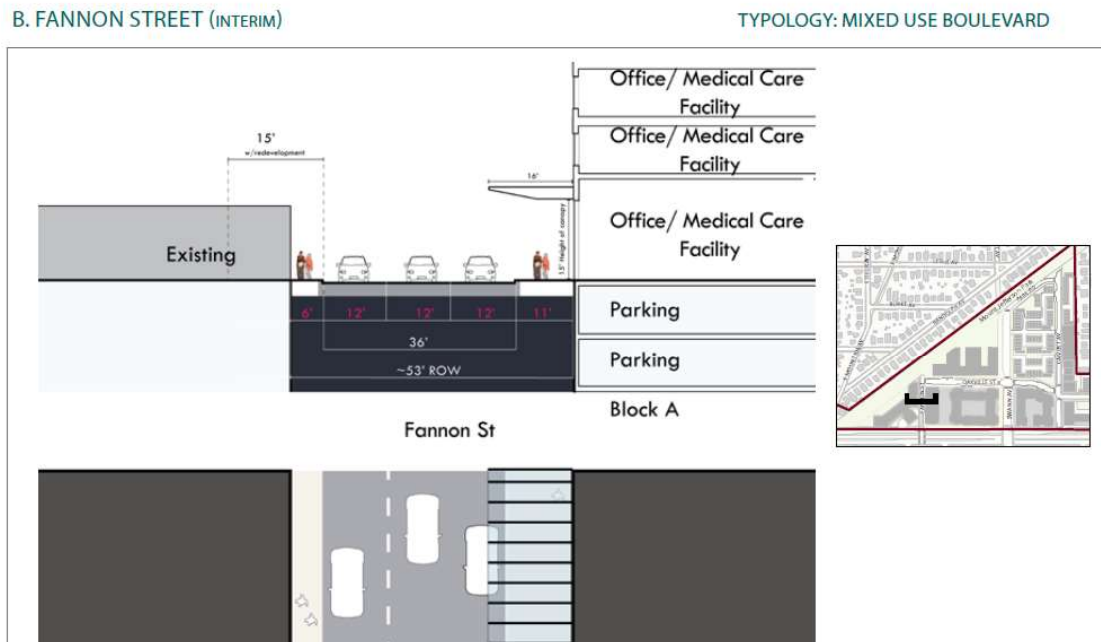


Street Cross Section Updates: Pages 117 – 125

p.117 Fannon Street (Interim) (2015)



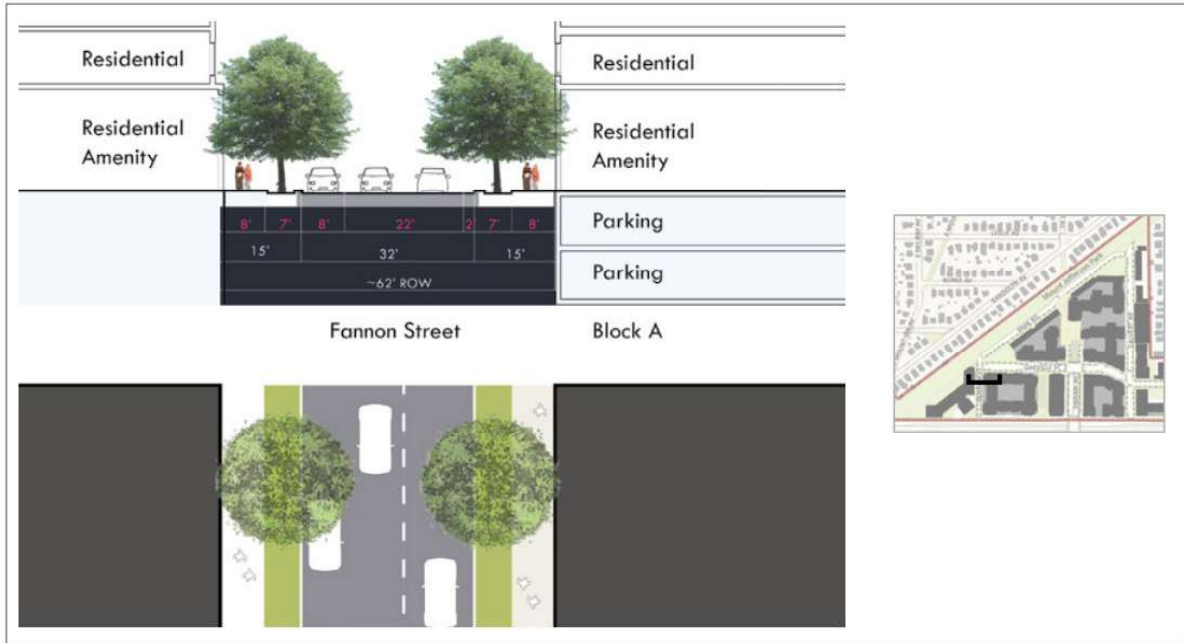
p.117 Fannon Street (Interim) (Proposed)



p. 118 Fannon Street (with Redevelopment) (2015)

FANNON STREET (WITH REDEVELOPMENT)

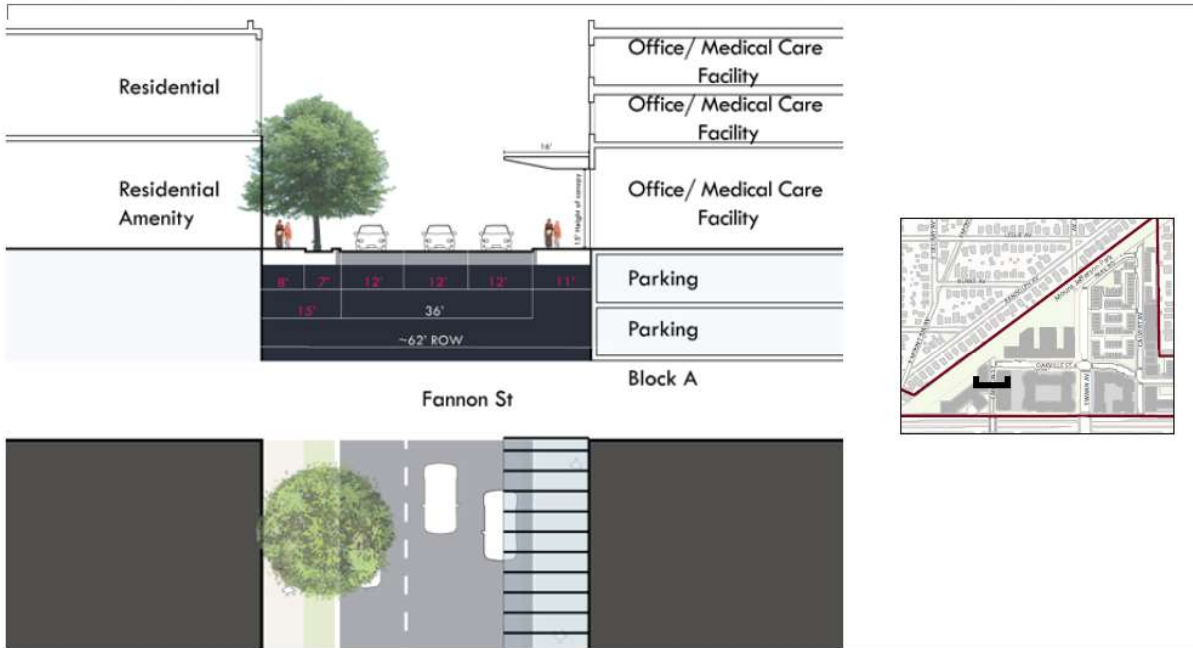
TPOLOGY: MIXED USE BOULEVARD



p.118 (Proposed)

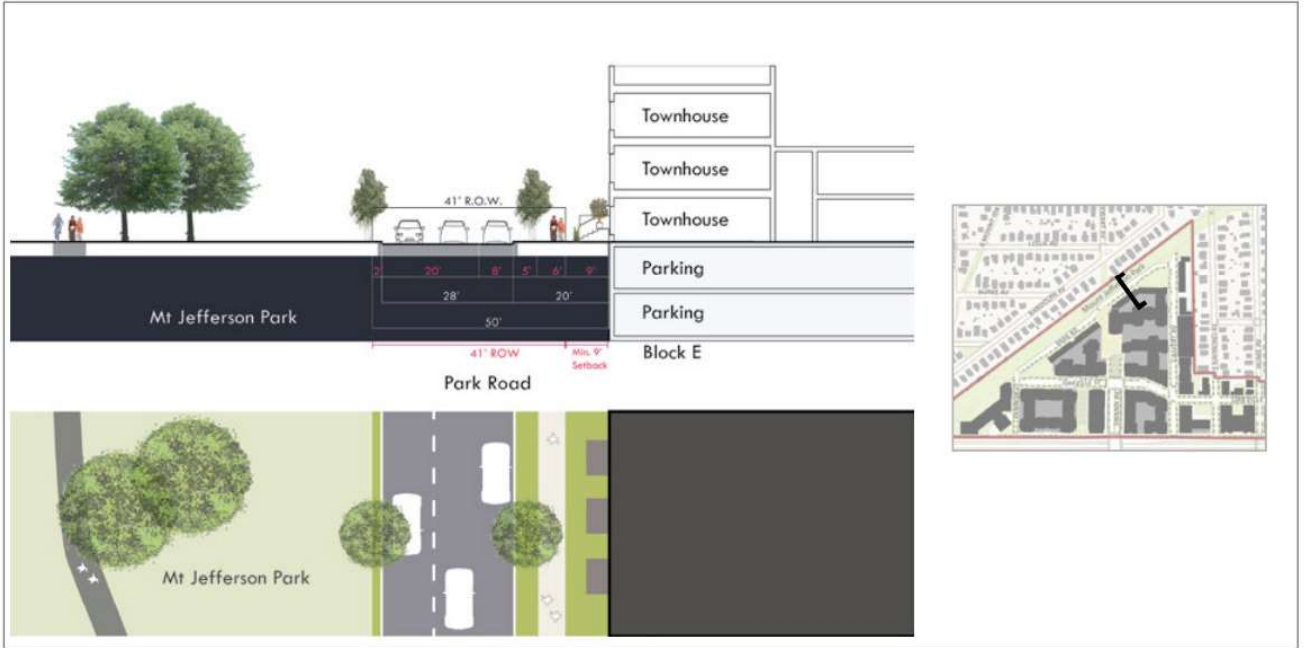
FANNON STREET (WITH REDEVELOPMENT)

TPOLOGY: MIXED USE BOULEVARD



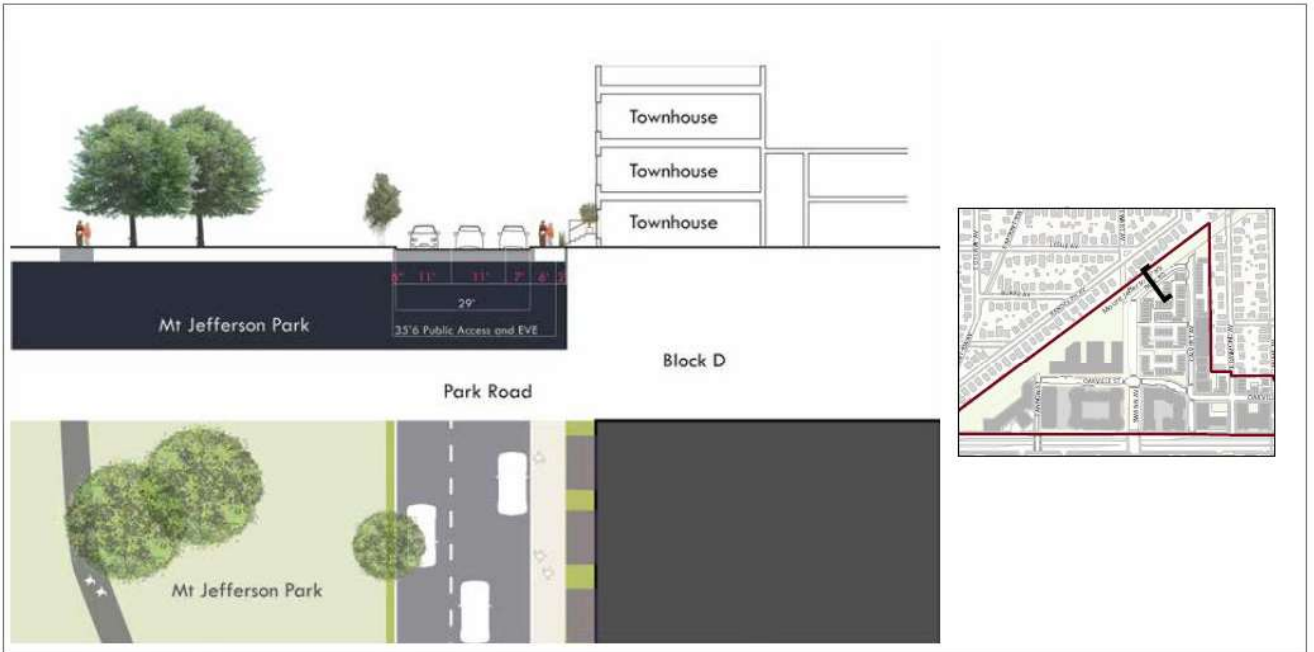
C. PARK ROAD

TYPOLOGY: PARK ROAD



C. PARK ROAD

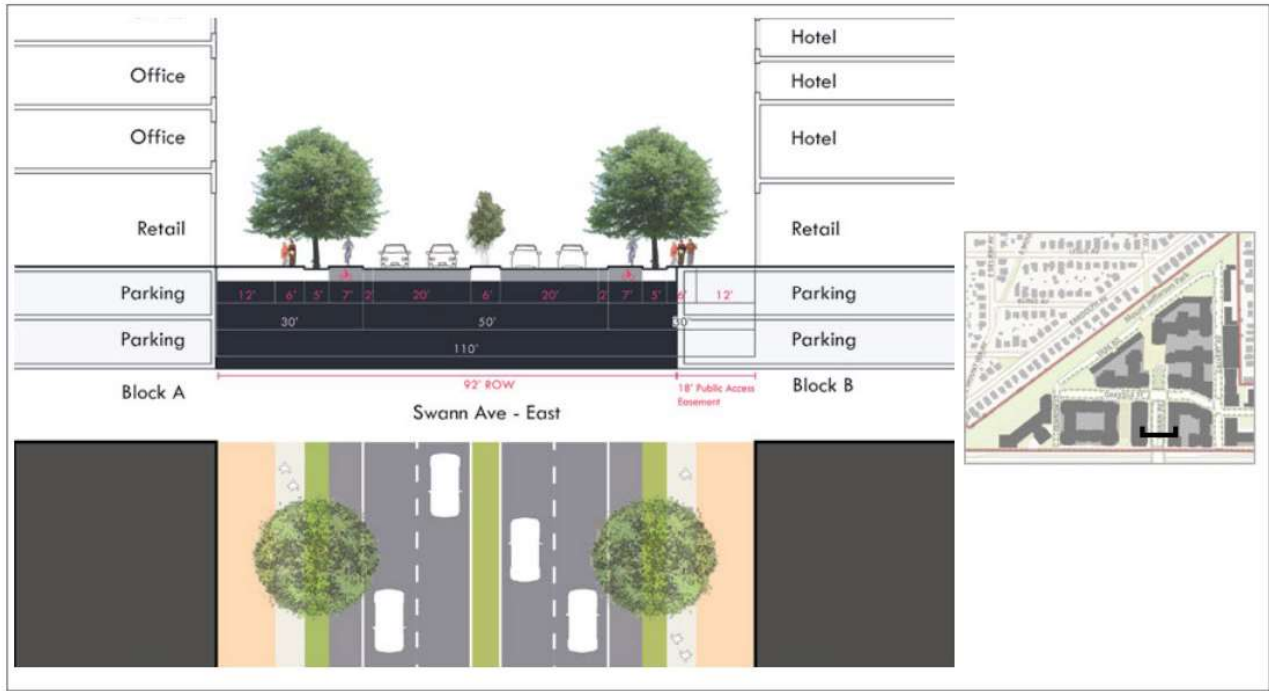
TYPOLOGY: PARK ROAD



p. 120 (2015)

D. SWANN AVENUE (EAST)

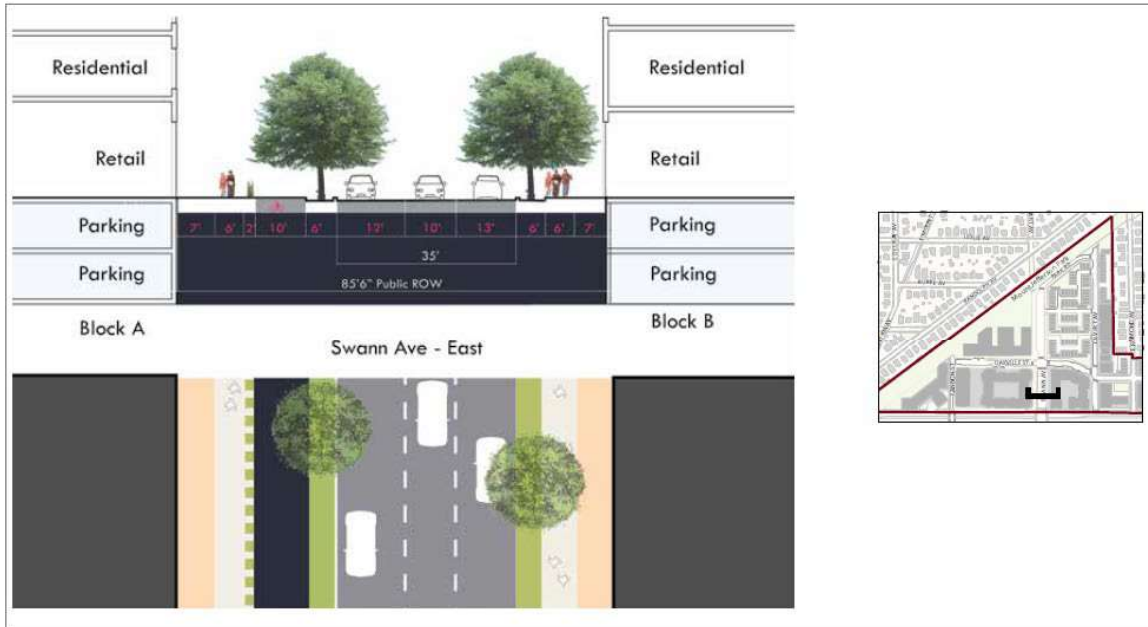
TYOLOGY: MIXED USE BOULEVARD



p. 120 (Proposed)

D. SWANN AVENUE (EAST)

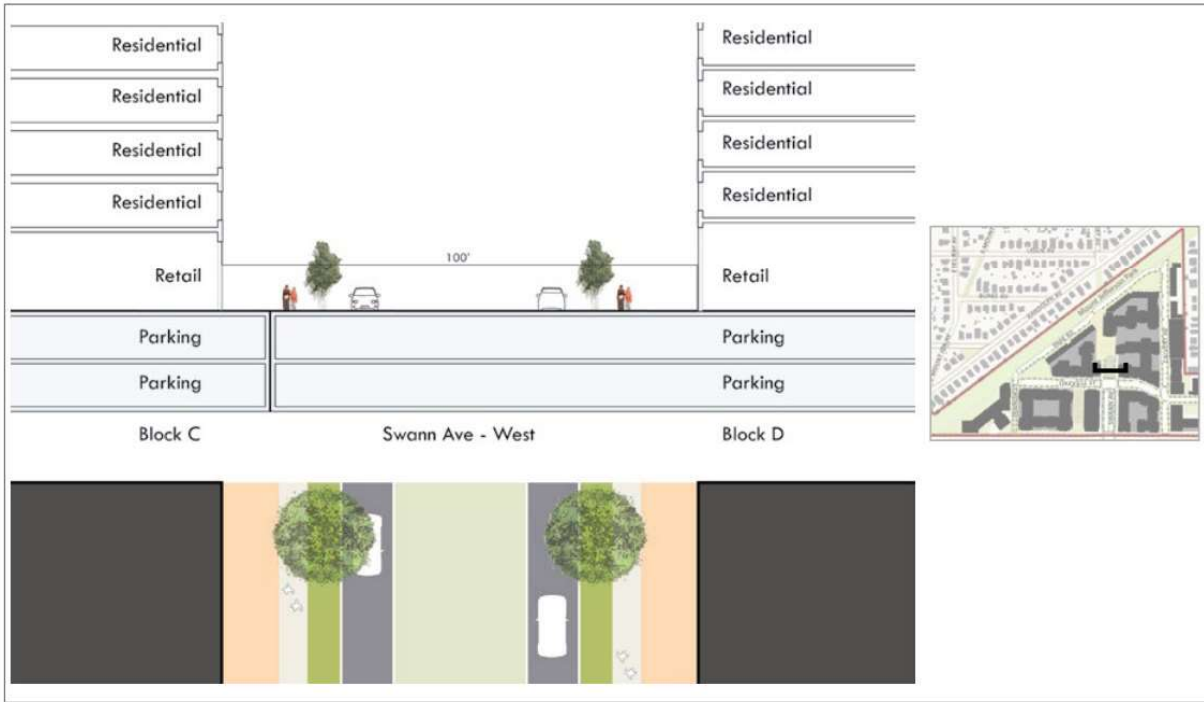
TYOLOGY: MIXED USE BOULEVARD



p.121 (2015)

SWANN AVENUE (WEST)

TYPOLGY: SHARED STREET



p.121 (Proposed)

SWANN AVENUE (WEST)

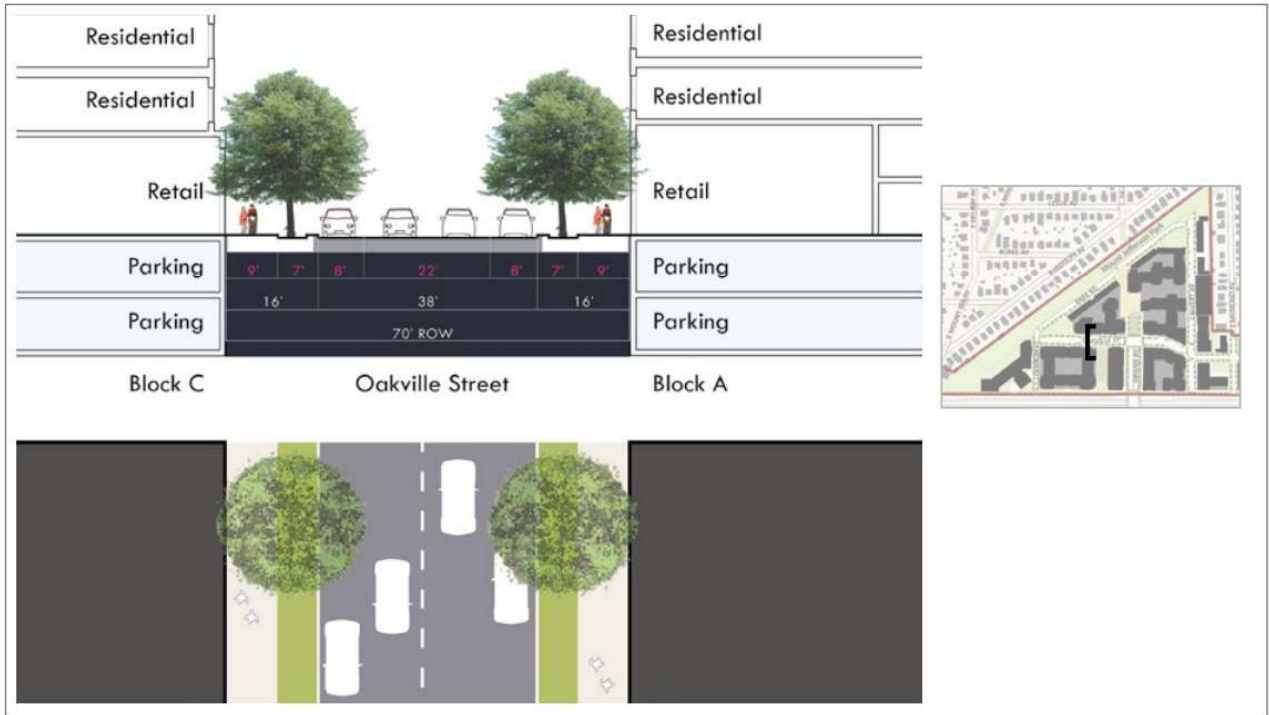
NEIGHBORHOOD RESIDENTIAL

TYPOLGY: SHARED STREET

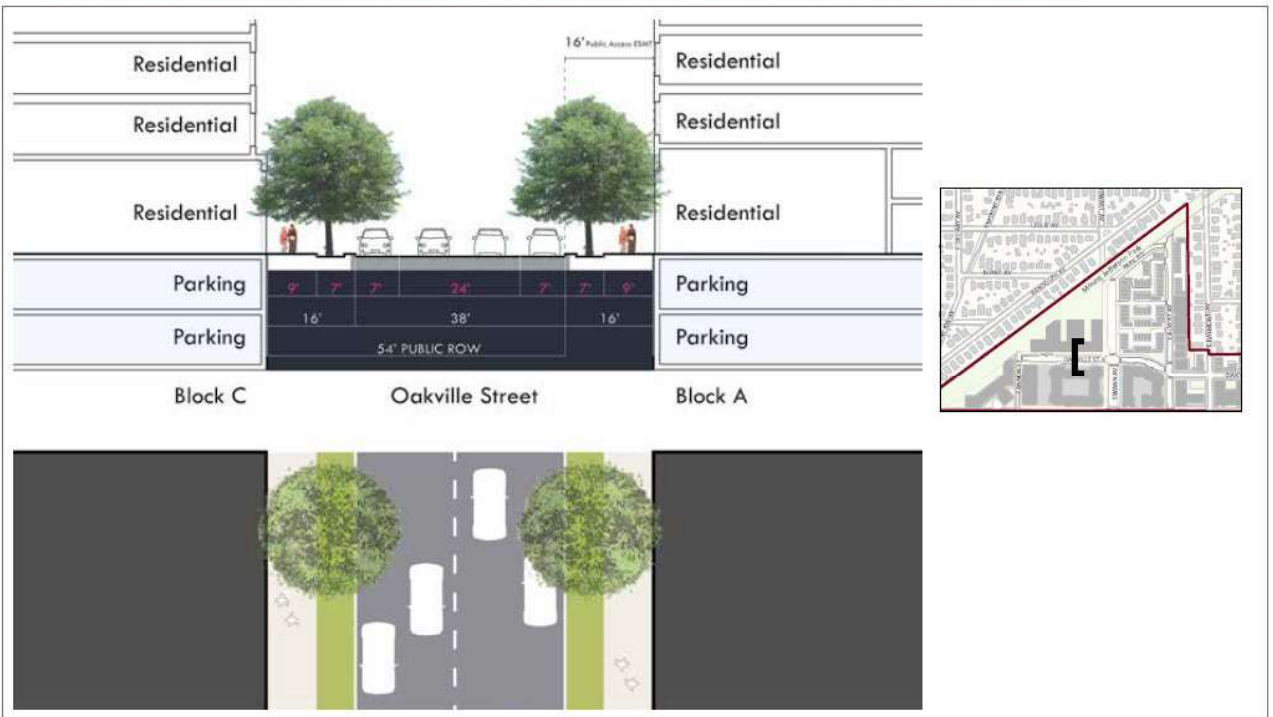


E. OAKVILLE STREET (FANNON STREET TO CALVERT AVENUE)

TYPOLOGY: MIXED USE BOULEVARD



E. OAKVILLE STREET (FANNON STREET TO CALVERT AVENUE) TYPOLOGY: MIXED USE BOULEVARD



F. CALVERT AVENUE (WEST - INTERIM)

TYOLOGY: MAIN STREET



F. CALVERT AVENUE (WEST - INTERIM)

TYOLOGY: MAIN STREET



CALVERT AVENUE (WEST - WITH REDEVELOPMENT)

TYPOLOGY: MAIN STREET



CALVERT AVENUE (WEST - WITH REDEVELOPMENT)

TYPOLOGY: MAIN STREET



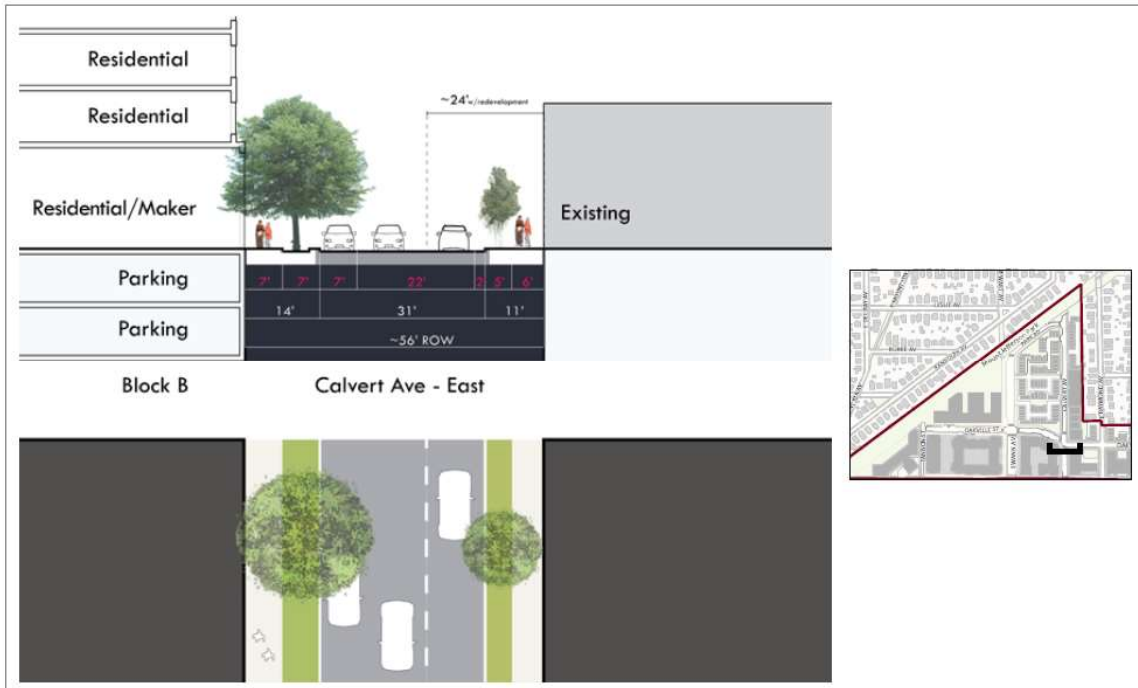
CALVERT AVENUE (EAST)

TYOLOGY: MIXED USE BOULEVARD



CALVERT AVENUE (EAST)

TYOLOGY: MIXED USE BOULEVARD



p. 24. Transportation Improvements (2015)

PHASE	DESCRIPTION	RESPONSIBILITY/ FUNDING	REQUIRED TIMING
1	Construction of lane modifications eastbound Swann Ave. at Route 1, as specified in Oakville Triangle CDP	Developer of Oakville Triangle	Operational prior to issuance of first Certificate of Occupancy (CO) for development within the Oakville Triangle CDP area (not including above or below grade parking).
1	Initial work on Phase 1 improvements at Route 1/E. Glebe Rd. (eastbound right turn modified to allow overlap right turns), as specified in Oakville Triangle CDP	Developer of Oakville Triangle to provide \$200,000	Prior to release of the final site plan for the first block within the Oakville Triangle CDP area.
1	Route 1 / E. Reed signal modifications (Address through Potomac Yard North SAP Amendment)	Developer contributions and /or incremental tax	Prior to 800,000 sf of development (net of parking)
2	Phase 1 improvements at Route 1/E. Glebe Rd.: Signal phasing and construction from Route 1 to new North-South Road	Developer of Oakville Triangle to provide \$1,400,000	Earlier of: (1) 1 year after issuance of final CO for completion of Phase I (Block A1, B & D) or (2) prior to first final site plan submission for fourth block in the project.
2	Route 1/E. Reed intersection improvements: Westbound lanes modified from exclusive left turn lane and shared through/right lane to exclusive left, through and right turn lanes	Developer contributions and /or incremental tax	Prior to issuance of CO at 1.6 million sf (Net without parking)
2	New signal at Montrose Ave./ Route 1.	Developer contributions and /or incremental tax	Prior to issuance of CO at 1.6 million sf (Net without parking)
2	Improvements on Montrose Avenue and at the intersection of Montrose Avenue at East Glebe Road / Ashby Street	Developer contributions and /or incremental tax	Prior to issuance of CO at 1.6 million sf (Net without parking)
2	New Route 1 pedestrian crossing/signal between East Custis Avenue and East Glebe Road.	Developer contributions and /or incremental tax	Prior to issuance of CO at 2 million sf (Net without parking)
3	Route 1 / E. Glebe Intersection improvements (Phase 2 from the new North-South Road to LaVerne Avenue) or comparable improvement – in consultation with property owners.	Developer contributions and/ or incremental tax	Prior to issuance of CO at 2 million sf (Net without parking)
3	Modification of lane configuration at Route 1/Custis Ave.	Developer contributions and/ or incremental tax	Prior to issuance of CO at 2 million sf (Net without parking)

p. 24 Transportation Improvements (Proposed)

PHASE	DESCRIPTION	RESPONSIBILITY/ FUNDING	REQUIRED TIMING
1	Construction of lane modifications eastbound Swann Ave. at Route 1, as specified in Oakville Triangle CDP	Developer of Oakville Triangle	Operational prior to issuance of first Certificate of Occupancy (CO) for <u>blocks 4A or Z</u> development within the Oakville Triangle CDP area (not including above or below grade parking).
1	Initial work on Phase 1 improvements at Route 1/E. Glebe Rd. (eastbound right turn modified to allow overlap right turns), as specified in Oakville Triangle CDP	Developer of Oakville Triangle to provide \$200,000 <u>\$350,000</u>	Prior to release of the final site plan for blocks 4A or 7 the first block within the Oakville Triangle CDP area.
1	Route 1 / E. Reed signal modifications (Address through Potomac Yard North SAP Amendment)	Developer contributions and /or incremental tax	Prior to 800,000 sf of development <u>With North Potomac Yard first building</u> (net of parking)
1	<u>New signalized intersection improvements and pedestrian crossings at Route 1/Fannon Street</u>	<u>Developer of Oakville Triangle</u>	<u>Operational prior to issuance of Certificate of Occupancy (CO) for the Block 4B.</u>
2	Phase 1 improvements at Route 1/E. Glebe Rd.: Signal phasing and construction from Route 1 to new North-South Road	Developer of Oakville Triangle to provide \$1,400,000	Earlier of: (1) 1 year after issuance of final CO for completion of Phase I (Block A1, B & D) or (2) prior to first final site plan submission for fourth block in the project.
2	Route 1/E. Reed intersection improvements: Westbound lanes modified from exclusive left turn lane and shared through/right lane to exclusive left, through and right turn lanes	Developer contributions and /or incremental tax	Prior to issuance of CO at 1.6 million sf (Net without parking)
2	New signal at Montrose Ave./ Route 1.	Developer contributions and /or incremental tax	Prior to issuance of CO at 1.6 million sf (Net without parking)
2	Improvements on Montrose Avenue and at the intersection of Montrose Avenue at East Glebe Road / Ashby Street	Developer contributions and /or incremental tax	Prior to issuance of CO at 1.6 million sf (Net without parking)
2	New Route 1 pedestrian crossing/signal between East Custis Avenue and East Glebe Road.	Developer contributions and /or incremental tax	Prior to issuance of CO at 2 million sf (Net without parking)
3	Route 1 / E. Glebe Intersection improvements (Phase 2 from the new North-South Road to LaVerne Avenue) or comparable improvement – in consultation with property owners.	Developer contributions and/ or incremental tax	Prior to issuance of CO at 2 million sf (Net without parking)
3	Modification of lane configuration at Route 1/Custis Ave.	Developer contributions and/ or incremental tax	Prior to issuance of CO at 2 million sf (Net without parking)

Chart 1: Public Benefits Phasing (2015)

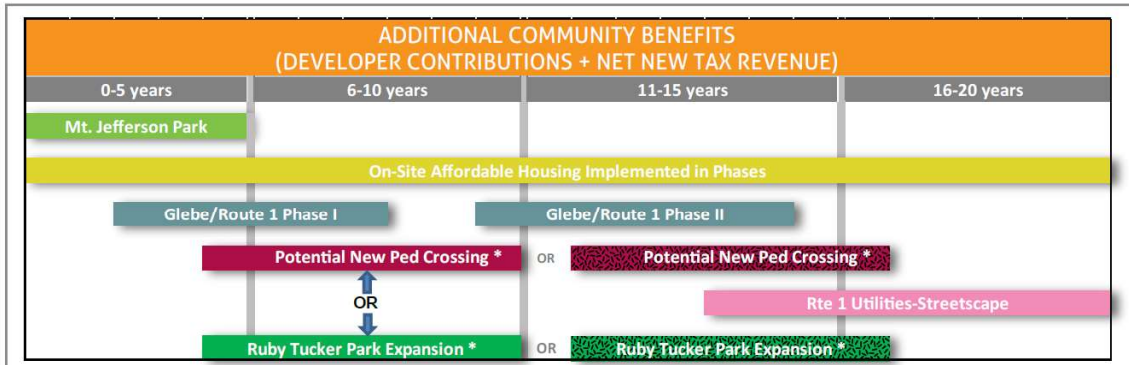


Chart 1: Public Benefits Phasing Proposed (2020)

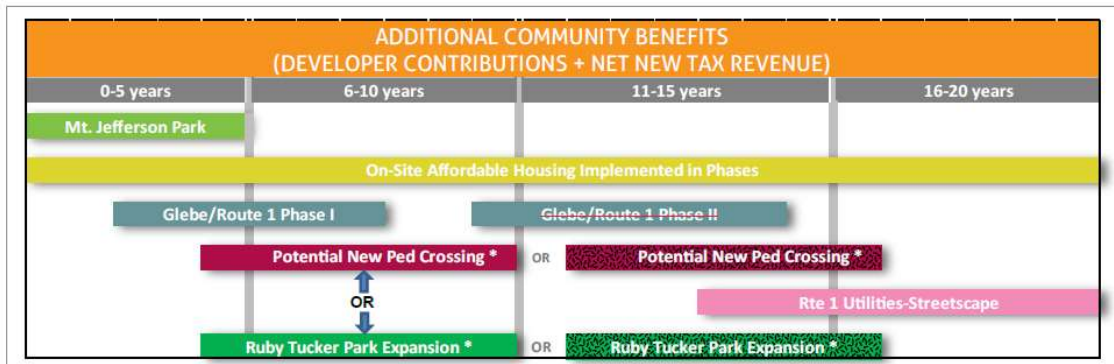


Chart 2: Development Summary Table (2015)

District #	Block #	Applicable notes (see notes page below)	Residential				Office	Hotel	Retail and Maker		Maximum Total (sq. ft.)
			1a		1b		2	3	4	5	
			Multifamily Residential (max. GFA)	Multifamily Residential (max. units)	Townhouse (max. GFA) Includes garages. See Note 1	Townhouse (max. units)	Office (max. GFA)	Max. GFA	Retail (Primary / Secondary) (max. GFA)	Retail (Maker Space Required (max. GFA) See Note c	
1	1	f	0	0	24,800	10	0	0	0	0	24,800
	2	f	0	0	24,800	10	0	0	0	0	24,800
2	3	a b f	140,500	149	0	0	0	0	0	9,500	150,000
	4	b k l m n	307,000	326	0	0	135,200	0	46,000	0	488,200
	5	k l m q	162,600	172	35,100	14	0	0	31,000	0	228,700
	6	k l m q r	377,400	399	31,000	14	0	0	81,700	0	490,100
	7	b k l m p	117,100	125	0	0	0	145,300	35,200	0	297,600
3	8		0	0	44,900	18	0	0	0	0	44,900
	9	d	0	0	0	0	0	0	0	21,500	21,500
	10		66,800	71	14,800	5	0	0	0	6,200	87,800
	11		75,600	80	0	0	0	0	0	19,200	94,800
	12		0	0	30,100	13	0	0	0	0	30,100
	13		80,000	85	0	0	0	0	0	27,900	107,900
	14		0	0	21,800	9	0	0	0	0	21,800
	15		0	0	28,000	11	0	0	0	0	28,000
	16	b	134,900	143	0	0	0	0	0	29,800	164,700
	17	b	120,000	127	0	0	0	0	0	6,100	126,100
4	18	f	0	0	14,800	5	0	0	0	0	14,800
	19		79,700	85	0	0	0	0	0	8,600	88,300
	20		68,200	72	0	0	0	0	0	5,900	74,100
	21		61,700	65	0	0	0	0	0	5,600	67,300
CDD Total			1,791,500	1,900	270,100	109	135,200	145,300	193,900	140,300	2,676,300

Chart 2: Development Summary Table (Proposed 2020 Amendment)

District #	Block #	Applicable notes (see notes page below)	Residential				Office	Hotel	Retail and Maker		Maximum Total (sq. ft.)
			1a		1b		2	3	4	5	
			Multifamily Residential (max. GFA)	Multifamily Residential (max. units)	Townhouse (max. GFA) Includes garages. See Note 1	Townhouse (max. units)	Office, Medical Care Facility (max. GFA)	Max. GFA	Retail (Primary / Secondary) (max. GFA)	Retail (Maker Space Required (max. GFA) See Note c	
1	1	f	0	0	24,800	10	0	0	0	0	24,800
	2	f	0	0	24,800	10	0	0	0	0	24,800
2	3	a b f	140,500	149	0	0	0	0	0	9,500	150,000
	4	b k l m n	307,000 310,000	330 326	0	0	135,200 115,000	0	46,000 40,000	0 46,000	488,200 465,000
	5	k l m q	162,600 200,000	172 240	35,100 0	14 0	0	0	31,000 0	0	228,700 200,000
	6	k l m q r	377,400 0	399 0	31,000 250,000	14 0	0	0	81,700 0	0	490,100 250,000
	7	b k l m p	117,100 270,000	125 260	0	0	0	145,300	35,200 25,000	0	297,600 295,000
3	8		0	0	44,900	18	0	0	0	0	44,900
	9	d	0	0	0	0	0	0	0	21,500	21,500
	10		66,800	71	14,800	5	0	0	0	6,200	87,800
	11		75,600	80	0	0	0	0	0	19,200	94,800
	12		0	0	30,100	13	0	0	0	0	30,100
	13		80,000	85	0	0	0	0	0	27,900	107,900
	14		0	0	21,800	9	0	0	0	0	21,800
	15		0	0	28,000	11	0	0	0	0	28,000
	16	b	134,900	143	0	0	0	0	0	29,800	164,700
	17	b	120,000	127	0	0	0	0	0	6,100	126,100
4	18	f	0	0	14,800	5	0	0	0	0	14,800
	19		79,700	85	0	0	0	0	0	8,600	88,300
	20		68,200	72	0	0	0	0	0	5,900	74,100
	21		61,700	65	0	0	0	0	0	5,600	67,300
CDD Total			1,607,400	1,707	454,000	171	115,000	145,300	65,000	140,300	2,381,700
			1,791,500	1,900	270,100	109	135,200	145,300	193,900	140,300	2,676,300

ATTACHMENT #4

OAKVILLE TRIANGLE & ROUTE 1

Corridor Vision Plan and Urban Design Standards & Guidelines

City of Alexandria, VA


ADOPTED BY ORDINANCE #4976 ON NOVEMBER 14, 2015
AMENDED 4/16/16 BY ORDINANCE #4998



Amended 4/16/16, MPA#2015-0007 by Ordinance 4998
Printed October 2016

TABLE OF CONTENTS

<p>1 VISION PLAN 7</p> <p>A. PLAN PRINCIPLES</p> <p>B. PLAN RECOMMENDATIONS</p> <p>C. PLAN IMPLEMENTATION</p> <p> i. Development Summary Table</p> <p> ii. Transportation Improvements</p> <p> iii. Non-Transportation Improvements</p>	<p>4 PLAN FRAMEWORK 47</p> <p>A. FRAMEWORK - STREET HIERARCHY</p> <p>B. BIKE PEDESTRIAN NETWORK</p> <p>C. BLOCKS</p> <p>D. ZONING</p> <p>E. LAND USE</p> <p>F. FUELING/ CHARGING STATIONS</p> <p>G. BUILDING HEIGHTS</p> <p>H. OPEN SPACE</p> <p>I. GATEWAY ELEMENTS</p>	<p>G. FUELING/CHARGING STATIONS</p> <p>H. BUILDING ENTRIES</p> <p>I. BUILDING ROOFS</p> <p>J. WALLS/FENCES</p> <p>K. PUBLIC REALM - STREETScape</p> <p>L. REAR OF BUILDINGS - ALLEYS</p>
<p>2 URBAN DESIGN STANDARDS AND GUIDELINES 29</p> <p>A. ROLE OF THE STANDARDS</p> <p>B. GENERAL STANDARDS</p>	<p>5 URBAN DESIGN 73</p> <p>A. BUILDING STREETWALL</p> <p>B. BUILDING HEIGHTS-VARIETY</p> <p>C. BUILDING MASSING</p> <p>D. BUILDING TRANSITIONS</p> <p>E. CHARACTER</p> <p>F. BUILDING TYPES</p> <p> i. Townhouses</p> <p> ii. Multi-family</p> <p> iii. Office and Hotel</p>	<p>6 GROUND FLOOR USES 9</p> <p>A. RETAIL USE</p> <p>B. RETAIL USE REQUIREMENTS</p> <p>C. RETAIL STOREFRONTS</p> <p>D. MAKER USE</p> <p>E. MAKER USE REQUIREMENTS</p> <p>F. MAKER USE STOREFRONTS</p>
<p>3 PLAN DISTRICTS 33</p> <p>A. DISTRICT 1</p> <p>B. OAKVILLE TRIANGLE</p> <p>C. DISTRICT 3</p> <p>D. DISTRICT 4</p> <p>E. ROUTE 1 FRONTAGE</p>		<p>7 SIGNAGE 105</p>

 STREETS	111
A. ROUTE 1 STREETScape	
B. FANNON STREET	
C. PARK ROAD	
D. SWANN AVENUE	
E. OAKVILLE STREET (FANNON STREET TO CALVERT AVENUE)	
F. CALVERT AVENUE	
G. OAKVILLE STREET (CALVERT AVENUE TO EAST GLEBE ROAD)	
H. ROUTE 1 AND EAST GLEBE ROAD INTERSECTION	
 SUMMARY OF RECOMMENDATIONS & DESIGN STANDARDS	
 APPENDIX	147

ACKNOWLEDGMENTS

CITY COUNCIL

Mayor William D. Euille
Vice Mayor Allison Silberberg
Councilman John T. Chapman
Councilman Timothy B. Lovain
Councilwoman Redella S. Pepper
Councilman Paul C. Smedberg
Councilman Justin Wilson

PLANNING COMMISSION

Mary Lyman, Chair
David Brown
Stewart Dunn, Jr.
Stephen Koenig
Mindy Lyle
Nathan Macek
Maria Wasowski

ADVISORY GROUP

Maria Wasowski, Chair
Pat Miller, Business Representative
Ben Flood, Del Ray Citizens Association Representative
Kory Mertz, Lynhaven Citizens Association Representative
Patricia Harris, Potomac Yard Representative
Frank Fannon, At-Large Representative
David Fromm, At-Large Representative
Rodrigo Letonja, At-Large Representative
Peter Pockock, At-Large Representative

FORMER MEMBERS

Andrew Dubinsky, Lynhaven Citizens Association Representative
Natasha Behbahany, Potomac Yard Representative

CITY MANAGERS OFFICE

Mark Jinks, City Manager
Emily Baker, Acting Deputy City Manager

OFFICE OF HOUSING

Helen McIlvaine, Acting Director
Eric Keeler, Division Chief
Tamara Jovovic, Housing Analyst

PLANNING AND ZONING

Karl Moritz, Director
Jeffrey Farner, Deputy Director
Thomas Canfield, NCARB, City Architect
Carrie Beach, Division Chief
Robert Kerns, Division Chief
Gary Wagner, Principal Planner
James Roberts, Urban Planner
Richard Lawrence, AICP, Urban Planner
Katherine Carraway, Urban Planner

RECREATION, PARKS AND CULTURAL ACTIVITIES

James Spengler, Director
Laura Durham, Open Space Coordinator
Dana Wedeles, Park Planner

TRANSPORTATION AND ENVIRONMENTAL SERVICES

Yon Lambert, Director
Carrie Sanders, Acting Deputy Director
Steve Sindiong, Principal Planner
Ravindra Raut, Civil Engineer

FORMER STAFF

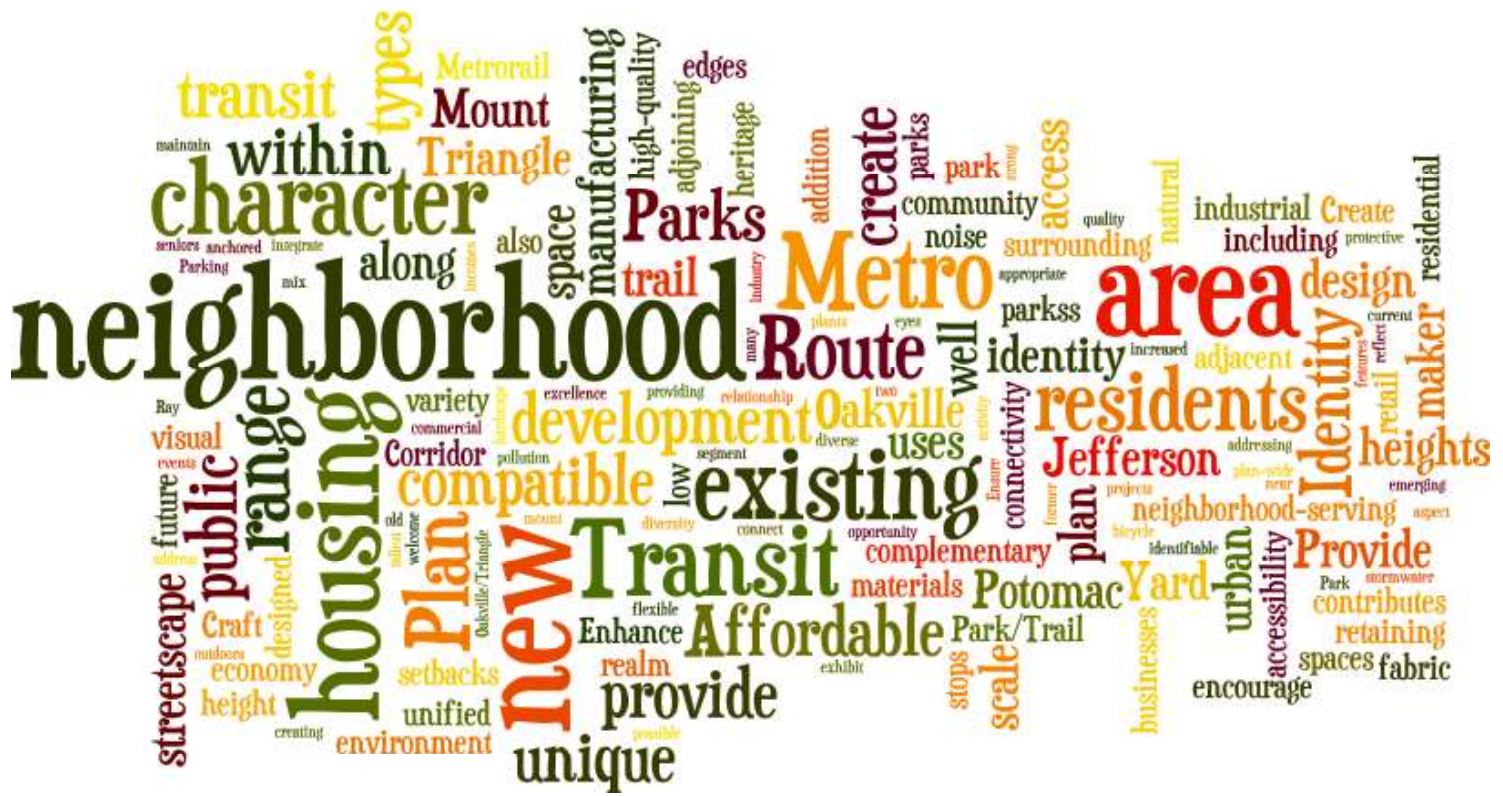
Amy Friedlander
C. Juliana Villabona
Sandra Marks, Deputy Director

INTENTIONALLY LEFT BLANK

1

VISION PLAN





VISION



The Plan envisions a future Oakville Triangle/Route 1 Corridor that is compatible with the fabric of existing neighborhoods, features an enhanced Mount Jefferson Park/Trail, and a blend of new and existing open spaces and land uses unified by an attractive urban streetscape. High quality architecture and urban design celebrate the area's industrial heritage, and new development includes a diversity of residential, office, hotel, *medical care facility*, and regional and neighborhood-serving retail uses, as well as an emerging "maker" economy. New development capitalizes on nearby transit by concentrating height at transit stops and in close proximity to the future Potomac Yard Metrorail Station, while ensuring compatibility with surrounding neighborhoods. As an inclusive neighborhood, the area provides housing options affordable to a range of incomes and welcoming to different household types. Residents, workers, and visitors walk and bike within the Plan area and to adjoining neighborhoods.

PLAN PRINCIPLES

- COMPATIBILITY WITH EXISTING NEIGHBORHOODS
- MOUNT JEFFERSON PARK ENHANCEMENT
- IMPROVED AND EXPANDED OPEN SPACES
- URBAN FORM AND DESIGN EXCELLENCE
- DEVELOPMENT NEAR TRANSIT
- TRANSPORTATION OPTIONS
- MIX OF USES, RETAIL AND MAKER SPACES
- DIVERSITY OF HOUSING OPTIONS

Create a high-quality built environment, streetscapes, and open spaces that foster a strong identity for the Plan area that is compatible with the adjoining neighborhoods of Del Ray, Lynhaven, and Potomac Yard, and Mount Jefferson Park. Create visual interest with a variety of building heights and facades along Route 1.

At the sensitive edges of the Plan area adjacent to low scale residential uses, step the new buildings down in height to provide appropriate scale and height transitions to the existing neighborhoods. Use building materials and design that are compatible with the character of the adjoining neighborhoods.

RECOMMENDATIONS:

- 1.1 Establish Design Standards and Guidelines to support the expectation of high quality architecture and to ensure compatibility with the adjoining neighborhoods.
- 1.2 Ensure that new development complies with the maximum and minimum height limits and appropriate building height transitions. (Figures 26 & 27, and 28.)

VISION PLAN



MOUNT JEFFERSON PARK ENHANCEMENT

Enhance the existing character of the Mount Jefferson Park as a natural urban retreat, while addressing issues of stormwater management, invasive plants, and improvements to the dog exercise area. Preserve the unique history of the trail by retaining the topography that recalls its former use as a rail line. Expand the trail and create a natural buffer along the edges to maintain its character as a place “set-apart,” while also opening up the trail for increased community accessibility and safety with more activity and “eyes on the park.”

RECOMMENDATIONS:

- 1.3 Require that the redevelopment of the Oakville Triangle site fund the final design and implementation of the approved Plan for Mount Jefferson Park.



IMPROVED AND EXPANDED OPEN SPACES

New development will be required to provide usable ground level public open spaces, including, but not limited to, ~~three new public open spaces~~ a new .7 acre central public open space within Oakville Triangle to build on and visually connect to the Mount Jefferson Park, as well as expansion of the existing Ruby Tucker Park to create a larger public open space in the northern portion of the Plan area. A ~~new hardscape plaza~~ new central open space in Oakville Triangle will provide a public gathering space for daily enjoyment and special events for residents and visitors, new and old. In addition, larger projects will provide rooftop amenity space to ensure adequate opportunity for new residents’ access to the outdoors, designed to be

compatible with and to minimize light and noise impacts on the adjoining neighborhoods.

RECOMMENDATIONS:

- 1.4 Require new development to provide ground level open space (publicly accessible where feasible) and rooftop amenity space within redevelopment sites as specified in Design Standards and Coordinated Development District (CDD) zoning for the Plan area.
- 1.5 Expand Ruby Tucker Park within the City’s existing right of way on Lynhaven Drive.



Figure 1: Mount Jefferson Park Concept Plan



URBAN FORM AND DESIGN EXCELLENCE

Redevelopment will exhibit excellence in building and urban design, and a variety of building heights, setbacks and high-quality building materials in compliance with the Oakville Triangle and Route 1 Urban Design Standards and Guidelines. New development is encouraged to incorporate aspects of the railroad and industrial heritage of the Plan area as well as identifiable characteristics of the adjacent neighborhoods, strengthening the connection to the surrounding community. The Plan recommends a unified streetscape

for Route 1, with wider sidewalks, street trees lining the pedestrian realm, and underground utilities to address the current lack of accessibility and visual clutter.

RECOMMENDATIONS:

- 1.7 Ensure high quality design and architecture and building materials. Encourage integration of the area's railroad and industrial heritage into new building, park and streetscape design. Encourage uses that will activate the streetscape.
- 1.8 Ensure that new buildings are designed as a collection of compatible but different buildings in scale, materials and architecture.
- 1.9 With redevelopment of sites on Route 1, require undergrounding of utilities and construction of a 25-foot streetscape. For the limited sites where this is potentially not feasible (see Figure 17), funding for these improvements will be part of the plan-wide implementation.



DEVELOPMENT NEAR TRANSIT

Create an urban mixed-use environment that minimizes dependency on the automobile and prioritizes walking, biking, and transit use. Focus taller building heights at the transit stops along Route 1 to encourage use of the Route 1 Transitway and future Potomac Yard Metrorail Station. Taller building heights will require appropriate scale transitions to the adjoining neighborhoods.

RECOMMENDATIONS:

- 1.10 Concentrate taller building heights at the locations of transit stations at Swann Avenue/Route 1 and East Glebe Road/Route 1, subject to the standards for required height transitions to the adjoining neighborhoods.

TRANSPORTATION OPTIONS AND CONNECTIVITY

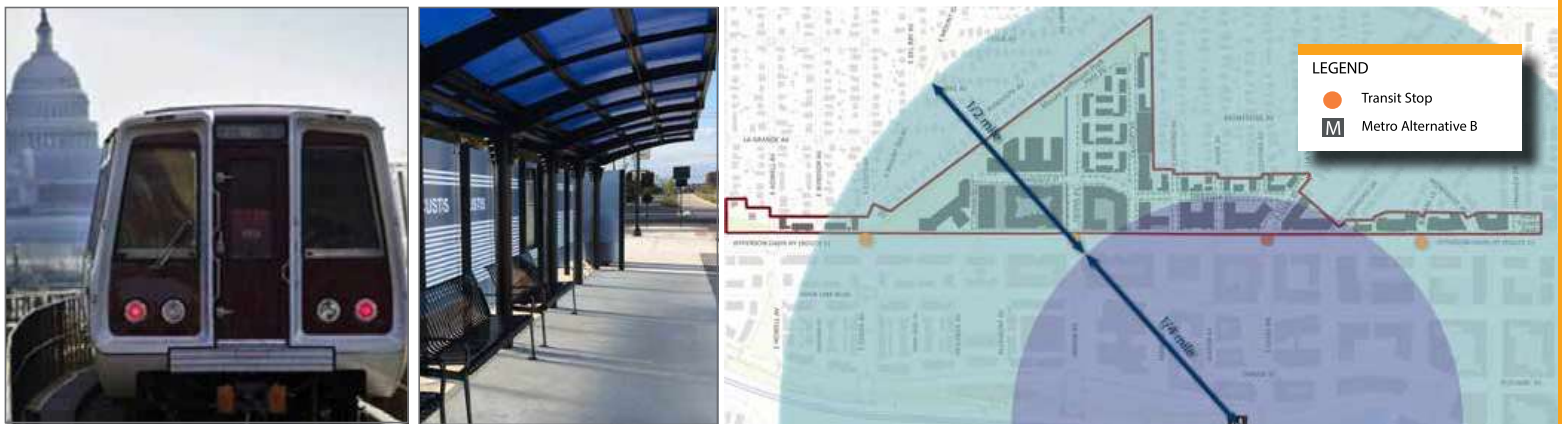
A comprehensive transportation study examined the transportation impacts within the Plan area and immediate surroundings. The planning-level analysis assumed full buildout by the year 2027.

The analysis assumed increases in traffic attributed to regional growth and other approved development anticipated by 2027, such as in North and South Potomac Yard. The analysis also assumed planned transportation improvements, including the Potomac Yard Metrorail station and intersection improvements at Route 1/East Reed Avenue, and assumes a significant number of trips accommodated by transit, walking or biking, given that Route 1 is a transit-oriented corridor with easy access to the Potomac Yard Metrorail station and Route 1 Transitway.

The study showed that with the construction of all the Plan's recommended transportation improvements, including new roadway connectivity, improved pedestrian and bicycle facilities in and through the Plan area, and mix of land uses which result in a greater shift to other modes such as walking and using transit, the transportation network operates more efficiently in the 2027 Development Scenario than the 2027 Baseline (No development) Scenario.

The Plan's comprehensive transportation strategy recommends a variety of multi-modal improvements. New blocks and connections enhance the road network within the Plan area including an extension of Oakville Street connecting the Oakville Triangle property to the south with East Glebe Road to the north. A new

Figure 2: Metro Proximity and Adjoining Transit Stops



traffic signal at Montrose Avenue and Route 1 improves east west connectivity and will be coordinated with improvements to the intersection of East Glebe Road/Montrose Avenue/Ashby Street and associated traffic calming strategies on Montrose Avenue. New sidewalks, trails, and bike facilities, including the planned pedestrian-bicycle connection between Stewart Avenue and Swann Avenue, will connect current residents with new neighborhoods and transit facilities. A new signalized pedestrian connection across Route 1 between East Custis Avenue and East Glebe Road will improve access between Oakville Triangle and Potomac Yard including the Potomac Yard Metrorail Station.

In addition, improvements are needed at the intersection at East Glebe Road and Route 1, an important intersection in the Plan area and the City as a whole. Improvements are recommended in a phased approach in coordination with the adjoining property owners.

Parking will be reviewed with each development application for compliance with City standards or applicable requirements, including the recently amended standards for residential development that take walkability and proximity to transit, among other factors, into account, as well as sensitivity to existing neighborhoods. Shared parking is strongly encouraged during the development review process.

RECOMMENDATIONS:

- 1.11 A transportation network that includes a new street grid to distribute vehicular traffic, improve traffic flow, and improve pedestrian, bicycle and transit connectivity.
- 1.12 A new north-south road between Fannon Street and East Glebe Road (extension of Oakville Street).
- 1.13 A pedestrian and bike connection between Swann Avenue and Stewart Avenue to provide connectivity between the neighborhoods and the future Potomac Yard Metrorail station. ~~The bike lane within the central urban plaza within the Oakville Triangle site will be designed as a shared space.~~
- 1.14 An improved pedestrian network that includes safe and accessible sidewalks along all streets within the plan area that connect to parks, retail, transit and trails.
- 1.15 Improvements to the Route 1/East Glebe Road intersection.
- 1.16 A new signal at the intersection of Route 1 and Montrose Avenue to improve east-west connectivity, to be coordinated with traffic calming improvements along Montrose Avenue and operational improvements at the intersection of Montrose Avenue at East Glebe Road/Ashby Street.
- 1.17 ~~An additional signalized pedestrian crossing across Route 1 between East~~

~~Custis Avenue and East Glebe Road to improve pedestrian access between Oakville Triangle and Del Ray/Lynhaven and the future Potomac Yard Metrorail station. An additional signalized crossing across Route 1 at Fannon Street, including pedestrian and intersection improvements, to improve pedestrian access between Oakville Triangle and Del Ray/Lynhaven and the Potomac Yard Metrorail station and provide efficient access to the medical care facility.~~

- 1.18 An improved bicycle network that includes bike lanes along Swann Avenue to connect Potomac Yard, the Mt. Jefferson Trail, and the Del Ray neighborhood.
- 1.19 Bicycle parking and opportunities for bike sharing.
- 1.20 Enhanced parking management, including performance parking, smart parking technology, and shared parking.
- 1.21 Transportation Management Plan (TMP) strategies such as a TMP District, transit incentives, vanpool and carpool sharing, car share, electric vehicle charging stations, and TMP monitoring.

A MIX OF USES, RETAIL AND MAKER SPACES

Create an urban mixed-use environment that facilitates a range of activity throughout the day and evening, during the week and on weekends, including residential, hotel, office, retail, and maker uses.

Oakville Triangle and the Route 1 Corridor are home to many neighborhood-serving businesses. The Plan encourages the inclusion of space within areas of the new redevelopment for these types of businesses to locate, retaining this important quality of life feature for local residents. In addition, these spaces will be suited to the emerging “maker” industry, a growing segment of the economy that features craft manufacturing

and repair type uses. Designated blocks of the Plan area will include first floor space to encourage these uses. Parking, loading, refuse, and noise abatement will be addressed in the design of the buildings to be compatible with the adjoining residential neighborhoods.

RECOMMENDATIONS:

- 1.22 Require a mix of land uses as depicted in Figure 25 and taller first floors heights where required. Parking, loading, refuse, and noise will be addressed in the design of the new buildings as part of the development review process.



DIVERSITY OF HOUSING OPTIONS

Develop with housing options affordable to a range of incomes and welcoming to different household types—including young professionals, families, seniors, and workers who are critical to the economic competitiveness and sustainability of the area. Oakville Triangle, North and South Potomac Yard, and the Route 1 Corridor host (and will continue to attract) a wide variety of employers. The success of these businesses, in part, relies on the availability of a diverse workforce. Providing affordable and life stage-appropriate housing in close proximity to jobs and transit will help improve workers' and residents' quality of life, reduce congestion, and strengthen retail and the City's economic base.



RECOMMENDATIONS:

- 1.23 Consistent with the policies and goals of the Housing Master Plan, encourage the inclusion of on-site affordable rental and home-owner housing opportunities and voluntary contributions to the Affordable Housing Trust Fund with each redevelopment in the Plan area.
- 1.24 Ensure a minimum of 65 affordable units within the Oakville Triangle site.
- 1.25 Explore the provision of potential ARHA replacement units in the Plan area.
- 1.26 Encourage universal design to allow residents to age-in-place.
- 1.27 Encourage a variety of housing types within the Plan area.

PLAN RECOMMENDATIONS

- 1.1 Establish Design Standards and Guidelines to support the expectation of high quality architecture and to ensure compatibility with the adjoining neighborhoods.
- 1.2 Ensure that new development complies with the maximum and minimum height limits and appropriate building height transitions. (Figures 26 & 27, and 28.)
- 1.3 Require that the redevelopment of the Oakville Triangle site fund the final design and implementation of the approved Plan for Mount Jefferson Park.
- 1.4 Require new development to provide ground level open space (publicly accessible where feasible) and rooftop amenity space within redevelopment sites as specified in Design Standards and Coordinated Development District (CDD) zoning for the Plan area.
- 1.5 Expand Ruby Tucker Park within the City's existing right of way on Lynhaven Drive.
- 1.6 The existing right-of-way located on Bellefonte Avenue and Route 1 will remain as right-of-way but will be used as open space and streetscape improvements. The City will explore acquiring the vacant property on Bellefonte Avenue, adjacent to Route 1, for open space.
- 1.7 Ensure high quality design and architecture and building materials. Encourage integration of the area's railroad and industrial heritage into new building, park and streetscape design. Encourage uses that will activate the streetscape.
- 1.8 Ensure that new buildings are designed as a collection of compatible but different buildings in scale, materials and architecture.
- 1.9 With redevelopment of sites on Route 1, require undergrounding of utilities and construction of a 25-foot streetscape. For the limited sites where this is potentially not feasible (see Figure 17), funding for these improvements will be part of the plan-wide implementation.
- 1.10 Concentrate taller building heights at the locations of transit stations at Swann Avenue/Route 1 and East Glebe Road/Route 1, subject to the standards for required height transitions to the adjoining neighborhoods.
- 1.11 A transportation network that includes a new street grid to distribute vehicular traffic, improve traffic flow, and improve pedestrian, bicycle and transit connectivity.
- 1.12 A new north-south road between Fannon Street and East Glebe Road (extension of Oakville Street).
- 1.13 A pedestrian and bike connection between Swann Avenue and Stewart Avenue to provide connectivity between the neighborhoods and the future Potomac Yard Metrorail station. ~~The bike lane within the central urban plaza within the Oakville Triangle site will be designed as a shared space.~~
- 1.14 An improved pedestrian network that includes safe and accessible sidewalks along all streets within the plan area that connect to parks, retail, transit and trails.
- 1.15 Improvements to the Route 1/East Glebe Road intersection.
- 1.16 A new signal at the intersection of Route 1 and Montrose Avenue to improve east-west connectivity, to be coordinated with traffic calming improvements along Montrose Avenue and operational improvements at the intersection of Montrose Avenue at East Glebe Road/Ashby Street.

- 1.17 ~~An additional signalized pedestrian crossing across Route 1 between East Custis Avenue and East Glebe Road to improve pedestrian access between Oakville Triangle and Del Ray/Lynhaven and the future Potomac Yard Metrorail station: An additional signalized crossing across Route 1 at Fannon Street, including pedestrian and intersection improvements, to improve pedestrian access between Oakville Triangle and Del Ray/Lynhaven and the Potomac Yard Metrorail station and provide efficient access to the medical care facility.~~
- 1.18 An improved bicycle network that includes bike lanes along Swann Avenue to connect Potomac Yard, the Mt. Jefferson Trail, and the Del Ray neighborhood.
- 1.19 Bicycle parking and opportunities for bike sharing.
- 1.20 Enhanced parking management, including performance parking, smart parking technology, and shared parking.
- 1.21 Transportation Management Plan (TMP) strategies such as a TMP District, transit incentives, vanpool and carpool sharing, car share, electric vehicle charging stations, and TMP monitoring.
- 1.22 Require a mix of land uses as depicted in Figure 25 and taller first floors heights where required. Parking, loading, refuse, and noise will be addressed in the design of the new buildings as part of the development review process.
- 1.23 Consistent with the policies and goals of the Housing Master Plan, encourage the inclusion of on-site affordable rental and home-owner housing opportunities and voluntary contributions to the Affordable Housing Trust Fund with each redevelopment in the Plan area.
- 1.24 Ensure a minimum of 65 affordable units within the Oakville Triangle site.
- 1.25 Explore the provision of potential ARHA replacement units in the Plan area.

IMPLEMENTATION

The Plan establishes a 20 year vision to enable the City to coordinate short-term and future growth in the Oakville Triangle-Route 1 Corridor. In addition to setting a framework for the type, scale and compatibility of anticipated development, the short-term and long-term vision for the area also establishes a plan for amenities and improvements.

There are a number of public benefits that have been identified as necessary to mitigate impacts, enhance the livability in the Plan area, in adjacent neighborhoods, and in the Alexandria community at large. These are:

- Implementation of the Mount Jefferson Park Plan;
- Phased improvements to the Glebe Road/Route 1 intersection. This will be done in consultation with the adjoining property owners;
- Streetscaping and undergrounding utilities for the Route 1 frontage;
- Expansion of the Ruby Tucker Park;
- Pedestrian Signal on Route 1 between East Custis Avenue and East Glebe Road;
- On-site affordable housing; and
- Installation of traffic signal at Montrose Avenue and Route 1 and improvements to Montrose Avenue and East Glebe Road.
- *Installation of traffic signal, pedestrian, and intersection improvements at Fannon Street and Route 1.*

The cost of the above public benefits has been estimated at a planning level basis. In order to fund these improvements, the Plan identifies three sources of funding. The specific allocation of the funding and phasing will be established as part of the CDD zoning and associated approvals.

1. TYPICAL REDEVELOPMENT CONDITIONS

Improvements are typically required as part of any development special use permit (DSUP) as part of redevelopment. These generally include elements such as:

- Underground utilities (property frontage);
- Community meeting space within the new multi-family, hotel, or office buildings;
- On-site ground level open space (public or private);
- Street and related improvements such as sidewalks, street right-of-way-necessary to serve the needs of the site;
- Public art under the City's Voluntary Art Contribution policy;

- Higher quality architecture and streetscape;
- Affordable housing under the City's Voluntary Affordable Housing Contribution policy and/or pursuant to the Housing Master Plan;
- Underground parking where specified;
- Capital Bikeshare; and
- Transportation Management Plan.

2. DEVELOPER CONTRIBUTIONS

As with other Small Area Plans, redevelopment sites that receive a rezoning will contribute to community improvements that mitigate the impact of the new development. The City anticipates that a portion of the property/site value increase as a result of proposed rezoning within the Plan area will come back to the City in the form of developer contributions to fund or directly implement plan area improvements related to the impact of the new development beyond those typically required as part of the DSUP process, such as the improvements to Mt. Jefferson Park. Developer contributions will be determined based on value increase and will be established as part of the Coordinated Development District (CDD) zoning and

associated approvals for the Plan area.

3. NET NEW TAX REVENUE

It is proposed that a maximum of 20% of the incremental (or net new) real estate tax revenues generated by the real estate value growth in this Plan area pay for a portion of the desired public benefits. Net new tax revenue is the City tax revenue over what would have been generated under existing zoning. This is a pay-as-you-go financing plan that will not require the use of any current City General Fund revenues. It does assume,

however, that the development build out would occur.

PHASING AND SEQUENCING PUBLIC BENEFITS

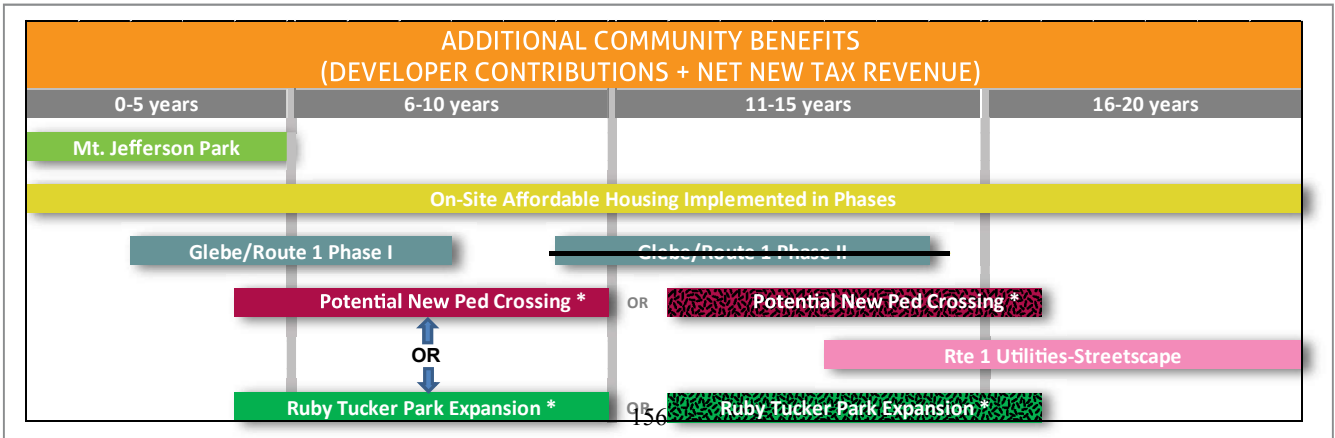
Equally important in the discussion of funding the public benefits is the phasing and sequencing of the benefits to establish which improvements need to occur first and which can occur more gradually over the life of the plan. Chart 1 shows the general phasing of the benefits over the life of the Plan. Some transportation infrastructure will be required before certain development thresholds are met, and these conditions will be detailed in the CDD zoning and associated approvals for the Plan area. It is currently projected that the Ruby Tucker Park expansion will happen in the mid- to long- term, however, if and

when the redevelopment site adjacent to the park comes in sooner, the City will work with the developer to implement the open space expansion at that time. The Park will be designed in consultation with the community.

Because of the complex, urban infill redevelopment efforts contemplated in the Plan, and due to real estate demand driven market absorption rates which will set practical limits as to how much new development can occur each year, this Plan could take about 20-30 years to fully implement. Real estate development cycles and market demand will determine the actual rate of build out.

The Plan establishes the broad categories that need to be implemented. The specific requirements for phasing and implementation

Chart 1: Public Benefits Phasing



I. DEVELOPMENT SUMMARY TABLE

Plan/CDD Notes:

In order to ensure that the area develops according to the vision established in the Plan and within the capacity of current and planned infrastructure, the development summary table establishes the maximum development parameters for each block. Each block in the CDD is identified by number in the Development Summary Table and the Development Blocks Map (Figure A) below. It should be noted that while the development table reflects maximum development on each block, the final amount of development for each site will be determined based on all applicable requirements of the Plan and Design Standards as part of a future development special use permit process.

Chart 2: Development Summary Table

District #	Block #	Applicable notes (see notes page below)	Residential		Townhouse (max. GFA) Includes garages. See Note 1	Townhouse (max. units)	Office	Hotel	Retail and Maker		Maximum Total (sq. ft.)			
			1a	1b			2	3	4	5				
			Multifamily Residential (max. GFA)	Multifamily Residential (max. units)			Office, Medical Care Facility (max. GFA)	Max. GFA	Retail (Primary/Secondary) (max. GFA)	Retail (Maker Space Required) (max. GFA) See Note c				
1	1	f	0	0	24,800	10	0	0	0	-	24,800			
	2	f	0	0	24,800	10	0	0	0	-	24,800			
2	3	a b f	140,500	149	0	0	0	0	0	9,500	150,000			
	4	b k l m n	310,000 307,000	330 326	0	0	115,000 125,200	0	40,000 46,000	0	465,000 488,200			
	5	k l m q	200,000	162,600	240	172	0	35,100	0	14	0	31,000	200,000	228,700
	6	k l m q r	0	377,400	0	399	250,000	31,000	90	14	0	81,700	250,000	490,100
	7	b k l m p	270,000	117,100	260	125	0	0	0	145,300	25,000	35,200	295,000	297,600
3	8		0	0	44,900	18	0	0	0	0	44,900			
	9	d	0	0	0	0	0	0	0	21,500	21,500			
	10		66,800	71	14,800	5	0	0	0	6,200	87,800			
	11		75,600	80	0	0	0	0	0	19,200	94,800			
	12		0	0	30,100	13	0	0	0	0	30,100			
	13		80,000	85	0	0	0	0	0	27,900	107,900			
	14		0	0	21,800	9	0	0	0	0	21,800			
	15		0	0	28,000	11	0	0	0	0	28,000			
4	16	b	134,900	143	0	0	0	0	0	29,800	164,700			
	17	b	120,000	127	0	0	0	0	0	6,100	126,100			
4	18	f	0	0	14,800	5	0	0	0	-	14,800			
	19		79,700	85	0	0	0	0	0	8,600	88,300			
	20		68,200	72	0	0	0	0	0	5,900	74,100			
	21		61,700	65	0	0	0	0	0	5,600	67,300			
CDD Total			1,607,400 1,791,500	1,707 1,900	454,000 270,100	171 109	115,000 125,200	145,300	65,000 193,900	140,300	2,381,700 2,676,300			

1. Gross Floor Area (GFA) is defined as the sum of all gross horizontal areas under a roof or roofs. These areas are measured from the exterior faces of walls or from the centerline of party walls. Elevator and stair bulkheads, multi-story atriums and similar volumetric construction, not involving floor space are excluded. Below grade parking structure is excluded. See notes k and L on pg 23 for blocks 4-7.
2. The floor area defined for each block within CDD #24 is a maximum floor area subject to compliance with the Plan, the CDD requirements, and applicable requirements of the Zoning Ordinance.
3. Community facilities, public buildings and associated accessory uses may be provided on any block and shall not be deducted from the maximum permitted gross floor area; however the uses shall be subject to height requirements, the Plan and other applicable elements as part of the DSUP process.
4. For lots identified in the Plan with maker spaces on all four street-facing facades, if not feasible to provide maker spaces on all four sides, a minimum of 3 street-facing facades shall be used to provide maker spaces.
5. The mix of office and residential uses shall be consistent with the intent of the Plan.
6. Any conversions between residential units and commercial floor area shall occur at the ratio defined in row 11.
7. For lots identified in the Plan as appropriate for a gas/charging station, development associated with those uses may be exempted from GFA with SUP approval to a maximum of 6,000 sf.
8. Bonus density achievable through section 7-700 shall not be applicable in CDD #24.
9. The locations for uses shall be pursuant to the Land Use Map in the Plan.
10. Conversion rate from GFA sf to dwelling unit = 940 sf the final number of units to be determined at time of DSUP.
11. Residential sq.ft may be used as office. To be determined during DSUP process.
12. Hotel use may be permitted within the Plan area. The final location shall be determined during the DSUP process.

Figure A: Development Blocks



NOTES

Block/Lot Specific:

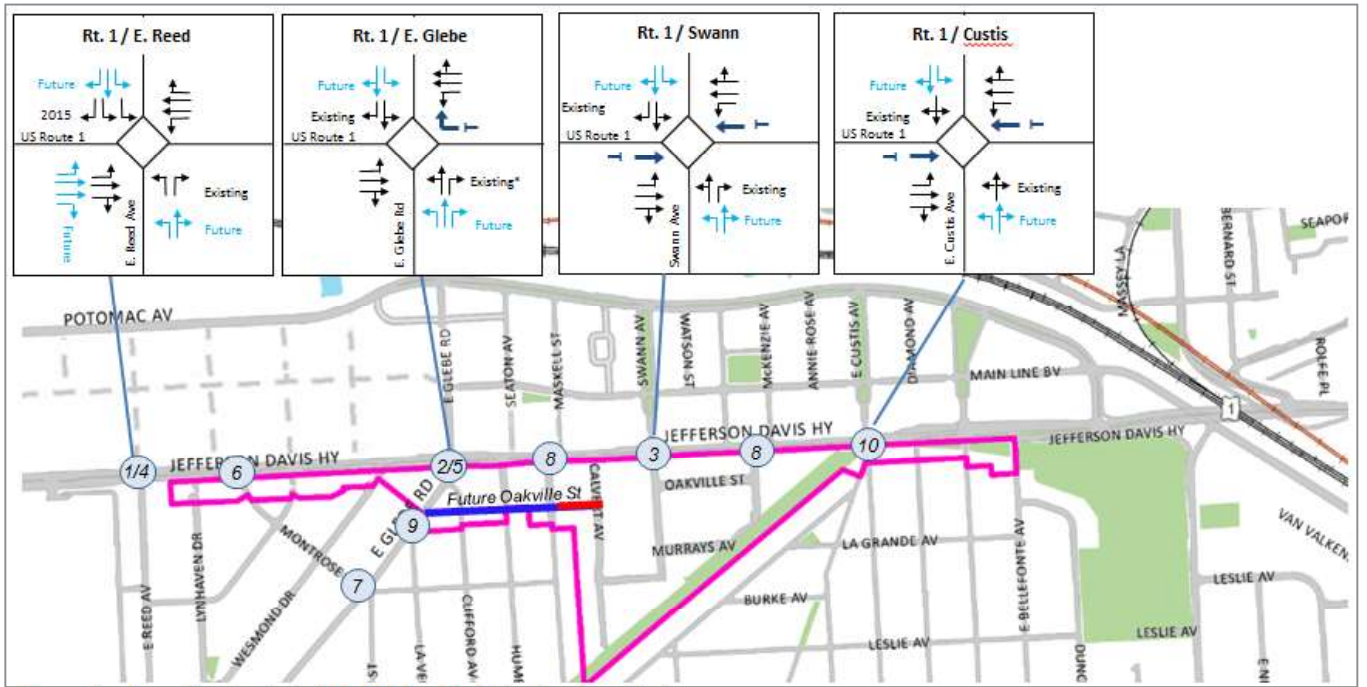
- a. Based on heights in the approved Oakville Triangle & Route 1 Corridor Vision Plan, as amended October 2015. If greater heights are allowed, maximums may be increased to the approval of PC & CC.
- b. A portion or all of residential gross floor area may be used for office. If approved by the City Council through a DSUP submission.
- c. If maker retail space is not feasible, as fully demonstrated through a market study, the sq. ft. may be converted to retail use (preferable), or alternatively to residential through the DSUP process, in which case the a conversion ratio shall be per row 11 above.
- d. Block 9: as part of the CDD, no additional development is assigned to this block. Uses within this block shall be limited to Maker Spaces pursuant to CDD #24.
- e. Not used.
- f. Maker space retail may be provided on this block.
- g, h, i. Not used.
- k. Service/BOH areas are not listed in this chart and may be calculated above the max. GFA in the Development Summary Table.
- l. Above grade structured parking areas are not listed in this chart and may be calculated above the max. GFA in the Development Summary Table (above)10. The locations for uses shall be pursuant to the Land Use Map in the Oakville Triangle & Route 1 Corridor Vision Plan.
- m. Development (sf) based on Preliminary CDD Concept Plan ~~CDD2014-0002 submission 10/29/15~~ as amended.
- n. Includes Block 4A and Block 4B.
- o. Not Used.
- p. Room numbers shall be commensurate with the approved CDD Concept Plan ~~CDD2014-0002 (max. 189 rooms)~~.
- q. ~~Townhouses shall be liner units or as generally depicted per the approved CDD Concept Plan CDD2014-0002~~ Final design and configuration of townhouses will be determined during the DSUP process.
- r. As per all blocks, required retail identified in the plan at ground floor locations is required. Non-ground floor retail may be converted to an alternate use through the DSUP process.

II. TRANSPORTATION IMPROVEMENTS

The following transportation improvements are required in/near the Plan area with the responsibility/funding as identified in the table below

PHASE	DESCRIPTION	RESPONSIBILITY/ FUNDING	REQUIRED TIMING
1	Construction of lane modifications eastbound Swann Ave. at Route 1, as specified in Oakville Triangle CDP	Developer of Oakville Triangle	Operational prior to issuance of first Certificate of Occupancy (CO) for <u>blocks 4A or Z</u> development within the Oakville Triangle CDP area (not including above or below grade parking).
1	Initial work on Phase 1 improvements at Route 1/E. Glebe Rd. (eastbound right turn modified to allow overlap right turns), as specified in Oakville Triangle CDP	Developer of Oakville Triangle to provide \$200,000 <u>\$350,000</u>	Prior to release of the final site plan for blocks 4A or 7 the first block within the Oakville Triangle CDP area.
1	Route 1 / E. Reed signal modifications (Address through Potomac Yard North SAP Amendment)	Developer contributions and /or incremental tax	Prior to 800,000-sf of development <u>With North Potomac Yard first building</u> (net of parking)
1	<u>New signalized intersection improvements and pedestrian crossings at Route 1/Fannon Street</u>	<u>Developer of Oakville Triangle</u>	<u>Operational prior to issuance of Certificate of Occupancy (CO) for the Block 4B.</u>
2	Phase 1 improvements at Route 1/E. Glebe Rd.: Signal phasing and construction from Route 1 to new North-South Road	Developer of Oakville Triangle to provide \$1,400,000	Earlier of: (1) 1 year after issuance of final CO for completion of Phase I (Block A1, B & D) or (2) prior to first final site plan submission for fourth block in the project.
2	Route 1/E. Reed intersection improvements: Westbound lanes modified from exclusive left turn lane and shared through/right lane to exclusive left, through and right turn lanes	Developer contributions and /or incremental tax	Prior to issuance of CO at 1.6 million sf (Net without parking)
2	New signal at Montrose Ave./ Route 1.	Developer contributions and /or incremental tax	Prior to issuance of CO at 1.6 million sf (Net without parking)
2	Improvements on Montrose Avenue and at the intersection of Montrose Avenue at East Glebe Road / Ashby Street	Developer contributions and /or incremental tax	Prior to issuance of CO at 1.6 million sf (Net without parking)
2	New Route 1 pedestrian crossing/signal between East Custis Avenue and East Glebe Road.	Developer contributions and /or incremental tax	Prior to issuance of CO at 2 million sf (Net without parking)
3	Route 1 / E. Glebe Intersection improvements (Phase 2 from the new North-South Road to LaVerne Avenue) or comparable improvement – in consultation with property owners.	Developer contributions and/ or incremental tax	Prior to issuance of CO at 2 million sf (Net without parking)
3	Modification of lane configuration at Route 1/Custis Ave.	Developer contributions and/ or incremental tax	Prior to issuance of CO at 2 million sf (Net without parking)

Figure B: Oakville-Route 1 Transportation Improvement Phasing



■ PHASE 1* - OPERATIONAL PRIOR TO 800,000 SQ. FT. (NET WITHOUT PARKING)

- 1 - Signal modifications at Route 1 and E. Reed Ave* (*with North Potomac Yard first building*)
- 2 - Signal modifications at Route 1 and E. Glebe Road
- 3 - Lane modifications at Route 1 and Swann Ave.
- 8 - New signalized intersection improvements and pedestrian crossings at Route 1/Fannon Street

■ PHASE 2 - OPERATIONAL PRIOR TO 1.6 MILLION SQ. FT. (NET WITHOUT PARKING)

- 4 - Intersection improvements at Route 1/ E. Reed
- 5 - Intersection improvements at Route 1/ E. Glebe (Phase 1)
- 6 - New signal at Route 1 and Montrose
- 7 - Improvements on Montrose and at Montrose/Ashby/E. Glebe
- 8 - Pedestrian crossing across Route 1 (at Fannon St. or Raymond Ave)

■ PHASE 3 - OPERATIONAL PRIOR TO 2 MILLION SQ. FT. (NET WITHOUT PARKING)

- 9 - Improvements at Route 1/E. Glebe (Phase 2)
- 10 - Lane modifications at Route 1/ E. Custis Ave

III. NON-TRANSPORTATION IMPROVEMENTS

The following transportation improvements are required in/near the Plan area with the responsibility/funding as identified in the table below

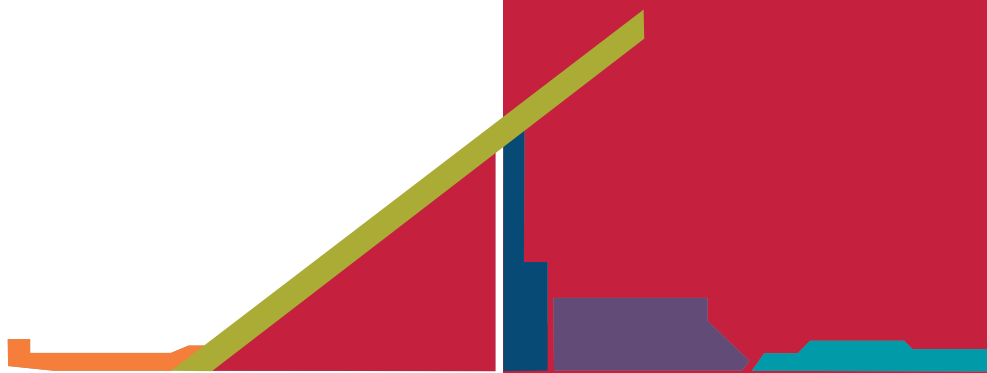
PHASE	DESCRIPTION	RESPONSIBILITY/ FUNDING	REQUIRED TIMING
All	Affordable Housing: The City will work with developers to achieve all applicable Housing Master Plan goals and objectives, including the desire to achieve new onsite affordable housing units with re-zonings.	Developer contributions	Phased throughout development.
OT	Affordable Housing: The Oakville Triangle site will provide 65 units of onsite affordable housing integrated within the development, at an approximate value of \$7.8M (2015).	Developer of Oakville Triangle	Phased throughout development of Oakville Triangle.
All	Streetscape/Undergrounding on Route 1 Frontage: All blocks expected to complete these improvements consistent with specifications established in the Plan as part of the redevelopment of their site. The remainder of the redevelopment sites will also be responsible for locating utilities on their site below grade as part of the development special use permit process. The cost for undergrounding utilities for some smaller and shallow blocks (see Plan, Figure 17, Rte 1 Streetscape Improvements) could be provided through the Plan area tax increment fund.	Standard Development Conditions, Developer contributions and/or incremental tax	Phased throughout development.
OT	Streetscape and Undergrounding on Route 1 Frontage: All utilities for Oakville Triangle Route 1 frontage to be located below grade.	Developer of Oakville Triangle	As part of construction of first building on the site.
OT	Streetscape and Undergrounding: The remainder of the above grade utilities for the site will be located below grade.	Standard Developer conditions	With redevelopment of each block.
All	Open Space: Consideration in the Plan's phasing ensures that sufficient, consolidated, high quality open space is provided based on planned development. On-site open space is required for the Oakville site consistent with the Plan. Significant on-site open spaces (e.g. Swann Plaza) are required to be provided with adjacent development blocks.	Developer of Oakville Triangle and other blocks in the Plan area	With redevelopment of each block.

PHASE	DESCRIPTION	RESPONSIBILITY/ FUNDING	REQUIRED TIMING
OT	Mount Jefferson Park: Construction of and funding for the Mount Jefferson Park Improvement called for in the Concept Plan will be completed by the developer in conjunction with redevelopment of the Oakville Triangle site.	Developer of Oakville Triangle	Timing for initiation and completion of park improvements is specified in the CDP for the Oakville Triangle site. Improvements shall be fully constructed and accepted by the City <i>in accordance with CDD conditions</i> prior to issuance of first Certificate of Occupancy for any building in the CDP area, or within a maximum 24 months of issuance of permit to demolish any building or structure in or adjacent to Park (whichever is earlier).
	Mount Jefferson Park: Additional improvements to the southern section of Mount Jefferson Park will be designed and provided in conjunction with redevelopment of adjacent Block 3 parcels, with requirements to be outlined in related DSUP.	Developer of Block 3	With redevelopment of the block.
All	On-site open space is required for Oakville site consistent with the Oakville Plan.	Developer contributions	With redevelopment of each block.
	Expansion of Ruby Tucker Park	Developer contributions and/or incremental tax	In conjunction with redevelopment of the property at intersection of Route 1/Lynhaven Drive, adjacent to park, or earlier if feasible

INTENTIONALLY LEFT BLANK

2

OVERVIEW - URBAN DESIGN STANDARDS AND GUIDELINES



I. ROLE OF THE STANDARDS AND GUIDELINES

The Design Standards & Guidelines are intended to provide requirements and guidance in written and graphic form for projects in the Plan area to implement the Vision Plan. Buildings, open space and the public realm shall be evaluated based on compliance with the applicable approvals,

zoning requirements, existing City Plans and policies, such as the Complete Street Design Guidelines, Green Building Policy, Housing Master Plan, and this document. Projects are required to comply with all Design Standards, graphics, and figures referenced herein to ensure that the built environment exhibits the

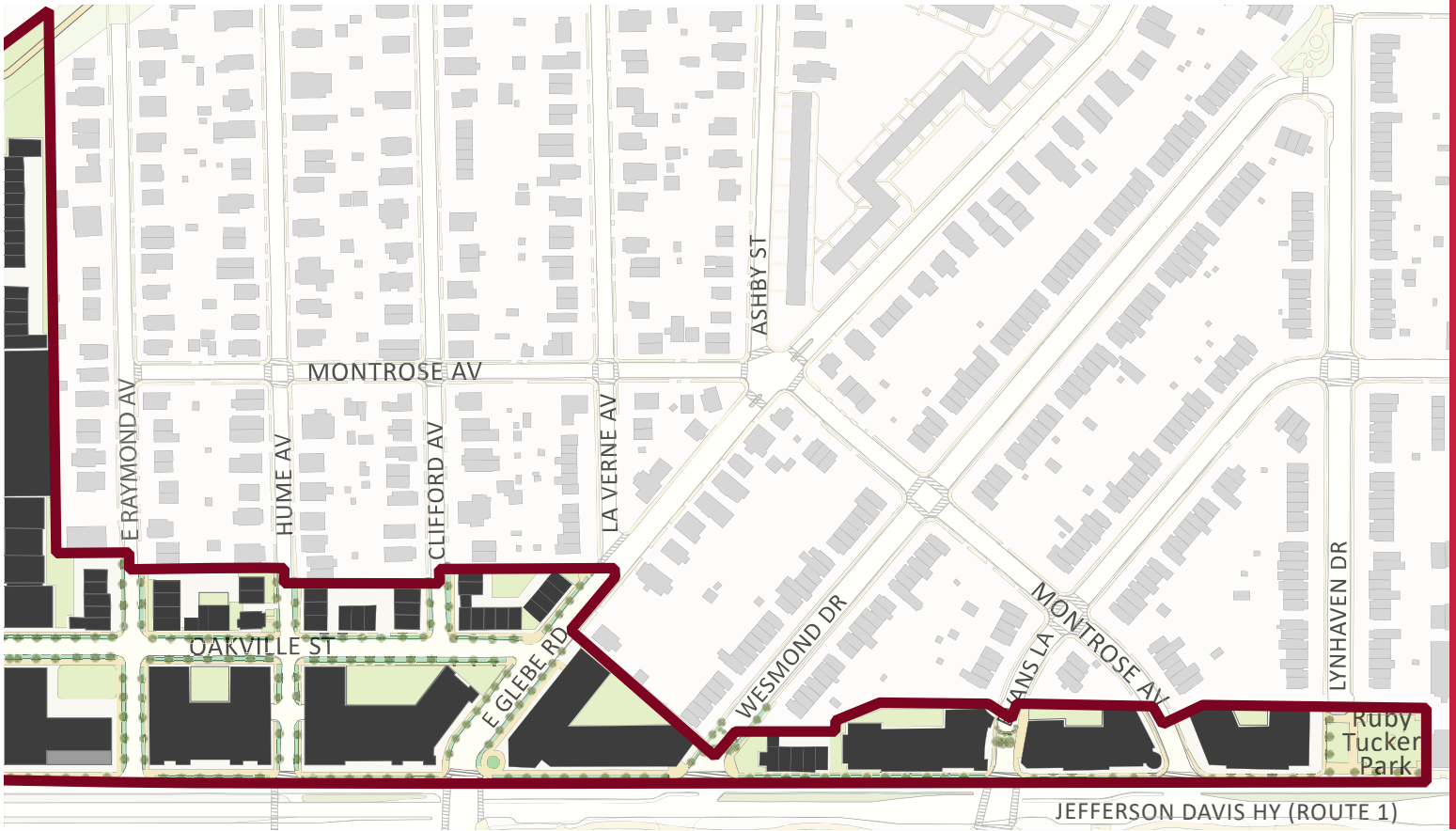
highest standards of design. Projects are also strongly encouraged to comply with the applicable Guidelines referenced herein.

Figure 3 : Illustrative Plan



DESIGN STANDARDS AND GUIDELINES

Note: Figure 3 is for illustrative purposes only. The final design and configuration of buildings and open spaces shall be designed as part of the development review process including compliance with these Standards and applicable requirements and City policies.



GENERAL STANDARDS

- 2.1 Projects are required to comply with all applicable Design Standards herein, and comply with the applicable approvals, zoning requirements, and existing City Plans and policies, such as the Complete Street Design Guidelines, Green Building Policy, Housing Master Plan, etc. These Standards and Guidelines ensure high quality design and architecture within the Plan Area. Standards shall require a higher level of review and the expectation is that new development will be required to comply with these Standards. Any deviation from the standards contained herein shall be evaluated and determined through the Development Special Use Permit (DSUP) process. Guidelines are advisory and new development is encouraged to incorporate them as appropriate.
- 2.2 Provide taller building height adjacent to the transit stops along Route 1 and in close proximity to the Potomac Yard Metrorail station.
- 2.3 Improve and enhance the Route 1 frontage by locating the utilities below grade and providing a 25 ft. setback-streetscape for the buildings adjacent to Route 1.

OPEN SPACE

- 2.4 Improve Mount Jefferson Park consistent with the Plan approved by the Parks and Recreation and Planning Commissions including the at-grade trail extension at the southern end of the Park.
- 2.5 Provide a ~~central public urban plaza~~ new central public open space within Oakville Triangle that includes usable amenities that help to meet the recreational needs of new residents.
- 2.6 Provide ~~five~~ four new ground level public green spaces adjacent to Mount Jefferson Park.
- 2.7 Expand the existing Ruby Tucker Park.

STREETS

- 2.8 Develop a generally orthogonal street grid pattern, including the introduction of several new streets.
- 2.9 Provide a hierarchy of streets that identify the character and function of each street.
- 2.10 Provide on-street parallel parking where feasible for the new streets (excluding Route 1).
- 2.11 New streets are designed to be low speed, local serving, pedestrian-oriented to encourage bicycle and transit use.
- 2.12 New streets should accommodate stormwater management.
- 2.13 Where possible, new streets should include safe, separate, lanes for bicycle traffic as recommended in the Transportation Master Plan.

PUBLIC REALM

- 2.14 Create pedestrian-oriented streetscapes.
- 2.15 Incorporate the provision of safe, efficient, and convenient pedestrian and bicycle circulation systems that connect neighborhoods, transit, and open spaces.
- 2.16 Provide careful attention to sidewalk design and detailing to support the walkability and sustainability of the Plan area.

BUILDINGS

- 2.17 Create an urban building scale and relationship between buildings, streets and open spaces that ensure and maximize walkability, create compact development and maximize the use of transit.
- 2.18 Create a variety of building massing (footprint and height) for the townhouses, multi-family, office and hotel uses.
- 2.19 Provide appropriate transitions to the existing single-family homes and townhouses to the west of the Plan area through scale transitions, setbacks and landscape buffers.

3

PLAN DISTRICTS



I. DISTRICTS

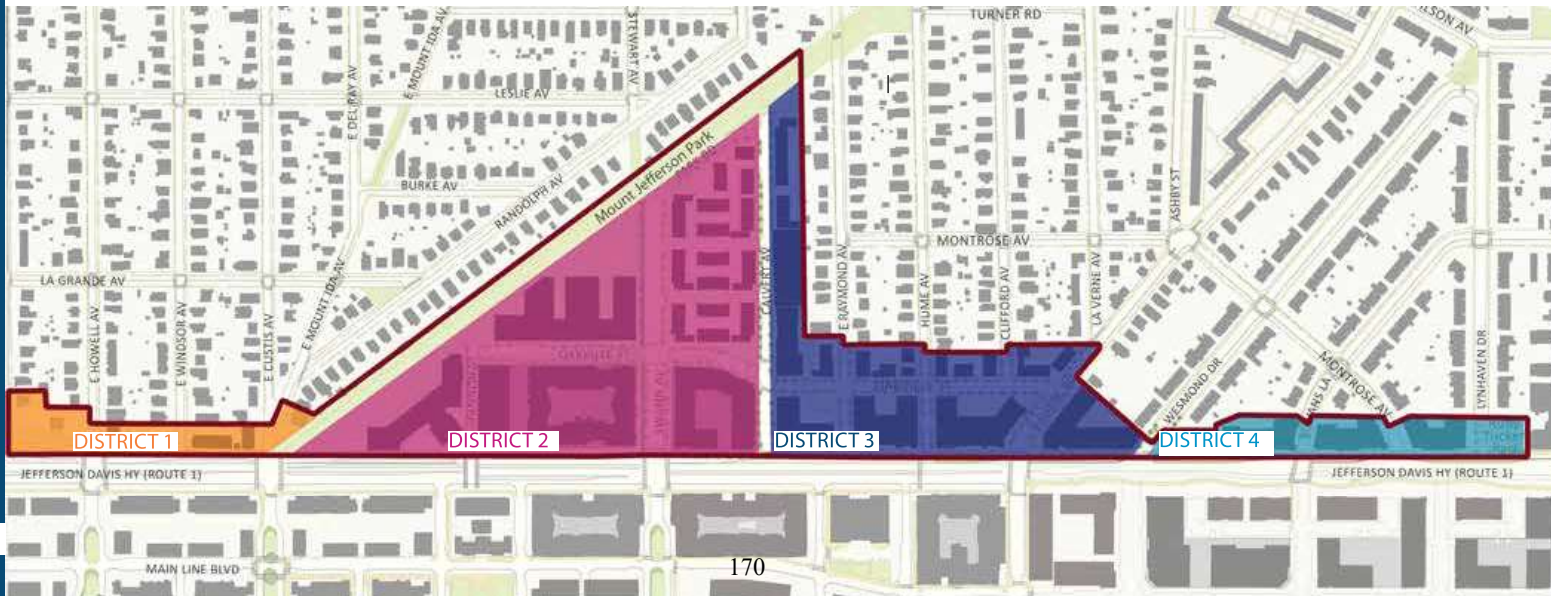
A defining element of Alexandria is its unique and distinct neighborhoods. The districts identified in the Plan are intended to appear and function as extensions of the adjoining unique neighborhoods of Del Ray, Lynhaven and Potomac Yard. The Plan districts are based on the depths of blocks, overall size, existing buildings and adjoining uses such as the Mount Jefferson Park.

The districts are delineated for planning purposes and serve as the basic structure for the design standards.

**“ WE MUST NOT BUILD HOUSING
WE MUST BUILD COMMUNITIES.”**

- MIKE BURTON

Figure 4: Plan Districts



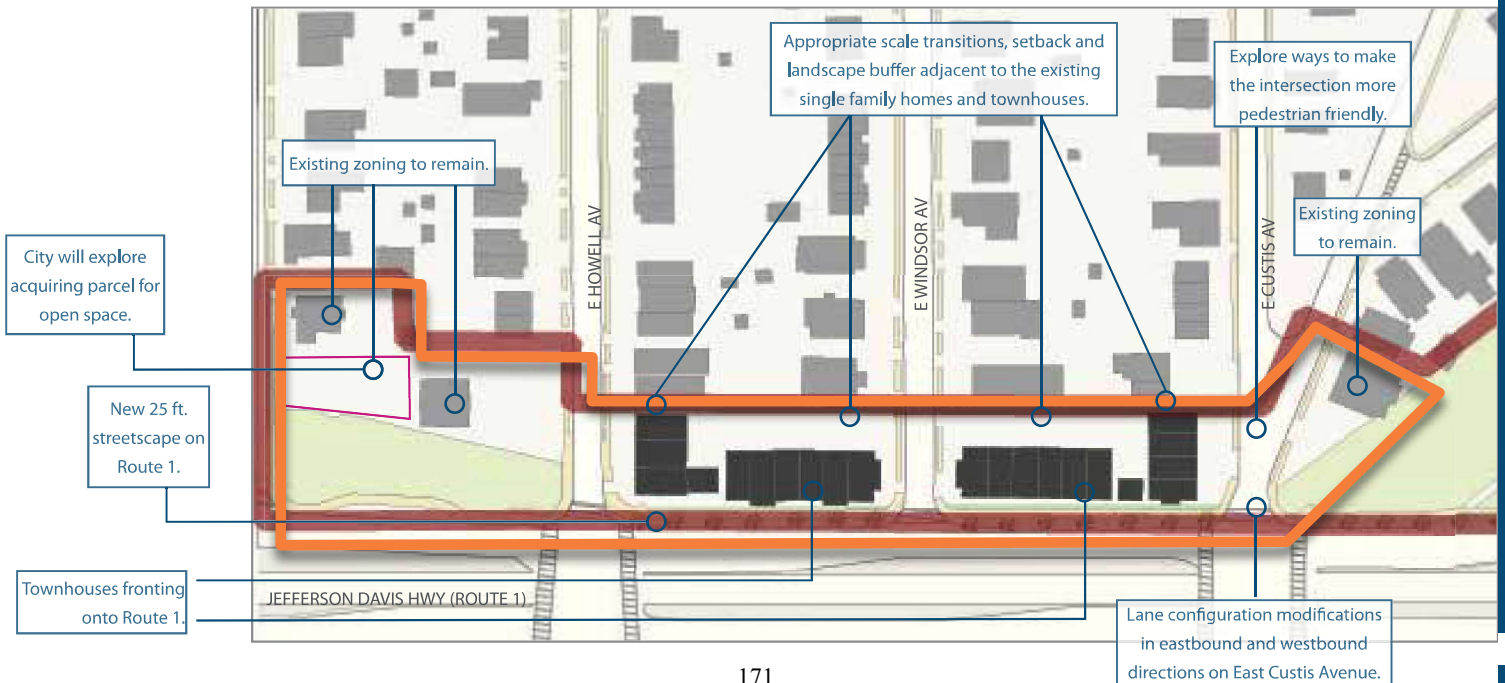
A. DISTRICT 1

Because of its shallow depth and limited length, there are limitations on the building types that can be located within this district. In addition, the adjoining single-family homes and townhouses are located immediately to the west, making appropriate height and scale transitions an important component of redevelopment for this district. The Plan recommends townhouses for this district. The townhouses will need to provide a variety of building heights, scale transitions and

appropriate setbacks to the adjoining single-family homes and townhouses to the west as required herein. The townhouses will also be required to provide a landscape buffer setback adjacent to the existing homes to the west. The townhouses will be comparable in scale to the adjoining townhouse units within Potomac Yard.

As further described in the Land Use Section in Chapter 4, this district has the potential to incorporate “maker space” in the first floor spaces.

Figure 5: District 1 Illustrative Plan



DISTRICT 1 CHARACTER (ILLUSTRATIVE EXAMPLES)

TOWNHOUSES



ROUTE 1
STREETSCAPE

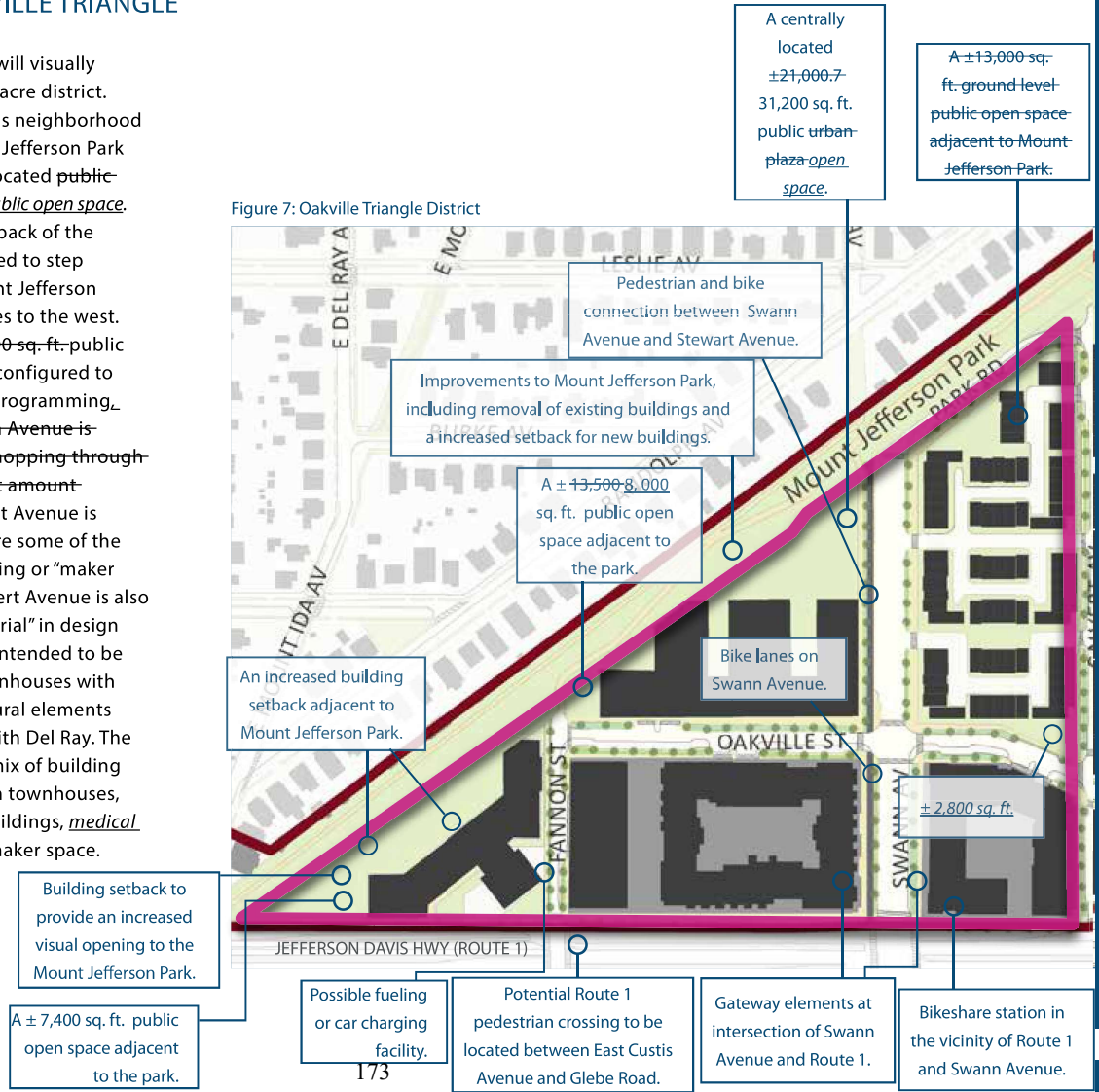


Figure 6: View Looking South, Route 1 and Custis Ave (Adjacent to Mount Jefferson Park)

B. DISTRICT 2 - OAKVILLE TRIANGLE

Establishing the street grid will visually unify this approximately 15-acre district. The defining elements of this neighborhood will be the adjoining Mount Jefferson Park frontage and the centrally located public urban plaza $\pm 31,200$ sq. ft. public open space. The building height and setback of the buildings have been designed to step down to the adjoining Mount Jefferson Park and single-family homes to the west. The centrally located $\pm 21,000$ sq. ft. public open space will need to be configured to accommodate events, and programming, and social gatherings. Swann Avenue is intended to provide retail shopping through the provision of a significant amount of ground floor retail. Calvert Avenue is intended to be an area where some of the existing neighborhood-serving or “maker uses” could be located. Calvert Avenue is also intended to be more “industrial” in design and character. Park Road is intended to be smaller scale 30’-45’ tall townhouses with front porches and architectural elements and materials compatible with Del Ray. The district is intended to be a mix of building types and uses ranging from townhouses, multi-family, office, hotel buildings, *medical care facility*, and retail and maker space.

Figure 7: Oakville Triangle District



OAKVILLE TRIANGLE CHARACTER (ILLUSTRATIVE EXAMPLES)

OAKVILLE TRIANGLE - SWANN AVENUE



PARK ROAD



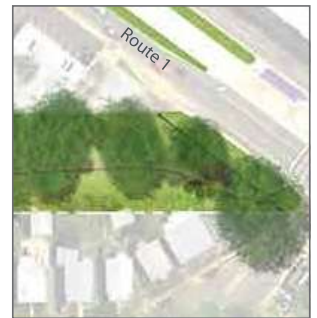
CALVERT AVENUE



PLAN DISTRICTS

DISTRICT 2 - OAKVILLE TRIANGLE - MOUNT JEFFERSON PARK IMPROVEMENTS

MOUNT
JEFFERSON
PARK

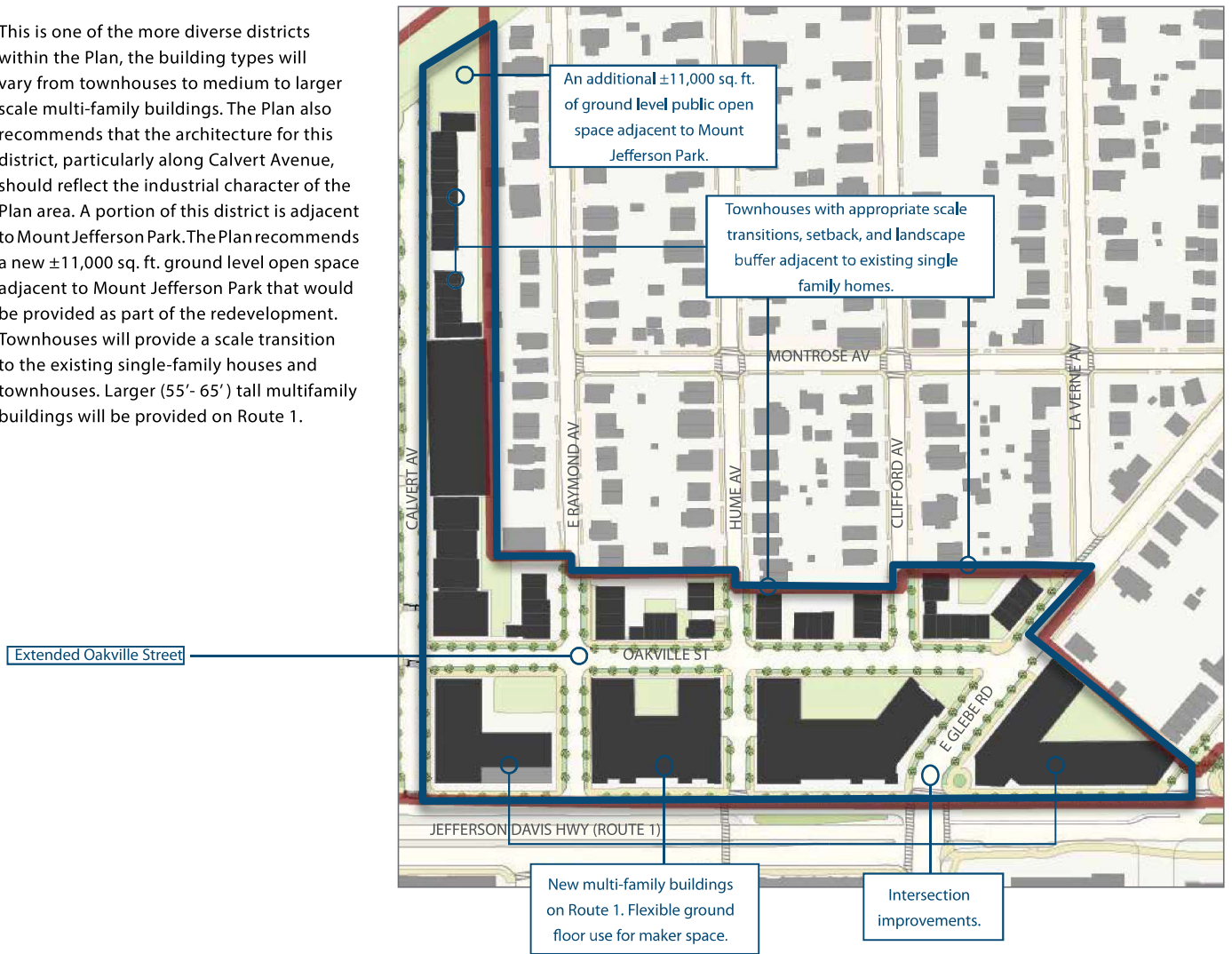


Note: Illustrative design shown are based on a previous concept. The final design for adjacent open spaces to Mount Jefferson Park will be designed during the DSUP process.

C. DISTRICT 3

This is one of the more diverse districts within the Plan, the building types will vary from townhouses to medium to larger scale multi-family buildings. The Plan also recommends that the architecture for this district, particularly along Calvert Avenue, should reflect the industrial character of the Plan area. A portion of this district is adjacent to Mount Jefferson Park. The Plan recommends a new $\pm 11,000$ sq. ft. ground level open space adjacent to Mount Jefferson Park that would be provided as part of the redevelopment. Townhouses will provide a scale transition to the existing single-family houses and townhouses. Larger (55'- 65') tall multifamily buildings will be provided on Route 1.

Figure 8: Plan District 3 Illustrative Plan



DISTRICT 3

The building types will range from multi-family buildings on Route 1 and a portion of Calvert Avenue and predominantly townhouses adjacent to existing neighborhoods. The Plan also permits office uses at the intersection of Route 1 and East Glebe Road, where taller heights are permitted due to the proximity to the transit stop on Route 1, and the Potomac Yard Metrorail station. On Calvert Avenue, the majority of the ground floor uses are spaces where some of the existing types of uses could be located. Flexible ground floor uses for maker type uses are recommended for the Route 1 and East Glebe Road frontages.

This district will include an extended Oakville Street connecting Calvert Avenue to East Glebe Road. As part of the redevelopment, an analysis will be conducted to determine appropriate design for the new intersection of Oakville Street and East Glebe Road. A reconfiguration of the intersection of East Glebe Road and Route 1 is recommended as part of the Plan. Similar to the other districts, appropriate scale transitions and setbacks will need to be provided adjacent to the existing single-family homes and townhouses to the west.

Figure 9: East Glebe-Route 1 Intersection, Conceptual Design



Figure 10: View of Proposed Oakville Street Extension, Looking South



DISTRICT 3 CHARACTER (ILLUSTRATIVE EXAMPLES)

OAKVILLE STREET
EXTENDED



Figure 11: View of Proposed Oakville Street, Looking South

ROUTE 1
FRONTAGE

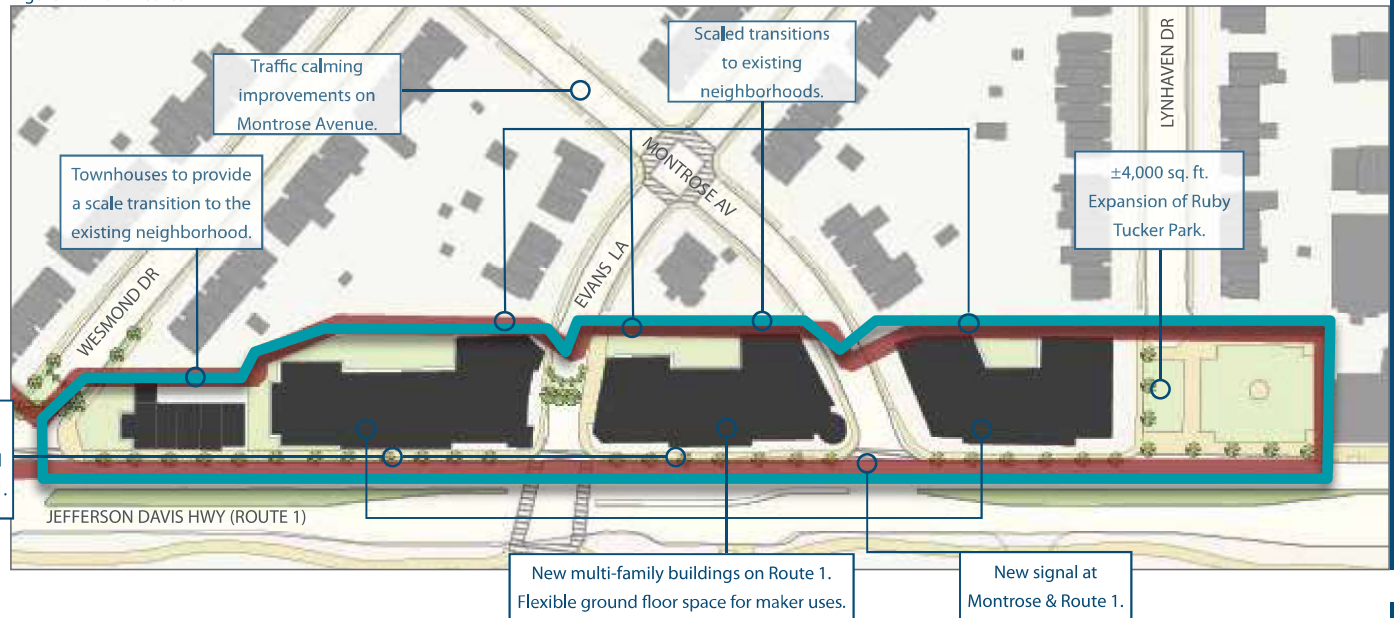


D. DISTRICT 4

The lots/blocks within this district are relatively shallow, creating challenges for redevelopment. In addition, the townhouses within Lynhaven are located immediately to the west of this district making appropriate height and scale transitions an important component of redevelopment. The Plan recommends townhouses for the shallow parcels and medium-scale multi-family buildings for the larger parcels, requiring setbacks and height and scale transitions to the existing townhouses. Flexible ground floor space for maker uses are recommended

along the Route 1 frontage. The Plan also recommends expanding the existing $\pm 10,450$ sq. ft. Ruby Tucker Park by approximately $\pm 4,000$ sq. ft., using the existing Lynhaven Drive right-of-way.

Figure 12: Plan District 4

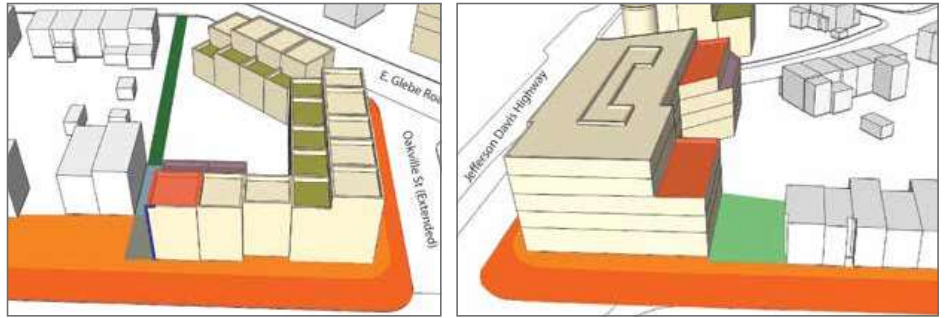


DISTRICT 4 NEIGHBORHOOD CHARACTER (ILLUSTRATIVE EXAMPLES)

ROUTE 1



SCALE TRANSITIONS



EXPANDED RUBY TUCKER PARK



Figure 13: Ruby Tucker Expansion (Route 1 Looking South)

Note: Design of open space is for illustrative purposes only.

PLAN DISTRICTS

E. ROUTE 1 FRONTAGE

The Route 1 frontage is a visually prominent gateway for the City, and serves as a “front door” across all Districts within the Plan area. The Plan recommends enhancements to the Route 1 frontage, including a 25 ft. streetscape, continual street trees, below grade utilities across all Districts, enhanced building requirements and an improved entrance to the Mount Jefferson Park. The heights on Route 1 are intended to provide a variety of heights ranging from 45 ft. to 100 ft. at Swann Avenue.

The Plan calls for access management on Route 1, restricting curb cuts, and requiring access to new buildings from side streets.

Figure 14: Illustrative of Route 1 Frontage Streetscape improvements and underground utilities



Figure 15 Route 1 Streetscape Cross-Section (Commercial)



Figure 16A: Route 1 Streetscape Cross-Section (Residential- Townhouse)

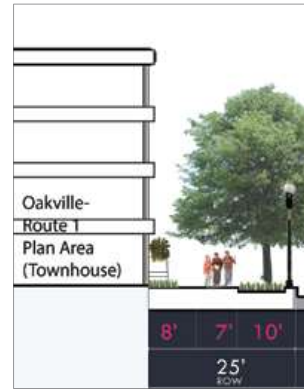


Figure 16B: Route 1 Streetscape Cross-Section (Residential- Multifamily)

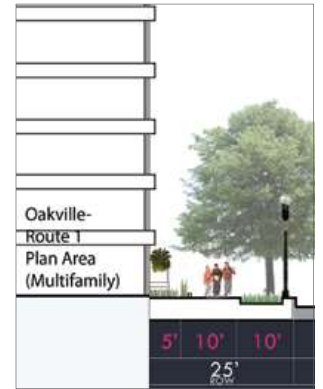


Figure 17: Route 1 Streetscape Improvements



INTENTIONALLY LEFT BLANK

4

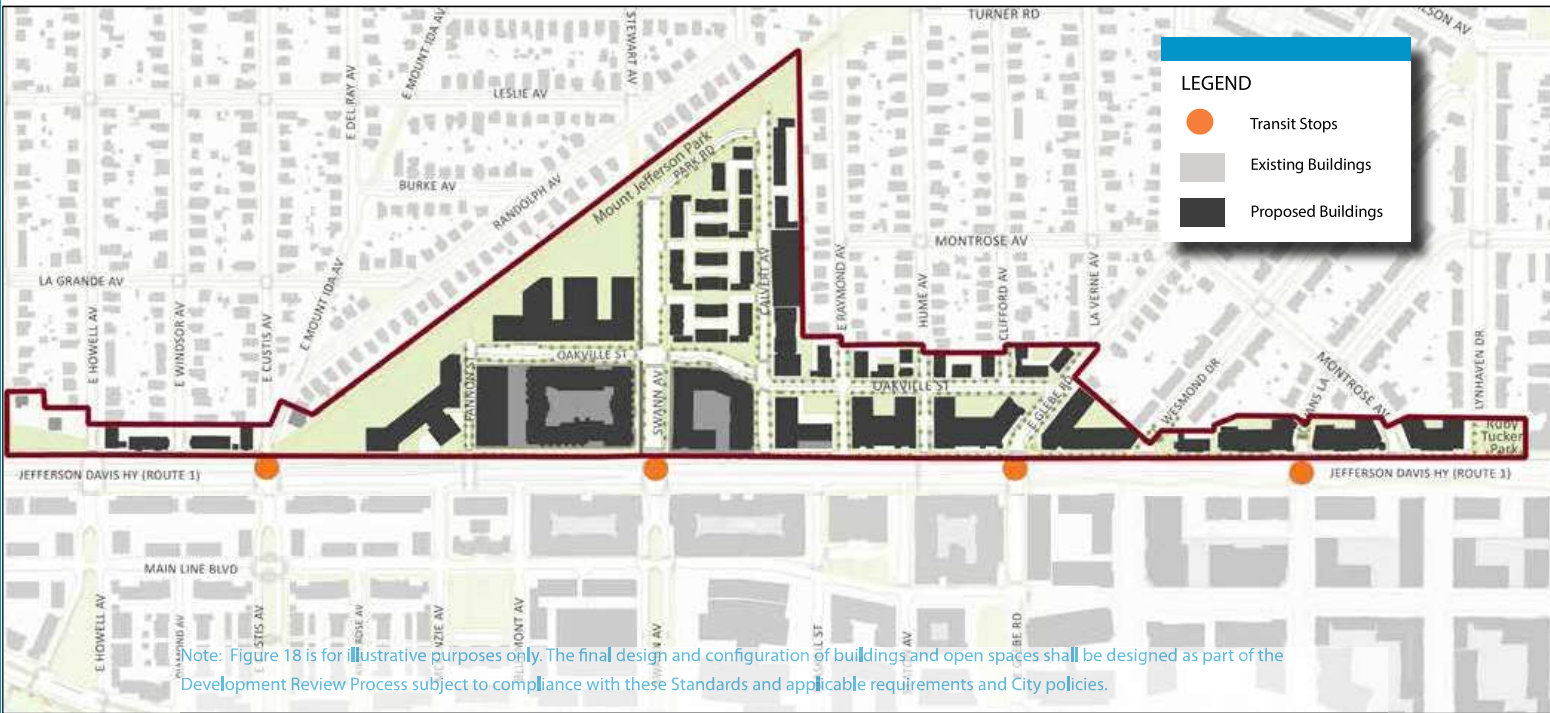
PLAN FRAMEWORK



ILLUSTRATIVE PLAN

The Plan creates a framework for streets, blocks, zoning, heights, and open spaces, which function as the foundation for the Plan area.

Figure 18: Illustrative Plan

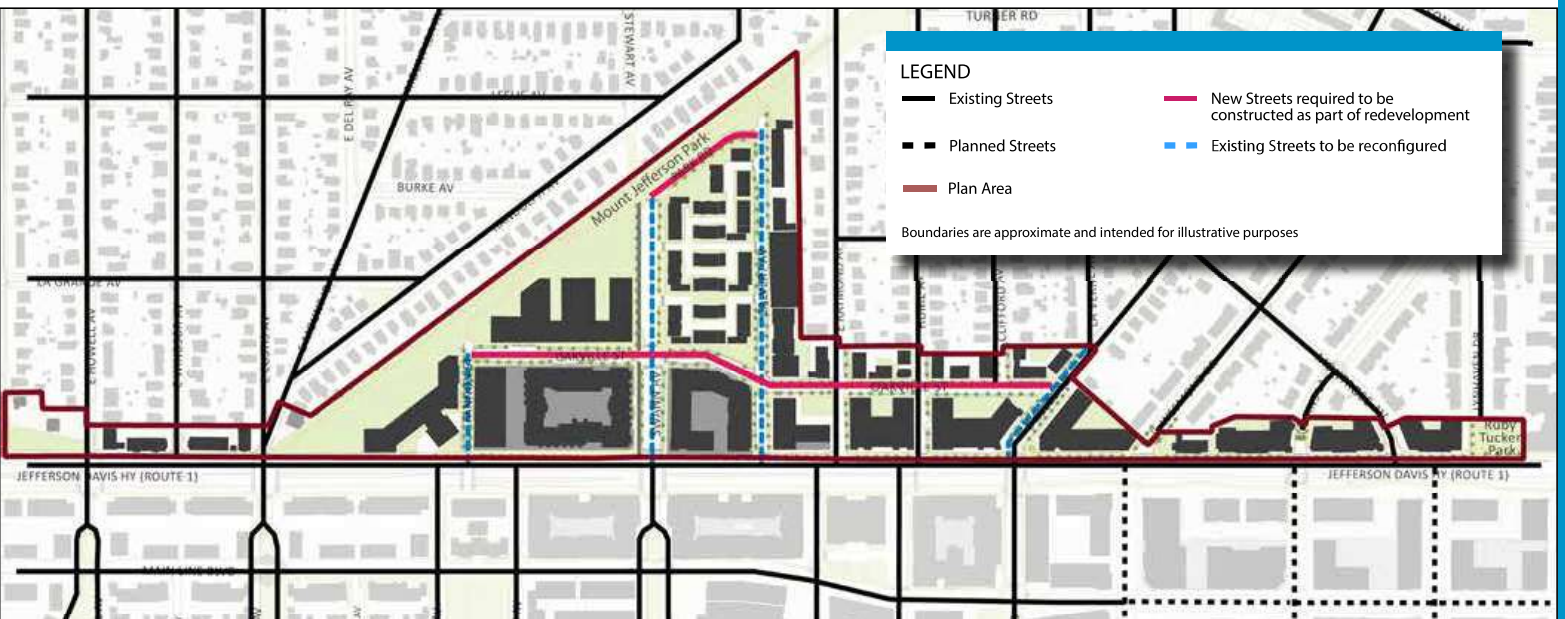


PLAN FRAMEWORK

A. FRAMEWORK STREETS

Figure 19 depicts the framework streets, or the streets that are required to be constructed and/or reconfigured as part of the redevelopment. The graphic also depicts the East Glebe Road and Route 1 intersection improvements.

Figure 19: Framework Streets



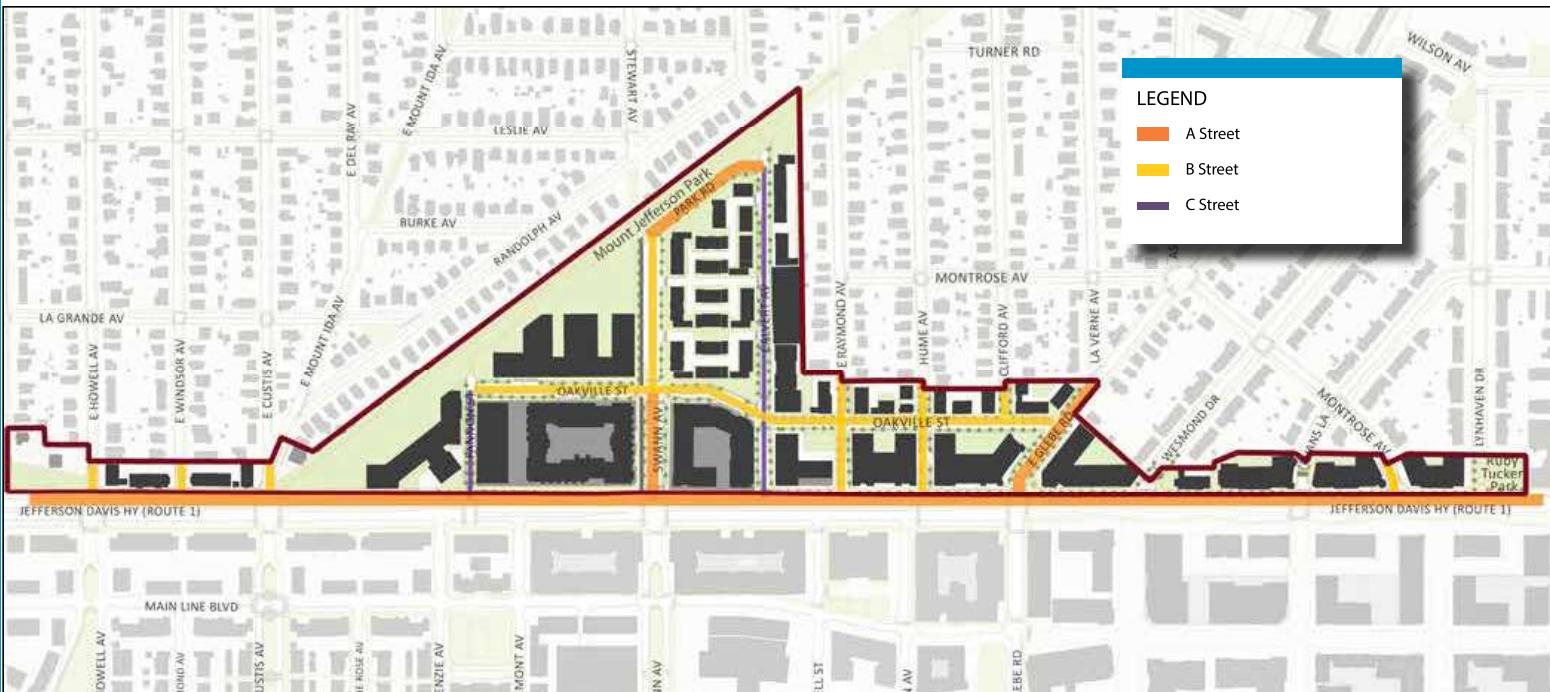
STREET HIERARCHY

In addition, a street hierarchy has been created to identify the character and function of each street. "A" streets are the most prominent, and create an "address" for the important buildings in each district. "B" streets connect "A" streets to each other and to service streets, and provide pedestrian and vehicular circulation for each of the neighborhoods. "C" streets

provide access and service entry to alleys. "C" streets are the least public in nature of all of the streets and, therefore, the least restrictive in design. The "C" streets allow the "A" and "B" street frontages to function as more public primary streets.

A hierarchy of streets is required to maintain a high-quality street environment and address a variety of needs – from the most prominent pedestrian and vehicle streets, to streets that provide service and access.

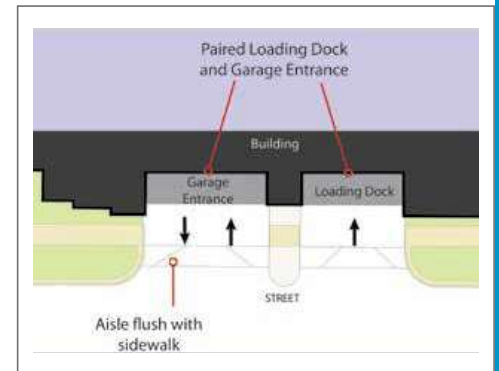
Figure 20A: Street Hierarchy



FRAMEWORK STREETS STANDARDS

- 4.1 The intersection of Glebe Road and Route 1 will need to be reconfigured as generally depicted within the attached cross-sections. ~~The timing and phasing of the improvements will be established as part of the CDD zoning for the Plan area. Funding will be proposed as part of the Oakville Plan CDD for the improvements.~~
- 4.2 A hierarchy of streets, as depicted in Figure 20A, is required to maintain a high-quality street environment and address a variety of needs and functions.
- 4.3 The streets are required to be constructed in the locations generally depicted in Figure 19 and in the dimensions configured in the street cross sections required herein.
- 4.4 "A" Streets: Curb cuts, entrances to parking garages and service bays are prohibited.
 - i. Buildings shall front the street.
 - ii. Active uses shall be located on all street frontages for each level of the building.
 - iii. The higher quality of architectural façade and streetscape treatment shall be used within the street hierarchy.
- 4.5 "B" Streets
 - i. Buildings shall front the street.
 - ii. Active uses shall be located on all street frontages for each level of the building
 - iii. A maximum of one curb cut per block face shall be permitted on each side of the street. To the extent possible, curb cuts should be aligned with curb cuts on the opposite side of the street. Offset curbs may be permitted, where it is deemed necessary. Additional curb cuts may be considered as part of the DSUP process.
 - iv. Main building and pedestrian entrances shall be located along "B" street frontages unless adjacent to an "A" street. A high quality of architectural façade treatment is required.
- 4.6 "C" Streets
 - v. Curb cuts for internal alleys and service shall be located on these streets, unless it can be determined that it is infeasible to do so.
- 4.7 Curb cuts shall be limited to the minimum necessary in number and width.
- 4.8 Residential entrances are encouraged on all streets. Where ground floor retail is provided or required, residential lobby entrances should be minimized but are not prohibited. The final location of residential entrances where retail is provided will be determined as part of the development review process.

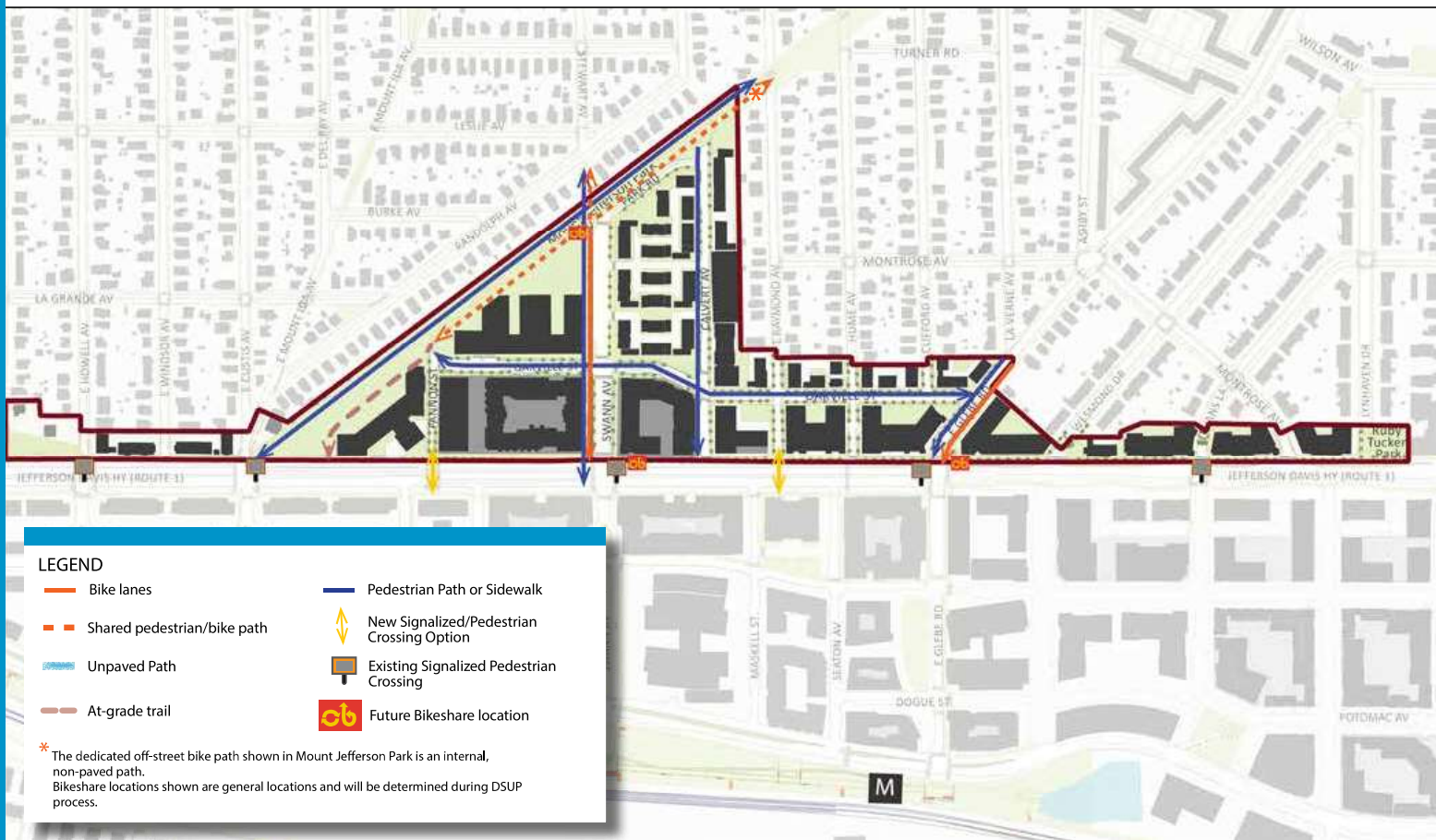
Figure 20B: Curb-Cut Diagram



Paired loading and garage entrances shall provide a drive aisle flush with the sidewalk. For purposes of the Standards, Figure 20B, constitutes one curb-cut.

B. BIKE AND PEDESTRIAN CIRCULATION

Figure 21: Pedestrian and Bike Circulation



PLAN FRAMEWORK

A basic foundation of the Plan is to provide safe attractive and well connected pedestrian environment. The requirements for streetscapes and streets create a pedestrian-oriented neighborhood, encouraging walking and transit use. The pedestrian areas will range in width from 25 ft. on Route 1 to 11 feet on the residential streets. There will also be new connections to the Mount Jefferson Park, to make the park more accessible and safe for all users.

In addition to pedestrian circulation, the bicycle network will be expanded for improved connectivity and safety including portions of Mount Jefferson Park, East Glebe Road, and Swann Avenue. The goal is to provide better connectivity with the Plan area, as well as to adjoining Del Ray, Lynhaven, Potomac Yard, and Potomac Yard Metrorail Station.



BIKE AND PEDESTRIAN NETWORK STANDARDS

- 4.9 The bike and pedestrian network as generally depicted in Figure 21 shall be implemented as part of the redevelopment within the Plan area.
- 4.10 A signalized pedestrian only crossing shall be provided as generally depicted in Figure 21.
- 4.11 The bike and pedestrian areas will be implemented consistent with the attached street cross-sections and the design of the Mount Jefferson Park Plan.



C. BLOCKS

One of the tools to ensure the Plan Area will develop as urban and pedestrian oriented is to require urban, human scaled blocks. Based on the required framework streets, the block sizes are roughly the size of blocks within the surrounding communities of Lynhaven, Del Ray, and Potomac Yard.

It is likely that it will take approximately 20-30 years to fully implement the vision of the Plan. During this time-frame, it may be considered more expeditious or cheaper to create larger “megablocks.” However this document requires that development occur according to the required framework streets and existing streets, which establish blocks within the Plan area.

Figure 22: Development Blocks in Plan Area



D. ZONING

A Coordinated Development District (CDD) is recommended for the Plan Area to implement the Plan. This approach is similar to the zoning used for the adjoining North Potomac Yard and South Potomac Yard. As depicted in Figure 24, the Plan does not propose CDD zoning for some of the properties due to their size and shape. For these properties the existing CSL zoning will be retained.

Figure 23: Existing Zoning

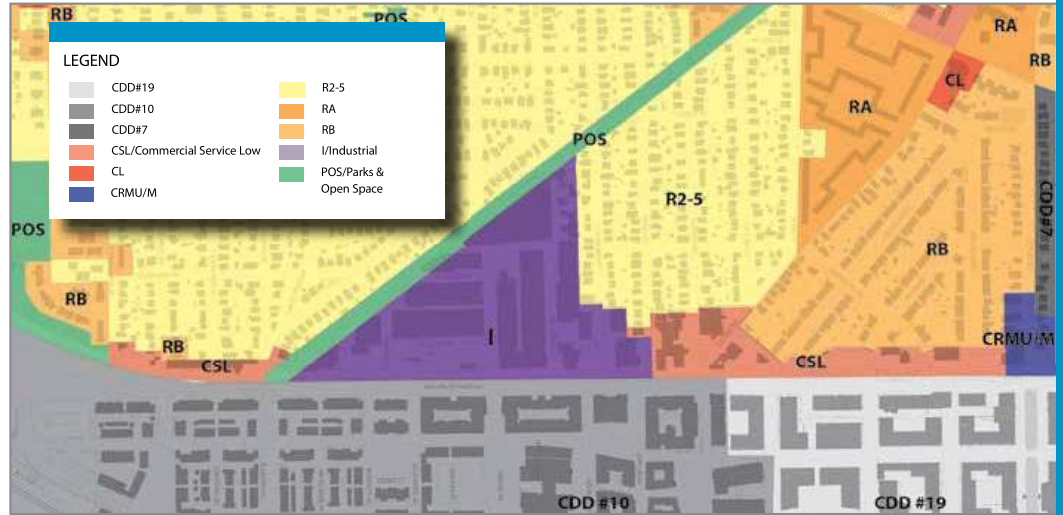
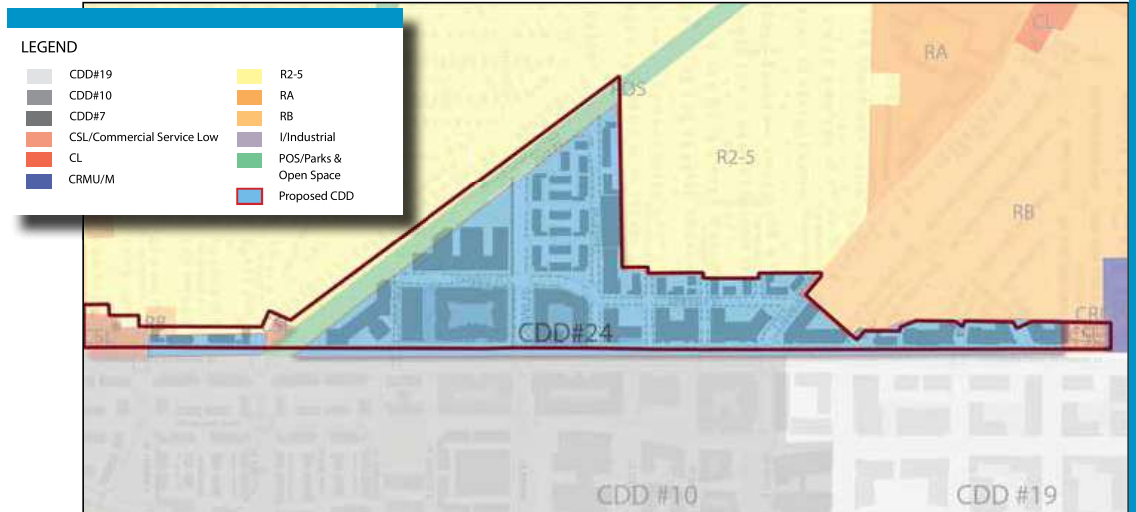


Figure 24: Proposed Zoning and Existing Zoning



E. LAND USE

The land use plan, Figure 25, permits certain uses for each block. Some of the blocks are permitted to be residential or office. In addition, although not required, fueling/charging stations are recommended at two locations within the Plan area. The Plan also permits maker space on the ground floor at the locations depicted in Figure 25 below. The

Plan recommends that some of the sites not be rezoned as depicted below, because of their size, shape, and/or location.

■ ACTIVE USES:

Active uses are specified on the ground level or frontages of many blocks in the Plan area to ensure a vibrant public realm. For purposes of the Design Standards, active uses shall be residential, office, medical care facility, retail, maker, and/or hotel uses.

Figure 25: Land Uses



DISTRICT 1

192

DISTRICT 2

PRIMARY RETAIL:

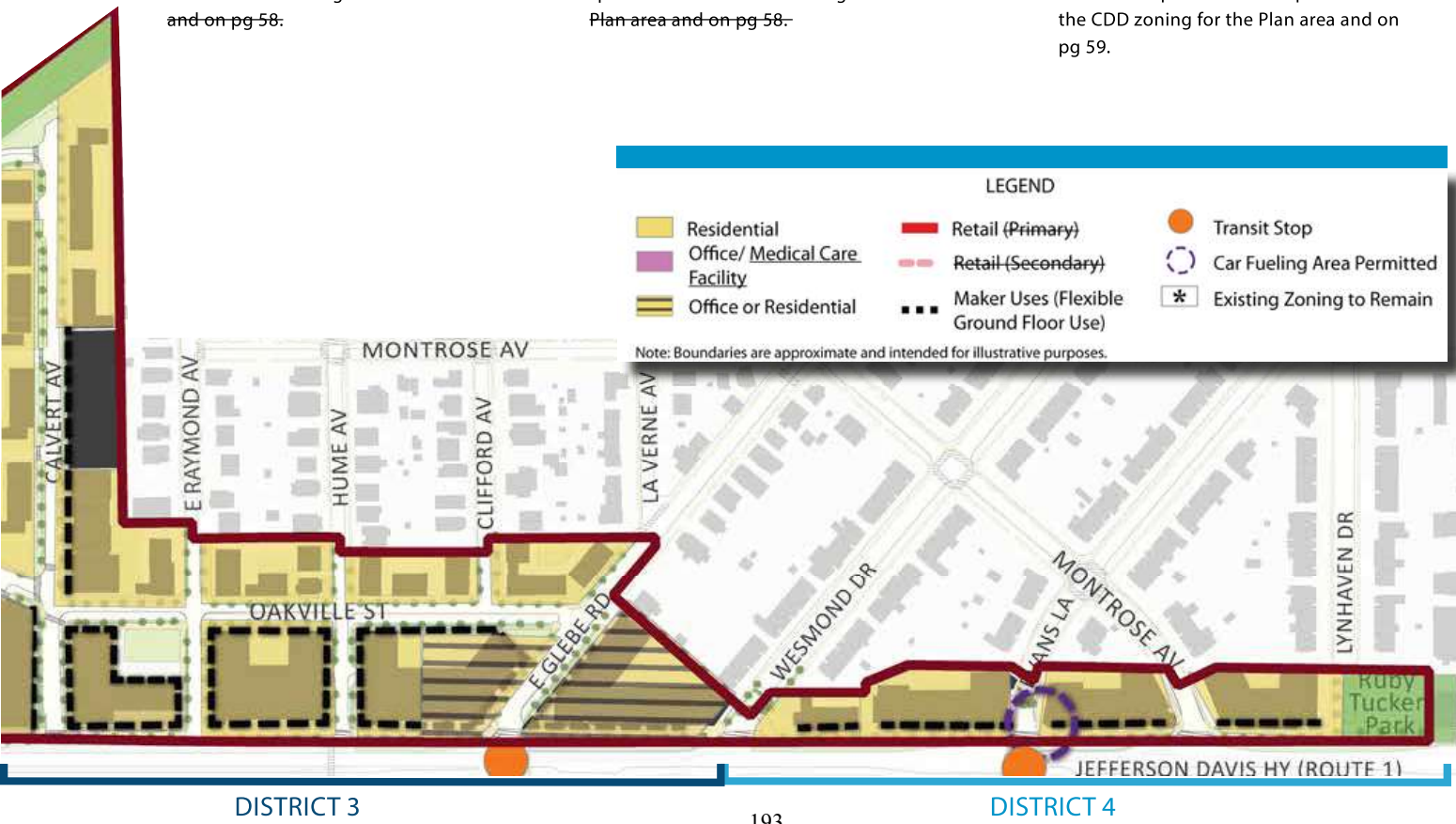
For purposes of the Design Standards, primary retail is intended as retail uses and restaurants *personal service uses*. Final uses and requirements are specified with the CDD zoning for the Plan area and on pg 58.

SECONDARY RETAIL:

For purposes of the Design Standards, secondary retail is a combination of retail, maker uses and personal service uses. Final uses and requirements are specified with the CDD zoning for the Plan area and on pg 58.

MAKER USES:

For purposes of the Design Standards, maker uses are uses typically involved in the production, repair, wholesale distribution, or sharing of resources. Final uses and requirements are specified with the CDD zoning for the Plan area and on pg 59.



PRIMARY-RETAIL

Intent: For the purposes of the Oakville Triangle/Route 1 Corridor Plan Area, the areas designated for “primary retail” are intended to achieve an experiential environment, where shopping, dining, and other establishments engage pedestrians and activate the street front with attractive, unique storefronts. Generally, this includes restaurants, *personal service uses*, and retail *as defined in the CDD*. As shown on the Land Uses Map, the area designated for primary retail is Swann Avenue within Oakville Triangle, with some additional primary retail designated for north and south of Swann on Oakville Street and Route 1. Examples of Primary Retail include restaurants, coffee shops, bakeries, gourmet food, home and fashion retail, and interactive establishments such as cooking or other experiential schools.

Allowed Uses: For the purposes of the Plan area, Primary Retail is defined to include:

- Restaurants
- *Personal service uses*
- Retail shopping establishments (excluding uses detailed below)

Additional Uses: Other uses consistent with the active pedestrian-oriented retail intent of the Plan, may be allowed at the discretion of the Director of Planning and Zoning. Private academic or commercial schools that focus on art, technical, and/

or trade instruction, experiential in nature, may be allowed at the discretion of the Director of Planning and Zoning.

Excluded Uses: For the purposes of the Plan, the following uses are considered incompatible with the intent of Primary Retail:

- Excluded retail shopping establishments: Appliances, Auto parts without service or installation on premises, Drug stores, lawn and garden supplies, Variety Goods
- Personal service establishments

SECONDARY-RETAIL

Intent: For the purposes of the Oakville Triangle/Route 1 Corridor Plan Area, the areas designated for “secondary retail” are intended to achieve a neighborhood-serving and personal services retail environment, where frequent shopping, repair, or service needs can be met. The main area designated for secondary retail is on Calvert Avenue, where there is also a significant degree of overlap with Maker Space Use (Figure 7).

Allowed Uses: For the purposes of the Plan area, Secondary Retail is defined to include:

- Retail
- *Personal service uses* (excluding uses detailed below)

- Amusement enterprises
- Restaurants

Additional Uses: Other uses consistent with the neighborhood-serving intent of the Plan herein, may be allowed at the discretion of the Director of Planning and Zoning.

Excluded Uses: For the purposes of the Plan area, Secondary Retail may not include the following:

- Retail shopping establishments shall not include appliance stores and auto parts stores
- Personal service uses shall not include appliance repair and rental, contractors’ offices, laundromats, and pawnshops

MAKER SPACE/ PWR USES (FLEXIBLE GROUND FLOOR USE)

Intent: Maker Space and Production, Wholesale Distribution, and Repair (PWR) Uses, as defined in the Plan, encompass a wide range of light industrial and/or personal service/neighborhood serving uses including but not limited to manufacturing (craft and light), small food production and distribution (including breweries, distilleries, and other beverage production), technology, media and communications, arts and creative uses, shared office spaces, and green industries.

Maker spaces and PWR uses establish areas

that maintain and promote mixed-use developments that integrate neighborhood-serving light industrial, commercial uses, and residential uses either within a single structure or multiple but integrated structures or within a neighborhood. These shall be located on the ground-floor facing a sidewalk and provide an eclectic mix of uses that serve residents and businesses. Maker space and PWR uses should be developed and integrated, whether through site design and/or building construction, so as to be in character with such developments, and to the effect to not be detrimental or a nuisance to nearby residential properties.

Allowed Uses: For the purposes of the Plan area, "Maker" space uses are defined to include:

- Light assembly, service and crafts
- Manufacturing (to include but not limited to), leather, software, green technology, beverage production, and miscellaneous manufacturing at the discretion of the Director of Planning and Zoning
- Automobile repair, light
- Automobile service station
- Catering operations
- Bakeries under 3,500 sf
- Wholesale business
- Medical laboratory
- Personal Service Uses, including: Art studios, and crafts studios or stores,

Barbershop, beauty salon, private commercial schools (fitness related only), musical instrument repair, Professional photographer studio, Dressmakers and tailors, Shoe repair, Furniture upholstery shops, Bicycle repair, Watch/jewelry repair, Printing and photography services

- Neighborhood-Serving Uses, including: Pet supply, grooming, and training with no overnight accommodation

Additional Uses: Other uses consistent with the maker use intent herein, may be allowed at the discretion of the Director of Planning and Zoning. For the purposes of the Plan area, Maker Use may include the following subject to future review, and also subject to a maximum of 20% for each building where maker uses are permitted. The 20% may be waived or modified by City Council as part of a Development Special Use Permit and/or SUP if the Council determines the use(s) are not feasible and the proposed ground floor use is consistent with the intent of the Plan:

- Retail shopping establishment (excluding the following: auto parts without service and installation, department stores, drug stores,

COMMUNITY FACILITIES

Community facilities owned and/or operated by the City, and accessory uses, including but not limited to uses such as a library, cultural center, recreation center, meeting spaces, or similar uses consistent with the intent of the Plan to create an urban pedestrian-oriented mixed-use community. Each use and building will need to comply with the applicable provisions of the Design Guidelines. All community facilities and/or uses will require approval of a development special use permit and all other applicable approvals for each building and/or uses. The floor area of each building(s) and/or use will not be deducted from the approved square footage within the approved CDD Concept Plan.

F. FUELING/CHARGING STATIONS

There are two existing gas stations in the Plan area. The land use plan (Figure 25) permits fueling/charging as part of the redevelopment at the two locations. If these facilities are provided, they will be subject to the following standards.



FUELING/CHARGING STATION GENERAL STANDARDS

- 4.12 Fueling/charging stations, if provided, shall be part of the redevelopment that implements the requirements of the Vision Plan and Design Standards and Guidelines.
- 4.13 Fueling/charging stations shall be integrated into the design of the building and not a secondary element.
- 4.14 If a canopy is provided, the design of the roof shall be considered from the adjoining building(s) and uses and shall include elements such as a green roof or comparable treatment. Canopy lighting shall be designed to minimize impact/visibility from adjoining neighborhoods.
- 4.15 Service components, such as payment, shall be integrated into the building to the maximum extent feasible.
- 4.16 The fueling/charging station will be subject to all applicable building, environmental, and zoning requirements and the CDD zoning(s) for the Plan area.



G. BUILDING HEIGHTS-TRANSITION ZONES

To ensure compatibility with adjoining neighborhoods, the Plan requires appropriate building height transitions where buildings either step down in height and/or provide setbacks and landscaping to buffer new development and adjoining properties. See Figures 37 and 38 standards for each building type.

Figure 26: Building Heights - Transition Zones



G. BUILDING HEIGHTS-MAXIMUM

Maximum building heights are intended to ensure buildings of compatible size and massing given the context of each block. Figure 27 depicts the maximum heights. Figure 28, establishes minimum heights for the blocks to achieve the intent of the Plan.

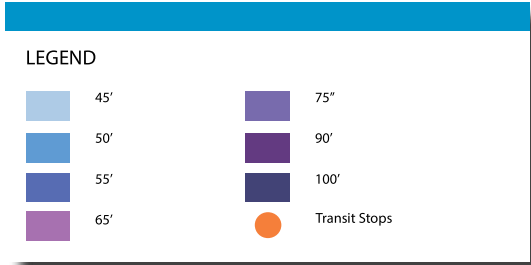
Additional height may be considered for the property at the corner of Fannon and Route 1 subject to the following

- Require a more appropriate setback (larger than 12 feet proposed) between the proposed building and the future lower trail in Mount Jefferson Park. Suggestion is to maintain the setback that is established in the northern portion of Oakville Triangle;
- Maintain a 45' height limit adjacent to Mount Jefferson Park consistent with the 45' height established in the northern portion of Oakville Triangle; and
- Allow for some flexibility of building height on the Route 1 frontage above the currently proposed 55 feet if 45' building height is maintained along Mount Jefferson Park.

A request to increase the allowed height on the parcel (above 55 feet) in the future will be evaluated as part of a DSUP application and based on the criteria established by the Advisory Group.

Figure 27: Maximum Building Heights





Note: The heights depicted are maximum heights. The heights shall be subject to all applicable standards herein, such as height transitions, setbacks, and variety in height and applicable building requirements.





H. OPEN SPACE

Open spaces are the living rooms of the City, the places where people come together to enjoy the City and each other. These spaces enhance quality of life, and form the stage and backdrop to the drama of urban life. Open spaces within the Plan area should be designed to accommodate a variety of users of all ages, to be determined through the development review process. Open spaces and improvements within the Plan area will include:

- Renovation of the 4.7 acre Mount Jefferson Park.
- Five additional open spaces adjacent to Mount Jefferson Park, ranging in size from ±4,400 sq. ft. to ±21,000 sq. ft.
- A ±21,000 sq. ft. central urban square within the Oakville Triangle.
- A central .7 acre public park in Oakville Triangle.
- An expansion of the existing Ruby Tucker Park for a total of ±14,500 sq. ft.
- Townhouse developments will include a minimum .15% ground level open

space as part of redevelopment. Roof-top amenity space is strongly encouraged.

- New multifamily development will include 25% ground level and 15% roof-top amenity space as part of the redevelopment.
- Mixed-use projects (with ground floor retail - commercial) will include a minimum of 15% ground level open space and 25% roof-top amenity space as part of redevelopment with a total of 40% on-site open space to include rooftop-amenity space as part of redevelopment.

Figure 29: Open Spaces (Public, Ground level and Roof-top)



PLAN FRAMEWORK

Note: For roof-top amenity space, the percentage shall be a percentage of the building.

OAKVILLE URBAN-PLAZA CENTRAL OPEN SPACE

The central urban plaza will be hardscape with appropriate plantings, shade options, and lighting. High-quality materials and finishes, as well as the inclusion of public art or other focal features, ensure this space will be a great amenity. The plaza will be a shared space for pedestrians and bikes.

The central .7 acre open space is intended to

be a combination of multiple outdoor "Rooms" for multigenerational programing, including but not limited to an outdoor meeting space, lawn area, children's play area and sustainability features and public art. The materiality of the park will also incorporate the industrial heritage of the site through materials such as wood, stone, weathered metal, etc.

RUBY TUCKER PARK EXPANSION

Ruby Tucker Park is proposed to be expanded from ±10,450 sq. ft. to ±14,500 sq. ft.

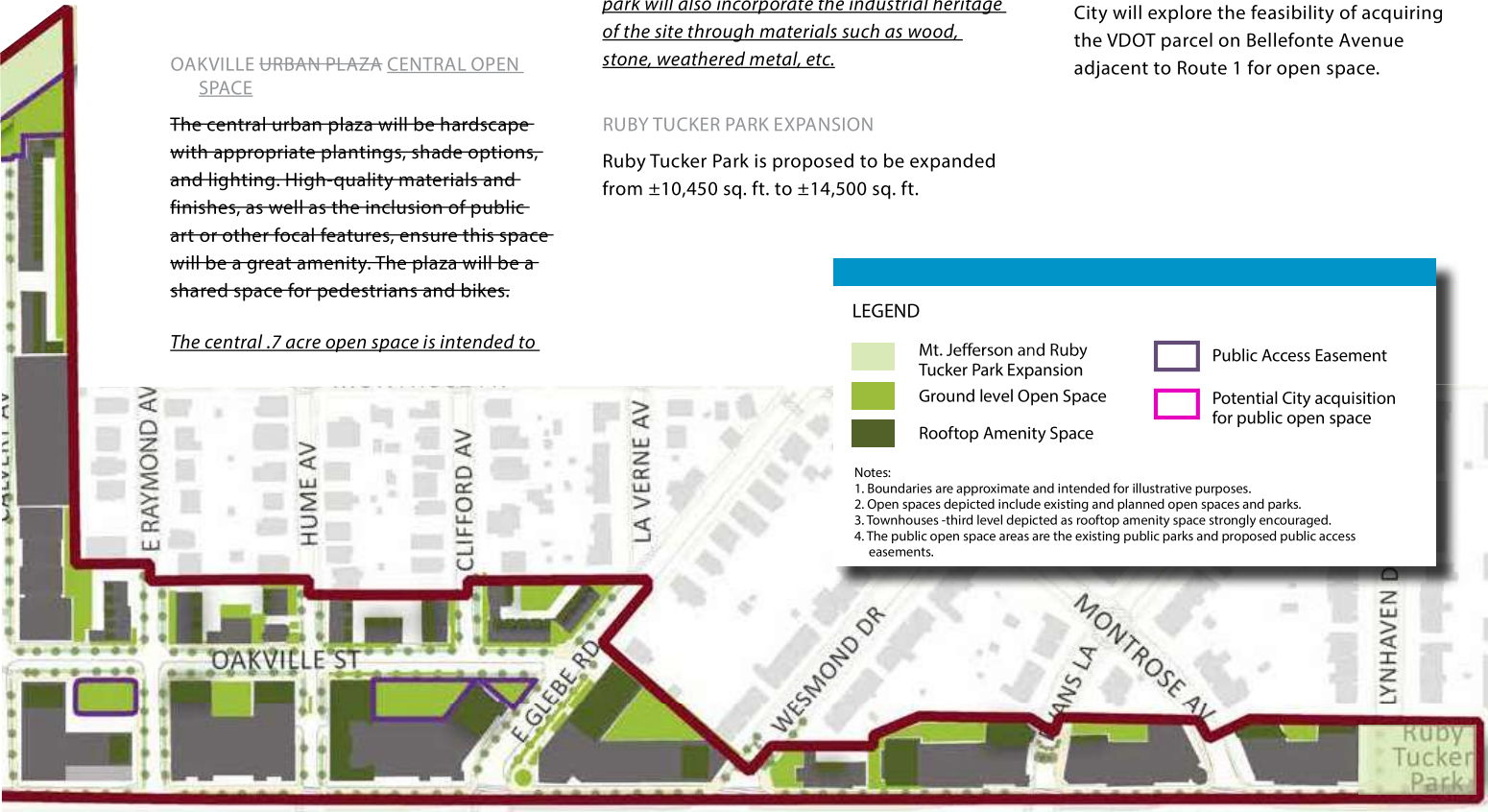
BELLEFONTE AVENUE OPEN SPACE

The existing right-of-way located on Bellefonte Avenue and Route 1 will remain as right-of-way but will be used as open space and streetscape improvements. The City will explore the feasibility of acquiring the VDOT parcel on Bellefonte Avenue adjacent to Route 1 for open space.

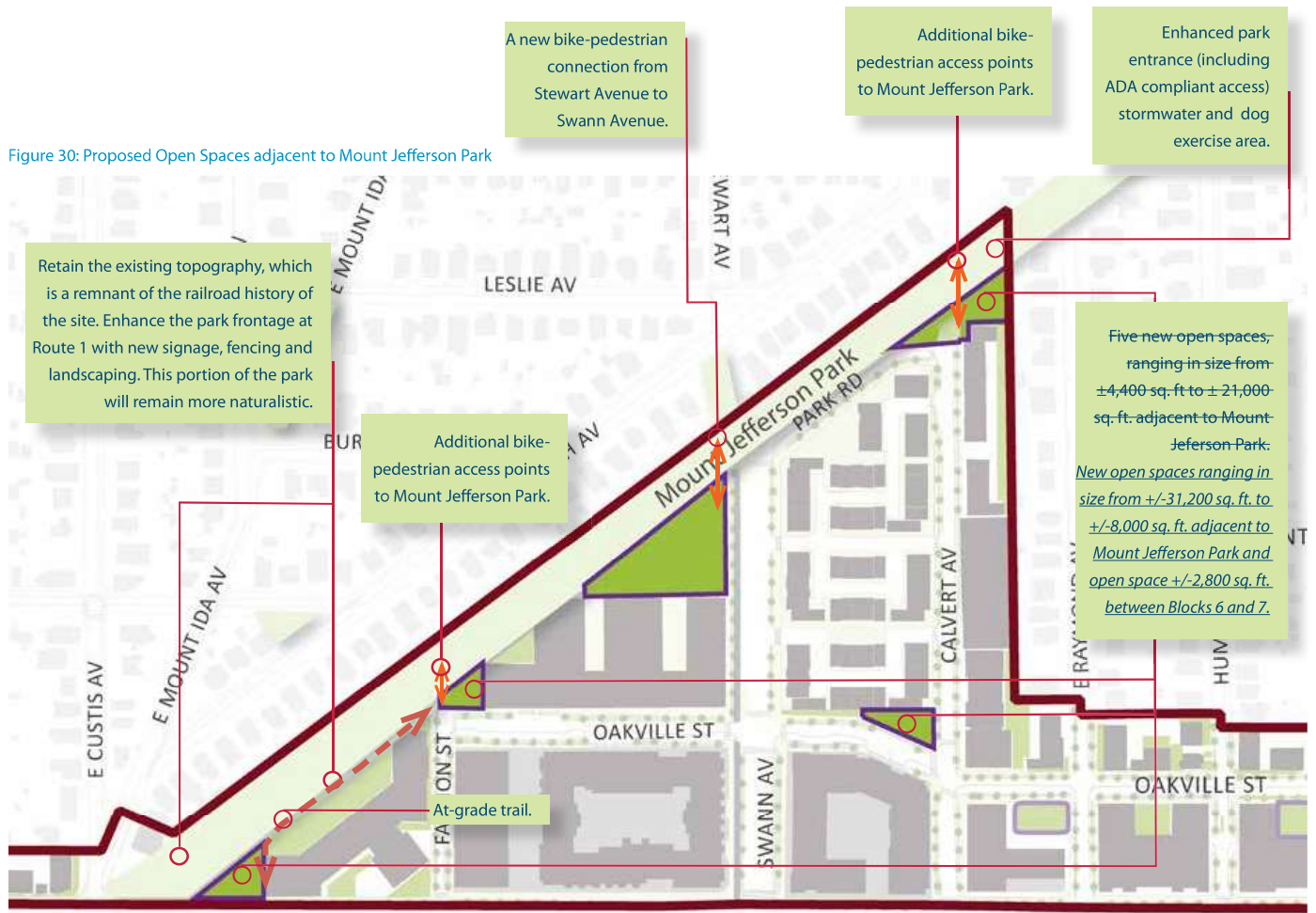
LEGEND

- Mt. Jefferson and Ruby Tucker Park Expansion
- Ground level Open Space
- Rooftop Amenity Space
- Public Access Easement
- Potential City acquisition for public open space

- Notes:
1. Boundaries are approximate and intended for illustrative purposes.
 2. Open spaces depicted include existing and planned open spaces and parks.
 3. Townhouses -third level depicted as rooftop amenity space strongly encouraged.
 4. The public open space areas are the existing public parks and proposed public access easements.



MOUNT JEFFERSON PARK IMPROVEMENTS



PLAN FRAMEWORK

■ GROUND-LEVEL OPEN SPACE:

These open spaces will range from front yards and courtyards to a new public open spaces and an urban plaza on Swann Avenue.

■ ROOFTOP AMENITY SPACE:

Rooftop amenity spaces are required for all new townhouses and multifamily buildings. These rooftop gardens and recreational amenities will provide residents and building tenants with high quality outdoor open spaces. The use of sustainable materials and concepts should be integral to the design of the open space. The rooftop amenity space should integrate with the architecture and serve as an extension of each building's common areas.

The rooftop amenity areas in close proximity to the adjoining neighborhoods will need to be designed in a compatible manner to prevent adverse affects of noise and light.



OPEN SPACE STANDARDS

- 4.17 Mount Jefferson Park shall be completed as part of the redevelopment of the Oakville Triangle site and shall be consistent with the park improvements approved by the Parks and Recreation and Planning Commissions.
- 4.18 The ~~five~~ new open spaces adjacent to Mount Jefferson Park shall be constructed as generally depicted in Figure 30, as part of the redevelopment adjacent to the Mount Jefferson Park. The at-grade trail (lower trail) extension on the northern portion shall be constructed by the adjoining property owner.
- 4.19 The central open space within Oakville Triangle shall ~~be hardscape~~ with have appropriate plantings, shade options, and lighting and shall contain high-quality materials and finishes, as well as the inclusion of public art or other focal features. The open space/~~plaza~~ shall be accessible to the public through the provision of a public access easement.
- 4.20 The Ruby Tucker Park shall be expanded by \pm 4,000 sq. ft by utilizing the existing Lynhaven Drive right-of-way.
- 4.21 New townhouse developments shall provide a minimum of 15% ground level open space. Roof-top amenity space is strongly encouraged.
- 4.22 New multifamily developments shall provide a minimum of 25% ground level open space and 15% roof top amenity space.
- 4.23 New mixed-use projects (with ground floor retail-commercial) shall provide a minimum of ~~15%~~ 20% ground level open space ~~and 25% roof-top amenity space~~ with a total of 40% on-site open space to include rooftop-amenity space as part of redevelopment.
- 4.24 For office and hotel uses, roof top open spaces or courtyards are encouraged.
- 4.25 Spaces shall be designed for their intended function; for example, plazas should be designed with adequate amounts of hardscape, electrical and water connections to accommodate public gatherings; large greens or parks should minimize hardscape areas that will detract from their intended appearance as a green oasis dominated by native vegetation, some lawn areas, and trees. Pedestrians-only and shared pedestrian/vehicular areas shall be designed to withstand the intended loading on paved or green surfaces.
- 4.26 The Plan's open space shall incorporate significant green and pervious elements, offer shade relief and contribute to the City's tree canopy goals where possible.
- 4.27 Spaces should be designed with consideration for climate and sun exposure during different seasons of the year. Opportunities for shade or sun, with water elements to offer a sensory change and softening of urban noise and wind protection, should be considered where appropriate.
- 4.28 Materials shall be selected that are durable and appropriate for the scale and context of the Plan area. Materials should be typical of the types used in the construction of urban spaces. Although materials must be suitable for significant pedestrian use, their quality and appearance shall reflect their importance as open space within the public realm.
- 4.29 Garden screen walls and/or retaining walls should be constructed of brick, stone, architectural precast or other highly finished appropriate material.
- 4.30 Pavement in open space shall be brick, stone, concrete pavers, or concrete. Large expanses of concrete without details, scoring patterns, or brick/stone banding are prohibited.
- 4.31 Children of all ages should have easy access to appropriately located, designed, and landscaped outdoor play areas suited to their development and play needs.
- 4.32 Landscapes shall be designed with sustainable plant selections that are horticulturally acclimatized to the Mid-Atlantic and DC National Capital Region, that require minimal maintenance and non-organic treatment, that utilize manipulation of rainwater for natural irrigation, and that provide natural pest control.

I. GATEWAY ELEMENTS - SIGNATURE FACADES

Gateway elements and signature facades are distinctive architectural elements and/or special building forms used to draw attention or reinforce points of interest that mark the location of “entries” and “places” within the Plan area. Gateway elements and signature facades shall be provided at visually prominent locations within the Plan area as shown in Figure 31. These locations shall be of the highest level of design excellence incorporating special building forms and/or the innovative use of materials.



Figure 31: Gateway Elements & Signature Facades



GATEWAY ELEMENTS/SIGNATURE FACADES STANDARDS

- 4.35 Gateway elements and signature facades shall be provided at locations as depicted in Figure 31.
- 4.36 Gateway elements and signature facades shall be proportional to the size and scale of the building.
- 4.37 Gateway element(s) shall provide distinctive three-dimensional forms, and unique shapes and materials to reinforce the significance of each location. Architectural features, such as towers, cupolas and lanterns should be used to address highly visible corners or terminated vistas. Gateway elements should provide special elements at street terminations to frame views.
- 4.38 Signature façades shall be designed to reflect their visually prominent location.
- 4.39 The gateway element will vary in height from the primary height of the building by being lower or slightly taller as permitted herein, or through the architectural treatment of the gateway element.

5

URBAN DESIGN CHARACTER



The character of the Plan area will be shaped by the quality of the buildings, public spaces and streetscapes. The standards require that the new buildings provide high quality architectural design and materials. The standards also require height and scale transitions adjacent to the adjoining neighborhoods through the provision of elements such as building shoulders, setbacks, height transitions and landscape buffers.

The standards do not mandate particular architectural style, but rather high quality materials and design. The intent of the standards is to achieve a variety of styles and forms to appear as a collection of buildings that have been constructed over time.

A. BUILDING STREETWALL

The proposed buildings are meant to create a well-defined edge, also known as the building streetwall, that frame the public streets and open spaces. The streetwall provides a sense of spatial definition to enable the street to function as an outdoor room and reinforce pedestrian activity on the sidewalk. The design, location and quality of the building adjacent to the street – the streetwall – is the portion which is

experienced the most by pedestrians and should be the area of the building façade which is given the most attention and the highest quality design and materials. While maintaining the streetwall is important, it is also important that some of the buildings have building breaks, front yards, recesses, and courtyards to reinforce the character of each district and to provide a variety of landscaping and building forms.

BUILDING STREETWALL STANDARDS

- 5.1 The streetwall height for each street frontage shall be a minimum of 35 feet on Route 1 and 25 feet for other street frontages, which shall be located parallel to the adjoining street or open space. In addition to the streetwall requirements, buildings are also subject to the height requirements herein. Where the building streetwall is taller than the minimum, pedestrian elements such as cornice and color shall be provided to ensure pedestrian scale buildings.
- 5.2 The streetwall shall generally be parallel to the street and at a minimum height as required herein. The streetwall for each block frontage shall be a minimum of 75% for residential, *medical care facility, and* office and hotel. For residential buildings where courtyards are provided, the streetwall shall be a minimum of 50%. A courtyard shall be limited to a maximum of two block frontage. Ground floor retail and maker uses shall provide a minimum of 85% continual streetwall.
- 5.3 The streetwall shall be setback a maximum of 30 feet from the property line. This shall be in conjunction with the massing requirements herein.
- 5.4 While a generally continual streetwall is required for each building, the streetwall shall incorporate articulation to ensure variety of the building as required herein.



B. BUILDING HEIGHTS-VARIETY

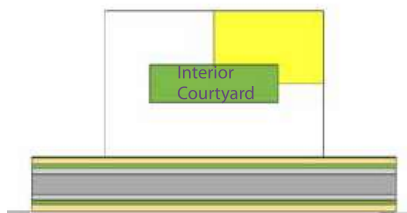
Each townhouse, multi-family, *and* office and hotel use shall provide variety in the building height of each building.

The intent of this provision is to ensure a significant variety of height for each new building.

The Plan also allows a modest (up to 15 feet) increase in height at the locations depicted in Figure 31. This is to enable more dynamic urban and architectural forms at visually important locations.

Figure 32: Percent Variation in Building Height

- 75% of building footprint to maximum building height provided
- 25% at least 1 story lower than maximum height provided

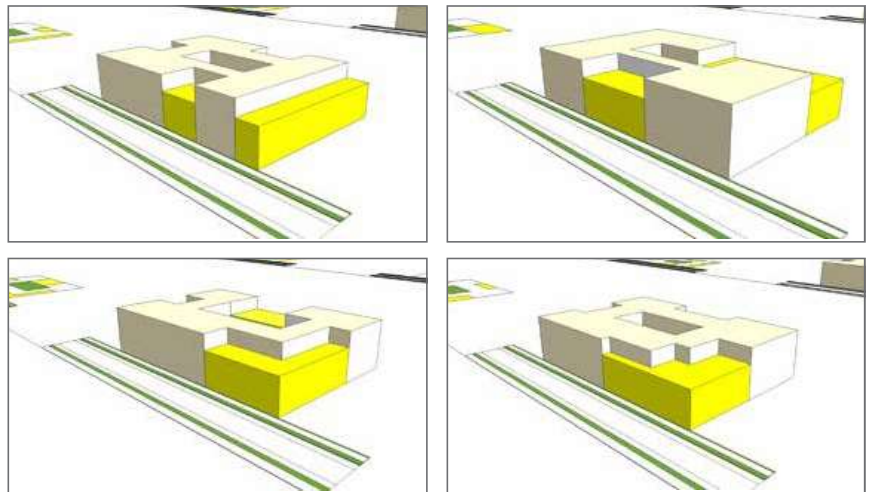


Note: Figures 32 and 33 are intended for illustrative purposes only. The allocation and distribution of the 25% will be determined as part of the development review process.

BUILDING HEIGHTS - VARIETY STANDARDS

- 5.5 Each multi-family building (excluding 2/2 stacked townhouses) shall provide a minimum of 25% of the building footprint below the maximum provided height (Figure 33). The specific allocation of the 25% shall be determined as part of the development review process. Building Breaks provided shall count toward the minimum requirement of this section. Office and hotel building shall provide a variety of height which shall be determined through the development review process.
- 5.6 For townhouses and 2/2 stacked townhouses a variety of heights shall be provided within each row of townhouses. The difference in height shall be a minimum of one level (story) variation between the townhouses. This can be achieved through variation in roof form, setbacks and height and the amount of variation shall be determined as part of the development review process.
- 5.7 Some buildings, at locations as depicted in Figure 31, where gateway elements are required may be permitted to exceed the maximum height by one full level (story), in an amount not to exceed 3,000 sq. ft. The locations shall be limited to locations depicted in Figure 31 - Gateway Elements.

Figure 33: Illustrative Example of Variety of Height



C. BUILDING MASSING

VARIATION IN BUILDING FOOTPRINT

The intent of this provision is to ensure variety in the building massing for multi-family, office, and hotel uses and to provide variation in the building footprint to create a more urban, pedestrian-scaled building.

BUILDING MASSING STANDARDS

- 5.8a At least 25% of the total multi-family building perimeter must be setback between 8-10 feet at the building face on at least 2 facades. Stepbacks may occur at the ground floor or above the streetwall where retail and/or maker space is provided on the ground floor.
- b For office and hotels, at least 15% of the total building perimeter must be setback between 2-8 feet at the building face on at least 2 facades. Stepbacks may occur at the ground floor above the streetwall where retail and/or maker space is provided on the ground floor.

Note: Figure 34A & 34B are intended for illustrative purposes only as different ways to apply the 30% requirement. The allocation and distribution of the 30% will be determined as part of the development review process.

Figure 34A: Variety of Building Footprints Axon Views

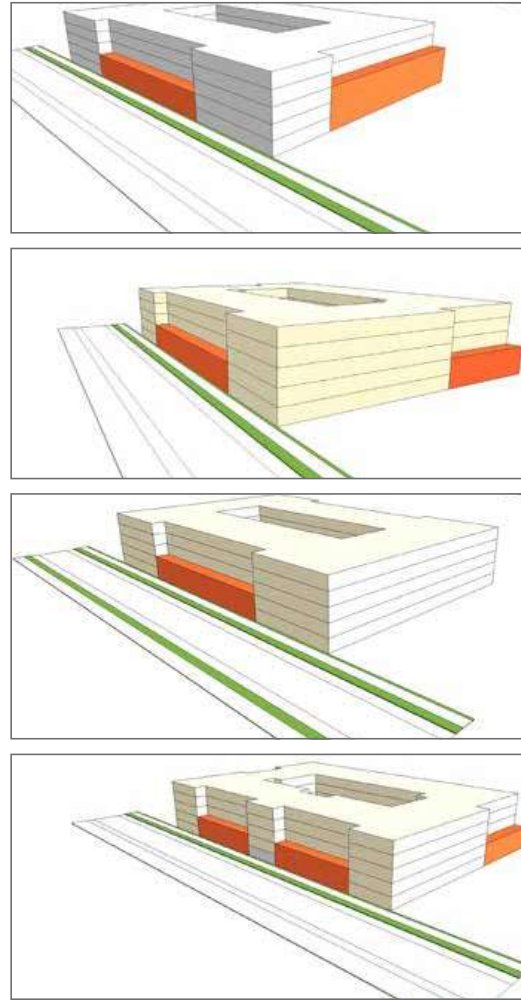
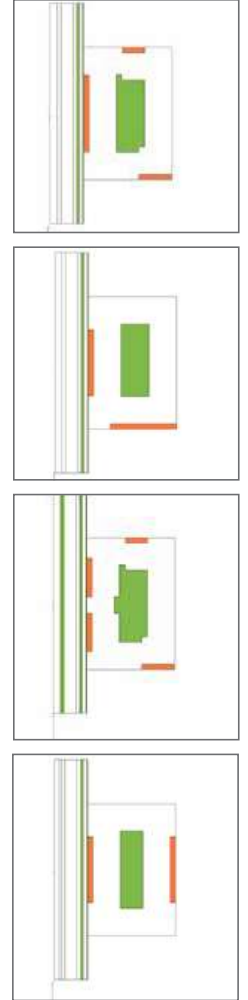


Figure 34B: Plan Views



URBAN DESIGN CHARACTER

OTHER EXPRESSIVE PLAN CHANGES

Where changes in wall planes and architectural elements are provided or required, they shall comply with Figure 35. This is intended to ensure that where elements such as projections or recesses are provided or required they are an appropriate depth to avoid the appearance of flat buildings and facades.

BUILDING MASSING STANDARDS

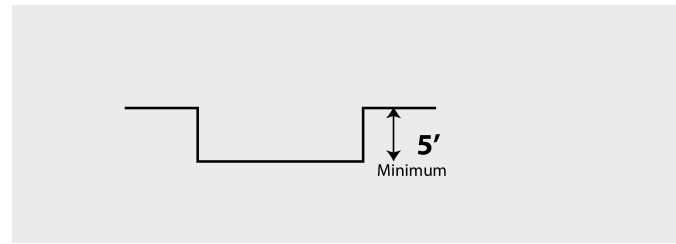
5.9 Where bays, pavilions, recesses, hyphens and screens or compatible elements are provided they shall comply with Figure 35.

Figure 35: Massing Requirements

Belle Pre



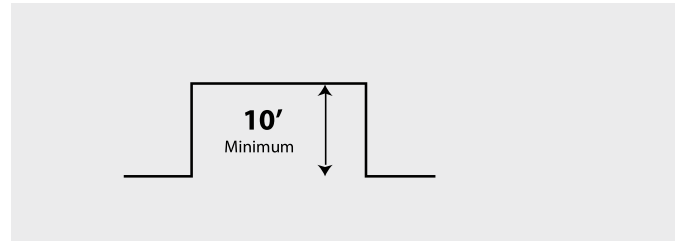
Bays - Pavilions



The Monarch



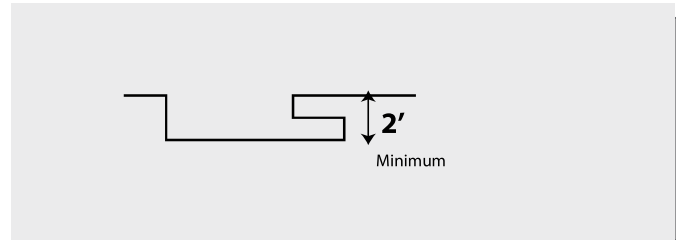
Building Recesses and Hyphens



Edmonson Plaza



Screens



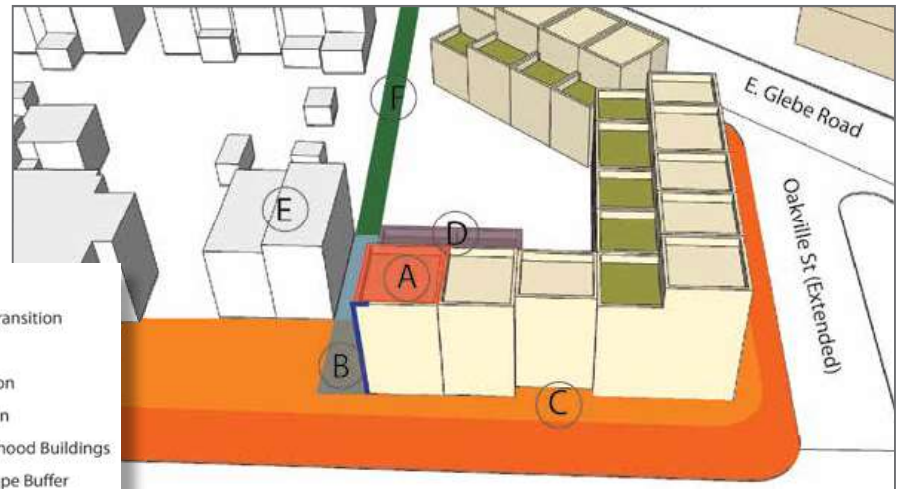
D. BUILDING TRANSITIONS

TOWNHOUSES

The Plan Area is adjacent to single-family homes and townhouses within Del Ray and Lynhaven. The Plan generally provides townhouses to transition to the existing homes. The Standards also require height transitions, setbacks and landscape-buffers to provide appropriate transitions as depicted in Figure 36.



Figure 36: Proposed Townhouses transitioning to existing homes



Note: Figure 36 is intended for illustrative purposes only.

BUILDING TRANSITION STANDARDS

I. TOWNHOUSES

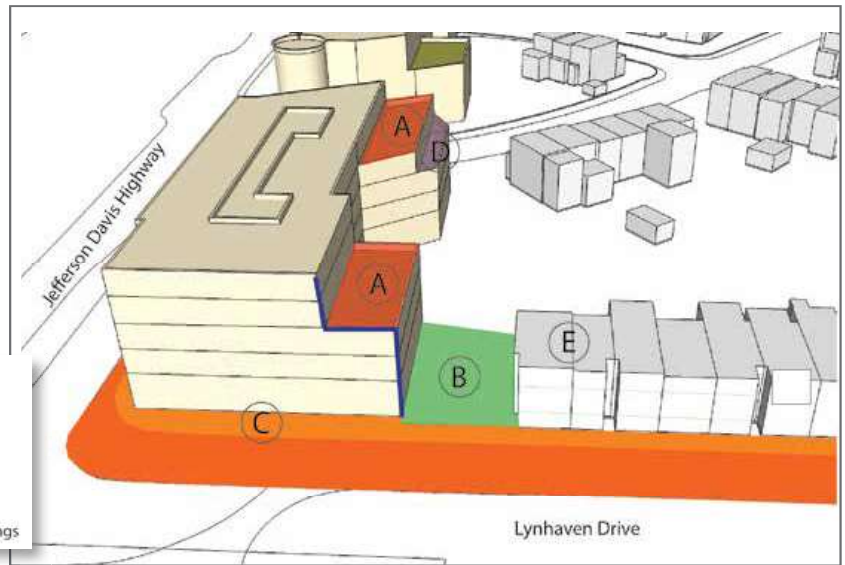
- 5.10 **A** - Where townhouses in the Plan area are adjacent to existing homes, the new townhouse shall be a maximum of one additional level or no more than 14 feet taller than the existing homes whichever is less. More than one unit of the townhouses may be required to comply with this requirement if deemed necessary as part of the development review process.
- 5.11 **B** - Townhouses shall provide a side yard setback of a minimum of 15 feet or a 1:3 floor to height ratio from the property line, whichever is greater, adjacent to the existing single-family homes or townhouses or buildings adjoining the Plan area. This area shall be landscaped.
- 5.12 **C** - Where new townhouses are constructed on new streets, they shall generally be oriented to face the existing streets, where feasible. Front yard setbacks for new townhouses on existing streets shall be compatible with the existing neighborhood. For new streets, the setback will be consistent with the cross-sections (Chapter 8) or a minimum of 4 ft, whichever is greater.
- 5.13 **D** - For four level townhouses adjacent to existing homes, the fourth level shall be setback a minimum of 15 ft. The third level shall generally be oriented to the existing homes.
- 5.14 **F** - In the rear, a setback-landscaped buffer of a minimum 8 feet shall be provided adjacent to the single-family homes and townhouses. Where feasible a larger rear setback shall be provided. A fence or wall may be required within the landscape buffer or setback as part of the development review process.

Note: The letters of the standard reference the requirements in Figure 36.

MULTI-FAMILY BUILDINGS

In some portions of the Plan Area, multi-family buildings are adjacent to existing single-family homes and/or townhouses. To ensure that the multi-family buildings provide an appropriate transition to the existing homes, the Standards require building shoulders (height transitions), appropriate setbacks and landscape buffers as depicted in Figure 37. For these portions of the Plan area that also permit office uses, the transition requirements also apply.

- A Building Shoulder
- Required Height Transition
- B Landscape Buffer/Setback
- C Front Setback
- D Building Stepdown
- E Existing Neighborhood Buildings



Note: Figure 37 is intended for illustrative purposes only.

II. MULTI-FAMILY (BUILDING TRANSITIONS)

Intent: Where new multi-family buildings (or office buildings) adjoin existing properties as depicted in Figure 37, the following transition setback, buffer standards and requirements shall apply.

5.15 A - Each multi-family building or office building shall provide a building shoulder as generally depicted in Figure 37. The shoulder shall provide a minimum setback of 15 ft. The building shoulder shall be no taller than 15 feet taller than the existing buildings-homes or a maximum of 45 feet whichever is less. The length, width, and depth of the building shoulder shall be determined as part of the development review process.

5.16 B - In addition to the building shoulder required herein, a landscaped setback a minimum 15 ft. shall be provided. Where an additional setback is feasible it shall be provided, as determined through the development review process. A fence or wall may be required within the landscape buffer or setback as part of the development review process.

5.17 To the extent feasible, the setback of the multi-family buildings (excluding Route 1) on existing streets shall be compatible with the existing neighborhood.

E. CHARACTER

The character of the Plan Area will be shaped in large part by the quality of the buildings, streetscapes, and public spaces. The standards require high quality materials and design. The design of each building is required to provide variety to appear as a collection of buildings

constructed over time. The consistent streetscape throughout the Plan area will visually pull together the buildings. Buildings, streetscapes, and open spaces are encouraged to incorporate the industrial heritage of the Plan Area.

BUILDINGS



STREETSCAPE



F. BUILDING TYPES

I. TOWNHOUSES

The townhouse building type is a small- to medium-sized attached structure that typically consists of 3–8 townhouse units placed side-by-side. This type is proposed in locations where it is necessary to establish appropriate transitions to the existing single-family house and townhouse neighborhoods adjacent to the Plan area. This building type is also located on the Route 1 frontages that do not have sufficient depth for multi-family buildings. The Plan requires that this building type have garage access from a rear alley.



BUILDING TYPE - TOWNHOUSE STANDARDS

1. Parking

5.18 The parking for each townhouse shall be provided from a rear alley. Front loaded townhouses are prohibited. Detached parking garages are encouraged.

5.19 Permanent surface parking lots are prohibited.

2. Streetscape Level – First Level

5.20 The first level shall be designed with the highest quality material and detailing.

5.21 Each unit shall be subject to the residential uses at grade requirements as required herein.

5.22 Building designs shall incorporate modulation and articulation such as massing reveals, changes of textures, materials, and/or colors, or shifts of the façade plane in order to create a pedestrian scaled façade. Shifts in the footprints shall be a minimum of 2-5 ft. to provide variety in the façade plane.

5.23 Each ground floor residential unit must have an individual entry door directly from the adjoining street. Mews units and configuration is prohibited. Where stoops are required or provided, they shall be oriented parallel to the adjoining sidewalk.

5.24 All units must include a minimum depth of 25 feet of occupied habitable space for each level unless less depth is required for variation in the streetwall.

5.25 A minimum of 50% of each group of townhouses for each project shall provide a front porch. The porches shall comply with the applicable requirements herein. *Final location of porches will be determined during the DSUP process.*

5.26 Townhouses shall provide a 2 to 10 ft. setback from the required sidewalk to provide space for individual front yards, plantings, porches, stoops and similar elements.

5.27 For each grouping of liner townhouses exceeding 120 feet in length, a setback of a minimum of 8 feet shall be provided or a building break, between the adjacent units. The final width shall be determined as part of the development review process.

5.28 For freestanding townhouses, a building break shall be provided to ensure that groupings of townhouses do not exceed 120 feet in length.



3. Building Character and Materials

- 5.29 Upper floor exterior terraces or balconies are permitted at the rear facade of the building. These may also be permitted on the front facade of a building at the discretion of the Director of Planning and Zoning as part of the Development Review Process.
- 5.30 Units shall be architecturally differentiated through the use of color and materials within each block. This is not intended to require variety for each unit, but rather within each group of townhouses.
- 5.31 All building materials shall be used to express their specific purpose and express the tectonic nature of the materials; for example, heavier materials support lighter materials.
- 5.32 Building materials for each façade shall consist of the following:
- Brick, glass, stone, wood, precast and/or metal
 - Stone, cast stone, metal or similar durable materials for trim
 - Fiber cement siding and panels (or comparable) may be provided at limited locations.
 - Prohibited materials include synthetic stucco, regular ground or split face CMU, and any masonry units with an expressed size of 8" x 16" and vinyl siding and any material not outlined above.
- 5.33 Sides and rears of buildings visible from a street or park shall use the same architectural treatment as the primary façade.
- 5.34 Blank façades shall be prohibited for street or park frontages.

4. Building Massing

- 5.35 Each townhouse shall comply with the maximum height (Figure 27), minimum height (Figure 28), and the required height transitions (Figures 26, 36, 37).
- 5.36 To comply with the applicable height requirements, the roof may be a flat or gable roof.



5. Building Fenestration

The size, frequency, and location of windows will be one of the primary visual characteristics of each building. All townhouses are subject to the following:

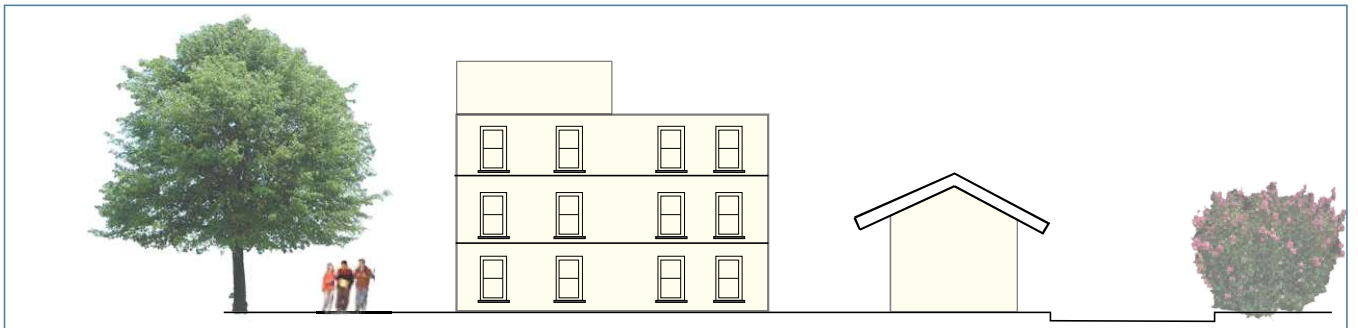
- 5.37 At least 25% of the each facade adjacent to a street or open space shall be devoted to transparent windows and doors to allow maximum visual interaction between sidewalk areas and the unit. The use of dark or mirrored glass is not permitted. A higher percentage is encouraged where feasible
- 5.38 Buildings shall generally provide a vertical fenestration pattern.
- 5.39 Mirrored reflective, frosted reflective or darkly tinted glass is prohibited.



Figures. 38: Townhouse Typologies



Townhouse with Optional Detached Garage



Note: Figure 38 is intended for illustrative purposes only. Drawings shown above are not drawn to scale

URBAN DESIGN CHARACTER

II. MULTI-FAMILY BUILDING

The proposed multi-family buildings will range in height from 55 feet to 75 feet tall. Parking for most of the buildings will be located entirely below grade. Many of the buildings are required to provide a taller first floor (15 ft. to 18 ft.) to enable ground floor retail or maker space.



BUILDING TYPE - MULTI-FAMILY STANDARDS

1. Parking

- 5.41 Parking for each building shall be located entirely below grade or entirely screened with an active use and shall be in accordance with the new residential parking standards. The screening of the parking with active uses shall be provided for each level of the entire perimeter of each street and/or park, courtyard, and/or open space frontage or where visible from an adjoining street or open space.
- 5.42 Permanent surface parking lot(s) are prohibited.

2. Streetscape Level – First Level

- 5.43 Active uses shall be provided for a minimum depth of 25 ft. for each street frontage and all street, open space, courtyard, and park frontages for each level and the entire length of the building frontage excluding necessary curb cuts and loading areas.
- 5.44 Building designs shall incorporate modulation and articulation such as massing reveals, changes of textures, materials, and/or colors, or shifts of the facade plane in order to create a pedestrian scaled façade
- 5.45 Unless required for the function of the building, blank walls in excess of 30 ft. in length or height are prohibited.
- 5.46 Individual and functional entries and “townhouse-scale” elements are required for the multi-family buildings at 20 to 30 ft. intervals, where ground floor commercial, retail uses or maker spaces are not provided where stoops are required or provided, they shall be oriented parallel to the adjoining sidewalk.

3. Building Break

- 5.47 A building break shall be provided for larger multi-family buildings such that the longest expressed element does not exceed 200 feet in length. The building break shall be a minimum of 30 feet in width.
 - a. Where retail-commercial use is provided or required on the ground floor the building break is required above the first floor retail-commercial use.
 - b. There may be a connector between the building break.



URBAN DESIGN CHARACTER

- c. As part of the development review process, a building break may not be required if a level of façade variation is provided comparable to the building break required above. In addition, if a building break is not required, the façade variation shall include variation in color and materials.
- d. In the event that the predominant portion of the building is setback a minimum of 40 feet, a building break is not required. However the expression of a building break is required at a distance not to exceed 200 feet in length, which shall be expressed through a building recess of a minimum depth of 10 feet and through the use of materials and color. The width of the recess shall be determined as part of the development review process.

4. Building Character and Materials

- 5.48 Buildings shall be architecturally differentiated through the use of color and materials within each block.
- 5.49 All building materials shall be used to express their specific purpose and express the tectonic nature of the materials; for example, heavier materials support lighter materials.
- 5.50 Building materials for each façade shall consist of the following:
 - Brick, glass, stone, wood, precast ceramic panels and/or metal
 - Stone, cast stone, metal or similar durable materials for trim
 - Fiber cement board and/or siding and/or panels (or comparable) shall be limited to a maximum of 20% of the materials used on the building facade visible from a street or park/open space.
 - Prohibited materials include synthetic stucco, regular ground or split face CMU, and any masonry units with an expressed size of 8" x 16" and vinyl siding.
- 5.51 HVAC and mechanical equipment shall be integrated into the overall building design and not be visible from an adjoining street and/or park. Wall units or vents shall be prohibited, unless recessed within a balcony or shall be integrated with the design of the building.
- 5.52 Sides and rears of buildings that are visible from an adjoining street and/or park shall be designed in a compatible manner utilizing a similar architectural treatment as the primary façade. Blank façades shall be prohibited for any street frontage.
- 5.53 The solid to void ratio shall consist of a minimum of 30% void for each building which shall exclude ground floor commercial-retail areas where provided.
- 5.54 Buildings shall generally provide a vertical fenestration pattern.

URBAN DESIGN CHARACTER

- 5.55 Mirrored reflective, frosted reflective or darkly tinted glass is prohibited.
- 5.56 Windows shall be used as an element that helps to articulate the character of a façade, and designed to reveal the thickness/depth of the façade wall.
- 5.57 Windows shall be well-proportioned and operable, if feasible.
- 5.58 Windows shall be grouped to establish rhythms across the façade and hierarchies at important places on the façade.

5. Building Fenestration

- 5.59 Window and door placement shall provide a high degree of transparency at the lower levels of the building, maximize visibility of pedestrian active uses, provide a human-scaled architectural pattern along the street and establish a pattern of individual windows and exterior openings within building façades that provides a greater variety of scale through material variation, detail and surface relief.



III. OFFICE AND HOTEL BUILDINGS

The proposed office and hotel buildings are located adjacent to transit stops on Route 1, as well as within ½ mile radius of the Potomac Yard Metrorail station. Parking for the buildings will be located entirely below grade. The buildings are required to provide a taller first floor (15 to 18 ft.) to enable retail or maker uses or flexibility depending on the location with the Plan area. The maximum allowed height for hotel and office ranges from 75 to 100 feet, respectively.



BUILDING TYPE - OFFICE AND HOTEL STANDARDS

1. Parking

5.60 Parking for each building shall be located entirely below grade or entirely screened with an active use. The screening of the parking with active uses shall be provided for each level of the entire perimeter of each street and/or park, courtyard, and/or open space frontage or where visible from an adjoining street or open space.

5.61 Permanent surface parking lot(s) are prohibited.

2. Streetscape Level – First Level

5.62 Window and door placement shall provide a high degree of transparency at the lower levels of the building, maximize visibility of pedestrian active uses, provide a human-scaled architectural pattern along the street and establish a pattern of individual windows and exterior openings within building façades that provides a greater variety of scale through material variation, detail and surface relief.

3. Building Character and Materials

5.63 Buildings shall be architecturally differentiated through the use of color and materials within each block.

5.64 All building materials shall be used to express their specific purpose and express the tectonic nature of the materials; for example, heavier materials support lighter materials.

5.65 Building materials for each façade shall consist of the following:

- Brick, glass, stone, wood, precast and/or metal
- Stone, cast stone, metal or similar durable materials for trim
- Prohibited materials include synthetic stucco, regular ground or split face CMU, and any masonry units with an expressed size of 8" x 16"

5.66 Sides and rears of buildings that are visible from an adjoining street and/or park shall be designed in a compatible manner utilizing a similar architectural treatment as the primary façade. Blank façades shall be prohibited for any frontage.



- 5.67 The solid to void ratio shall consist of a minimum of 30% void for hotel buildings and 35% void for office buildings and may include spandrels.
- 5.68 A minimum of 50% of the linear ground floor retail façade (as measured from floor-to-floor) and any second floor retail façade shall be constructed of transparent materials.
- 5.69 Buildings shall generally provide a vertical fenestration pattern.
- 5.70 Mirrored reflective, frosted reflective or darkly tinted glass is prohibited.
- 5.71 Windows shall be used as an element that helps to articulate the character of a façade, and designed to reveal the thickness/depth of the façade wall.
- 5.72 Windows shall be well-proportioned and operable, if feasible.
- 5.73 Windows shall be grouped to establish rhythms across the façade and hierarchies at important places on the façade.
- 5.74 Strip and/or ribbon windows shall be prohibited on all building façades.



G. PORCHES

To create a secondary scale at the pedestrian level and to reinforce the character of Del Ray and Lynhaven into the Plan area, porches are required for a portion of the new townhouses. They are also encouraged for new multi-family buildings where multi-family residential uses come to the ground.

Figure 39A: Front Porch Requirements

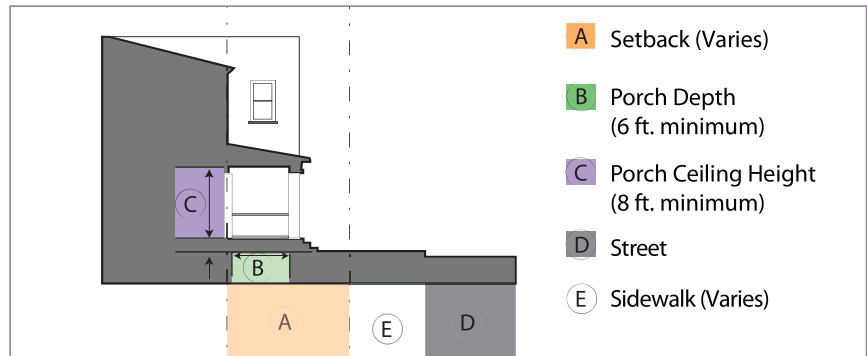
Standards	
Width, Clear	8' min.
Depth, Clear	6' min.
Height, Clear	8' min.
Finish Level above Sidewalk	12" min.

PORCH STANDARDS

The porches for the townhouses as required herein will project from the primary façade and generally have a small-to-medium setback from the sidewalk. The projecting porch shall be open on three sides.

- 5.75 Front porches shall be provided for a minimum of 50% of all new townhouses constructed as part of each project. Front porches shall remain open, not closed.
- 5.76 Where porches are provided for multi-family buildings, the porches shall comply with the applicable requirements herein. Front porches shall remain open, not closed.
- 5.77 If provided, second floor balconies shall have a minimum depth of three feet and a minimum underside clearance of 8 feet. Exceptions shall include Juliette balconies.
- 5.78 Although not required for multi-family buildings, porches or stoops are encouraged.
- 5.79 The material of the porch shall be compatible with the design of the building. Materials shall be wood and/or metal.
- 5.80 New porches shall comply with the requirements of Figure 39A.

Figure 39B: Front Porch Requirements Diagram



H. BUILDING ENTRIES

Building entries assist in enhancing the scale, activity and function of each building. This is achieved by requiring building entries at frequent intervals for the street and park frontages. Building entries should also reinforce pedestrian activity and circulation along the street. The building entries are required to be distinctive features and be an integral part of the design of the building, with a size and scale appropriate to the scale of the building. The entries should be easy to locate from the street for pedestrians and motorists.



BUILDING ENTRY STANDARDS

- 5.81 Building entrances shall be given prominence on the street frontage, sized and scaled appropriately for the scale of the building and have a change in material, wall plane, and/or color.
- 5.82 The primary pedestrian entrance shall front the adjoining street.
- 5.83 Enhanced level of architectural design and treatment are required, and, where appropriate, landscape treatment should emphasize the primary entrance.
- 5.84 Differentiate architecturally between residential and commercial entrances in mixed-use buildings.
- 5.85 Entries shall provide protection from the elements, with canopies, recesses, or roof overhangs to reinforce the pedestrian scale.
- 5.86 Unless ground floor retail is provided, buildings that have frontage on more than one street shall provide their primary entry based upon street hierarchy (ex: primary entry provided on "A" street vs. "B" street) and secondary entrances on the remaining streets. Corner entrances are permitted.
- 5.87 For required retail frontages, the width of residential and/or office lobbies shall be the minimum necessary.
- 5.88 Encourage the provision of entrances to retail, residential and other active ground level uses generally every 20 to 75 ft. along the street frontage.
- 5.89 Explore the provision of as many entries as possible at the street frontages.
- 5.90 For residential frontages, the frequency of the entries must relate to the size of the unit fronting the street, and shall occur on average every 20 to 30 feet along public rights-of-way. Two entries may be grouped together.
- 5.91 Handrails for all building types shall not be permitted within the required/ provided sidewalk.

I. BUILDING ROOFS

The roof of the building is required to be integrated as part of the architectural form of the building. Also where flat roofs are provided, there is an opportunity for roof-top open spaces.

BUILDING ROOF STANDARDS

- 5.92 Rooftop equipment (including elevator equipment, HVAC equipment, etc.) shall be concealed in penthouse structures and designed as an integral part of the building and/or screened with a parapet. Mechanical penthouses and roof top equipment shall be designed as an extension of the building, employing building materials and design treatments consistent with the exterior façades of the building when visible from a public street or open space.
- 5.93 To the extent where visible from the street, roof penetrations such as vents, attic ventilators, flues, etc. shall be placed to limit their visibility from the street and designed in material and color to match the color of the roof, except those made of metal, which may be left natural.
- 5.94 Sloped roofs shall be metal, slate, tile, or other comparable high quality material.



J. WALLS/FENCES

Wall and fences provide transitions between the private and public realm and contribute to the spatial definition of streets and privacy of yards and courtyards. The Standards require high quality materials and height limits for fences and walls.

WALLS/FENCES STANDARDS

- 5.95 The height, length, and visual impact of walls and fences shall be pedestrian scale and in no case shall exceed 3 ft. in height in the front or side yards. In the rear yards, 6 ft. fences may be provided, if approved as part of the development review process.
- 5.96 Materials for walls shall be brick and/or stone. Garden screen wall and/or retaining walls should be constructed of brick, stone, architectural precast or other highly finished appropriate material
- 5.97 Materials for fences shall be decorative metal or wood.
- 5.98 Greenwalls and living walls are strongly encouraged.



K. PUBLIC REALM STREETSCAPE

The design of the streets, sidewalks, and streetscape will play a role as important as the design of buildings in enhancing the streets and promoting pedestrian-oriented streets. Elements such as street and sidewalk widths, trees, lighting, street furniture, and pavement materials need to be integrated to ensure the provision of pedestrian oriented streets. In addition to requiring a pedestrian-oriented environment, the Standards also require the implementation of green and sustainable infrastructure. Through promotion of the use of sustainable elements, such as permeable paving and the incorporation of stormwater and water elements in the public realm, the standards will assist in integrating the environmental sustainability of the site as part of the streetscape design. The intent of the design standards for the public realm-streetscape strive to achieve the following:

- Improved street ecology
- Unified streetscape design.
- Universal design.
- Integrating pedestrians with transit
- Extensive greening
- Space for public life
- Enhanced pedestrian safety
- Public art

For additional requirements, including standards related to sidewalks and street trees, refer to the City's Complete Streets Design Guidelines Manual.



PUBLIC REALM - STREETScape STANDARDS

1. Sidewalks

5.99 Refer to attached street cross-sections in Section 8.

2. Street Trees

5.100 Refer to Complete Streets Guidelines.

3. Street Furniture

5.101 Each project shall provide street and on-site furniture and amenities for public use. Street furniture shall include benches, bicycle racks, and trash receptacles.

Benches

5.102 Benches shall be located on public streets and shall be the Victor Stanley Classic Series CR-96 or similar as approved by the City of Alexandria. A minimum of two benches shall be provided for each block in appropriate locations based on the specific ground floor use and the location of bus stops and public open space.

Bike racks

5.103 To encourage and facilitate biking as a means of transportation, bike racks that conform to the City's bike rack standards shall be provided and placed in groups at convenient, safe, well lit paved areas in the building or curb zone. Bike racks shall also be provided in parking garages and at appropriate park amenities.

Trash/Recycling Receptacles

5.104 The trash receptacle to be used throughout the area is the Iron Site Bethesda Series Receptacle with domed lid (model SD-42) by Victor Stanley with black, powdercoat finish (or equal as approved by the City of Alexandria). Trash receptacles shall also include accommodations for recycling. One trash receptacle shall be located at each intersection.

4. Lighting

5.105 Street light fixtures shall be single black Dominion Virginia Power acorn lighting fixtures for all streets (except Route 1) with a standard black finish.

5.106 The street light fixtures on Route 1 shall be double acorn with a standard black finish.

5.107 All street lights shall be placed to avoid conflict with street trees, and shall not be located within the sidewalks but rather shall be placed between the street trees.

5.108 All street lights shall be designed to minimize light spillover. Where located next to residential uses, street lights should include shielding as needed to prevent lighting from directly entering residential windows or adjoining public parks.

L. REAR OF BUILDINGS - ALLEYS - TOWNHOUSES - MULTIFAMILY BUILDINGS



REAR OF BUILDINGS - ALLEYS STANDARDS

- 5.109 Use same material on rear facades as the front and side of townhouses.
- 5.110 Paving material should be designed for durability. Change paving materials, colors in alleys to minimize visual expanse the asphalt paving of the alley
- 5.111 Add elements such as porches and bays where feasible to soften the rear facades and alleys.
- 5.112 Add landscaping and trees to minimize the visual impact to the adjoining homes.

Intent: The sides and rears of some of the proposed townhouses and multifamily will be visible from the adjoining neighborhoods. Therefore, the standards require materials, architectural treatment, and landscaping to be compatible with the adjoining neighborhoods.

6

GROUND FLOOR USES

- RESIDENTIAL
- RETAIL
- MAKER



A. RESIDENTIAL USES AT GRADE

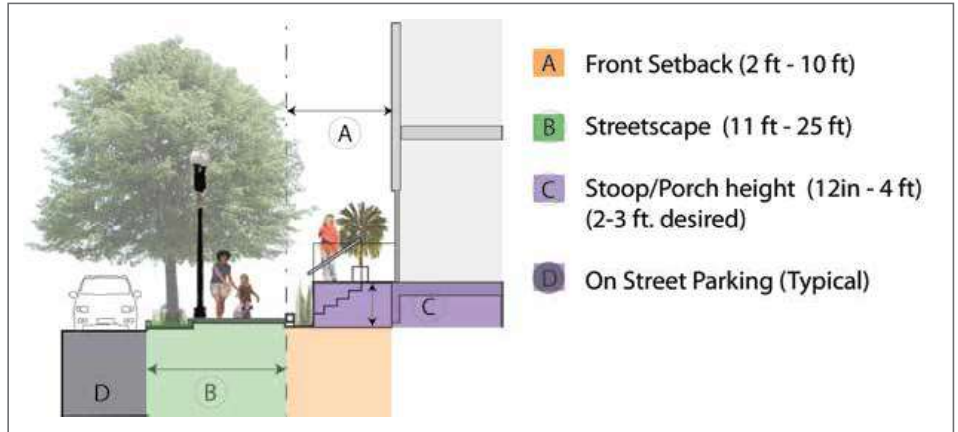
To ensure an appropriate relationship between the ground floor residential uses and the adjoining sidewalk, the residential uses are required to provide a transition. This transition between the sidewalk and the residential building is achieved with front setbacks for porches or landscaping and elevation of the ground floor uses enables sufficient privacy for ground floor residents, and an appropriate relationship between the pedestrian and the building.



RESIDENTIAL USES AT GRADE STANDARDS

- 6.1 Residential buildings shall provide a front setback of 2-10 feet from the required sidewalk to provide space for individual front yards, plantings, landscaping, fences, stoops, and similar elements.
- 6.2 Ground floor levels for all residential uses shall be elevated a minimum of 12 inches and maximum of 4 feet above the adjoining sidewalk. 2-3 feet is desired.
- 6.3 For multi-family buildings (where ground floor commercial space is not provided) individual and functional entries shall be provided at 20-30 ft. intervals.
- 6.4 Where at-grade accessible units are appropriate, alternatives shall be considered to the satisfaction of the Director of Planning and Zoning.

Figure 40: Residential Uses at Grade Requirements



GROUND FLOOR USES

The City's successful retail streets and storefronts reflect a fine-grain pattern of multiple shops and businesses. Within a given block, the variety of retail offerings, complexity of window displays and multiple entrances provide the pedestrian with a significant level of visual interest. The successful performance of the retail areas will be directly related to the successful design and construction of their retail storefronts. It is the intent of the retail storefronts that all retail tenants will have the opportunity to design and install their own storefronts. Storefronts should be "individual" expressions of a tenant's identity. Tenants and buildings will be required to avoid uniform storefronts.



Figure 41: Ground Floor Uses: Primary, Secondary Retail, and Maker Spaces



RETAIL USE AND RETAIL STOREFRONT STANDARDS

- 6.5 The retail frontages shall be designed to create a comfortable yet highly animated pedestrian environment utilizing a rhythm of multiple retail frontages architecturally articulated through materials, colors, numerous entrances, display windows, canopies and signage.
- 6.6 Building materials shall be high-quality and contribute to a human-scaled public realm. Blank walls shall be prohibited.
- 6.7 To establish pedestrian-scaled design on the ground floors of larger buildings, use window groupings, material changes, or columns on the principal façade to accentuate individual storefronts and denote a smaller increment of building bays.
- 6.8 For retail, generally provide transparent windows for a minimum of 70% of the retail area. Flexibility may be considered based on creativity and the overall compatibility and character of the storefront design, meets the intent of the Standards, and is approved by the Director of P&Z.
- 6.9 Corner retail storefronts shall extend at least 40 ft. along the side street and/or park-open space, and shall also be expressed in the architecture.
- 6.10 The design of the storefront shall be appropriate to the scale and architectural design of the building.
- 6.11 The design and construction materials of the ground-level storefronts shall be appropriate for a retail street, to help contribute to an active pedestrian-oriented street. These shall include: how the storefront fits into the architecture of the buildings; relationship to varying grades along the storefronts, and the flexibility to adjust store entries; visibility of storefronts (including clear glass); sidewalk spaces for outdoor retail displays or dining; sign and logo requirements; and the design, materials and colors of awnings or canopies to protect pedestrians and windows.
- 6.12 The materials for the retail storefront shall consist of stone, metal, glass and/or wood. Construction detail and finish shall adhere to craftsman's standards. Durable materials such as these are especially critical at the street level where pedestrian contact will be considerable. Storefronts should be predominantly glass to provide views into the store. Translucent composite materials may be acceptable and reviewed as part of the development review process.
- 6.13 The design of the retail storefronts shall be administratively approved subject to the standards required herein.

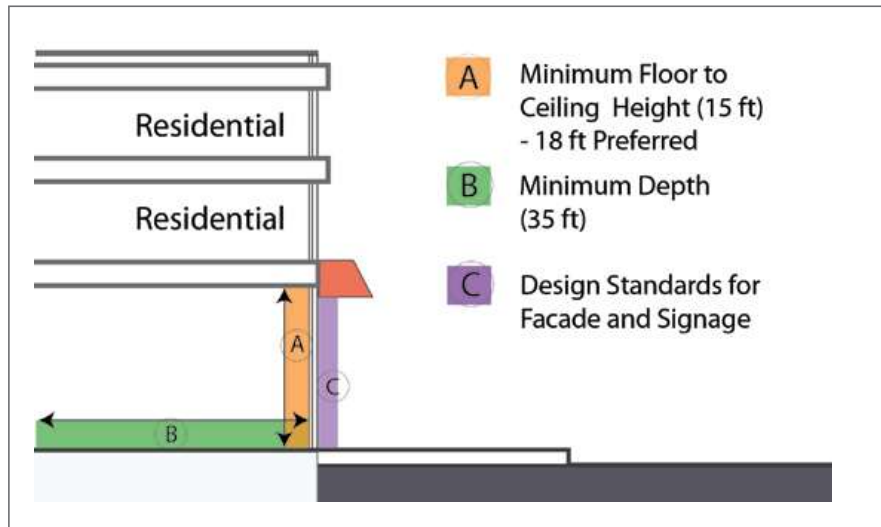


GROUND FLOOR USES

The goal of maker spaces is to enable uses that diversify the City's economy, complement and enhance the neighborhoods, and provide locations for existing and new small businesses and emerging industries. Maker types of uses include craft manufacturing, light production, wholesale, repair, and can include other neighborhood-serving light industrial uses. These uses typically require taller ceiling heights, and deeper bays, than typical retail, and often showcase their work with large windows or garage bays at street level.

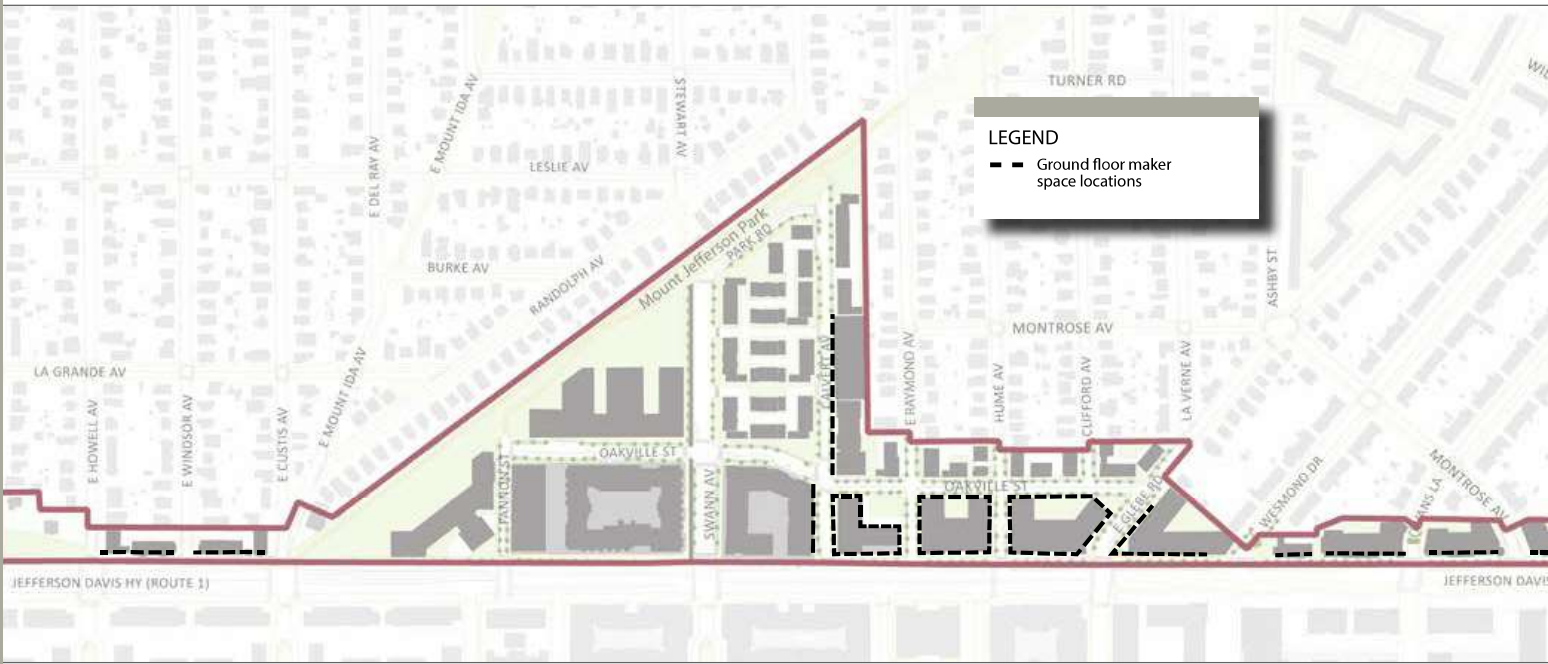
The specific requirements for the types of uses and associated requirements will be established as part of the CDD zoning for the Plan area.

Figure 42: Minimum Maker Space Requirements



GROUND FLOOR USES

Figure 43: Ground floor Maker Spaces



GROUND FLOOR USES

MAKER SPACE STANDARDS

- 6.14 Each maker space shall provide a minimum of 40% transparency (garage doors, doors and windows) at the street level.
- 6.15 A garage door or comparable sized opening shall be provided for each space or approximately every 20-30 feet.
- 6.16 Garage and/or roll up doors shall be glass and metal.
- 6.17 Signage shall comply with the applicable signage provisions herein.
- 6.18 The uses shall be subject to all applicable requirements of the CDD zoning and associated requirements.
- 6.19 The floor to ceiling height shall be a minimum of 15 ft., with 18 ft. preferable. The minimum depth of each space shall be a minimum of 35 feet.
- 6.20 Adequate loading, access, refuse collection, and noise attenuation shall be addressed during the development review process.
- 6.21 Flexibility may be granted for exhaust, fans, and vents on primary building facades that support the building function/use. Final location and treatment will be determined as part of the development review process.

INTENTIONALLY LEFT BLANK

7

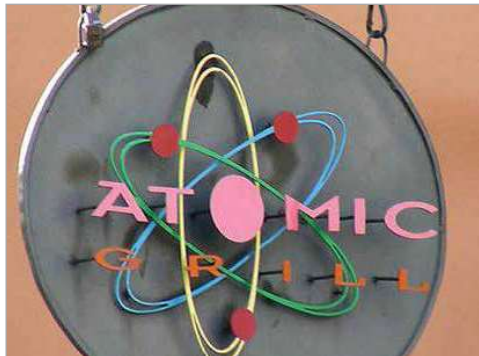
SIGNAGE



SIGNAGE

The intent of the signage requirements is to encourage creativity, uniqueness and high quality graphics, while also being compatible with the adjoining residential neighborhoods. Signs shall be designed to form an attractive composition integrated into the architectural design of the buildings or retail storefront elements. Tenants are encouraged to take maximum advantage of store logos, specialty letter types and graphic flourishes. Variety and creativity of design are encouraged. Retail tenants are encouraged to explore a variety of diverse signage styles with the objective of integrating the design into the whole storefront design, not as an applied afterthought. National and regional stores are encouraged to consider the mixed-use and urban character and look for ways to create signage that is unique and distinctive.

The design standards establish the overall requirements for signage within the Plan area. To the extent that property owners or tenants wish to establish additional provisions, a coordinated sign program special use permit may be requested. While a coordinated sign program may be requested, it will still be required to adhere to the intent of the design standards.



SIGNAGE

WALL SIGNS



MURAL SIGNS



AWNING SIGNAGE



WINDOW SIGNS



BLADE SIGNS



RETAIL USE AND MAKER USE SIGNAGE STANDARDS

- 7.1 Each retail tenant shall install a minimum of one sign for each retail street frontage appropriate to the scale of each façade. In addition, each retail tenant shall provide a second pedestrian-oriented sign such as a projecting sign, blade or window sign for each street frontage.
- 7.2 Retail tenants shall be allowed a maximum of 1 sq. ft. of sign area per linear foot of tenant storefront and a maximum of 50 sq. ft. of signage. The Director of P&Z may approve signage for retail uses up to 2 sq. ft. per linear foot of frontage for exceptional design.
- 7.3 Hotel uses shall be permitted a maximum sign area of 2.5 sq. ft. per linear foot of building frontage not to exceed 75 sq. ft. For purposes of calculating sign area, building frontage shall be limited to frontage on a public street. Hotels located on a corner cannot use total allowed sign area on one frontage.
- 7.4 Retail, residential, hotel and office signs shall be limited to a maximum height of 20 ft. above the grade of the adjoining sidewalk. The Director of P&Z may permit hotel signage above 20 ft. above the grade of the adjoining sidewalk provided that the illumination does not have an adverse impact on adjoining residential uses or. However, in no case shall signage exceed 50 ft. above the grade of the adjoining sidewalk. The intent is to minimize visibility from adjoining neighborhoods and Mount Jefferson Park.
- 7.5 Awnings shall be permitted to project up to 4 ft. from the building; greater projections require approval of the Director of P&Z. Greater projections which encroach into the public street may require City Council approval of an encroachment.
- 7.6 Projecting signs are required for each retail use and shall be appropriately sized and proportional to the building and/or storefront. Signs projecting over the sidewalk shall be a minimum of 8 ft. above the sidewalk. Projecting signs may be internally illuminated if approved by the Director of P&Z. Externally illuminated projecting signs shall have lighting fixtures that are complementary and integrated into the storefront design.

- 7.7 Retail tenants may incorporate window graphics; however, at no time shall the window graphics exceed 20% of the window area. The Director of P&Z may approve a maximum up to 40% if the design is consistent with the intent of the Standards.
- 7.8 Signage shall be located to not obscure architectural design elements such as projections, cornices, or change of building material or pattern.
- 7.9 Each maker tenant shall install a minimum of one sign for each street frontage appropriate to the scale of each façade.
- 7.10 A-Frame and similar signage shall comply with the findings of the Ad hoc Workgroup on A-Frame Signs and applicable City requirements and policies.

AWNINGS SIGNS AND BANNERS (RETAIL AND MAKER SPACES)

- 7.11 Awnings, when projecting from the building face, shall allow a clearance of 9 ft. from the grade of the adjoining sidewalk.
- 7.12 Fixed lightweight metal and glass structures are acceptable.
- 7.13 Awning or canopy material shall be a woven fabric or other material that conveys the aesthetic of the natural material of canvas, metal, glass etc.

RESIDENTIAL (MULTI-FAMILY BUILDINGS)

- 7.14 Signage shall be located to not obscure architectural design elements such as projections, cornices, or change of building material or pattern. Signs shall be limited to a maximum height of 20 feet above the grade of the adjoining sidewalk. Signage taller than 20 feet may be permitted if approved by the Planning Commission and City Council as part of a Coordinated Sign Special Use Permit. The size of the signs shall be limited to 50 sq. ft. The signage shall be located to minimize impacts on adjoining neighborhoods and parks.

MATERIALS – CONSTRUCTION (ALL SIGNS)

- 7.15 Signs shall be in the form of a painted dimensional sign, flat sign, blade sign, illuminated sign, fabricated dimension sign or awnings.
- 7.16 Materials shall be durable natural materials such as cast, polished or painted metal; glazed and ceramic tile; etched, cut or stained glass; cast stone and carved natural stone.
- 7.17 All methods of attachment including fasteners, mounting brackets and other mechanisms must be concealed from view.
- 7.18 Letters and graphics mounted directly onto building façades shall be pin mounted at least 2 inches from the surface onto which the sign is mounted and provide dimensional returns.
- 7.19 Neon signs, signs painted directly on building storefronts, and wall murals may be considered based on creativity and the overall compatibility and character of the tenant storefront design, if approved by the Director of P&Z, and meets the intent of the Standards.

ILLUMINATION- LIGHTING

- 7.20 Back-lit, halo-lit illumination or reverse channel letters with halo illumination.
- 7.21 All illuminated signs and exterior lighting shall be controlled by a time clock, which shall coincide with the normal business hours.
- 7.22 Blade signs shall generally be externally illuminated with decorative bracketed lighting. Internally illuminated blade signs may be considered based on design, if approved by the Director of P&Z and meets the intent of the Standards.

- 7.23 In general, lighting should be designed and located to accommodate public safety without creating glare or excessively high light levels. Fixtures should be chosen to control light trespass either vertically (toward the sky) or horizontally onto neighboring properties.
- 7.24 High pressure sodium vapor (yellow orange) lighting is prohibited for exterior use including buildings, parking facilities, service areas, signage, etc. Such lighting is also prohibited inside parking garages or building entries where it would be visible from the outside.
- 7.25 For any building or project, exterior light fixtures – their design, size, finish, location, etc. - should be compatible with, and appropriate for, the building architecture, materials and colors.

PARKING SIGNS

- 7.26 All parking signage shall comply with the City Wayfinding Guidelines and shall include garage identification and directional signs.

PROHIBITED SIGNS – FOR ALL USES AND BUILDING TYPES

- 7.27 Box signs and signs employing flickering rotating or moving lights shall be prohibited.
- 7.28 Panel box signs shall be prohibited. External raceways are discouraged.
- 7.29 Storage cabinets, carts, window signs, posters, shelving, boxes, coat racks, storage bins, closets, and similar items shall not block the visibility of the interior of the store from the street. This condition, however, is not intended to prevent retailers from displaying their goods in display cases that are oriented towards the street frontage.
- 7.30 All window coverings shall be open as much as possible and provide some interior accent lighting when the business is closed.

- 7.31 All banners relating to commercial promotions, leasing, hiring or advertising shall be prohibited.
- 7.32 Vinyl or plastic awnings, translucent acrylic or comparable shall be prohibited.
- 7.33 Building signage on the western portions of the proposed buildings or visible from Del Ray and/or Lynhaven or Mount Jefferson Park shall be prohibited.

PROCESSING – REVIEW

- 7.34 Each retail, multi-family, office and hotel tenant proceeding with permitting and/or fabrication shall submit detailed drawings and samples to be approved by the Department of P&Z.
- 7.35 Each sign(s) shall require a separate sign permit.
- 7.36 For larger/more prominent signs, the Director of P&Z may require a full-size mock up (constructed from foam core or illustration board) and/or a photomontage image.

WAYFINDING SIGNAGE

- 7.37 All new development sites shall provide wayfinding signage consistent with the City's Wayfinding Design Guidelines Manual.

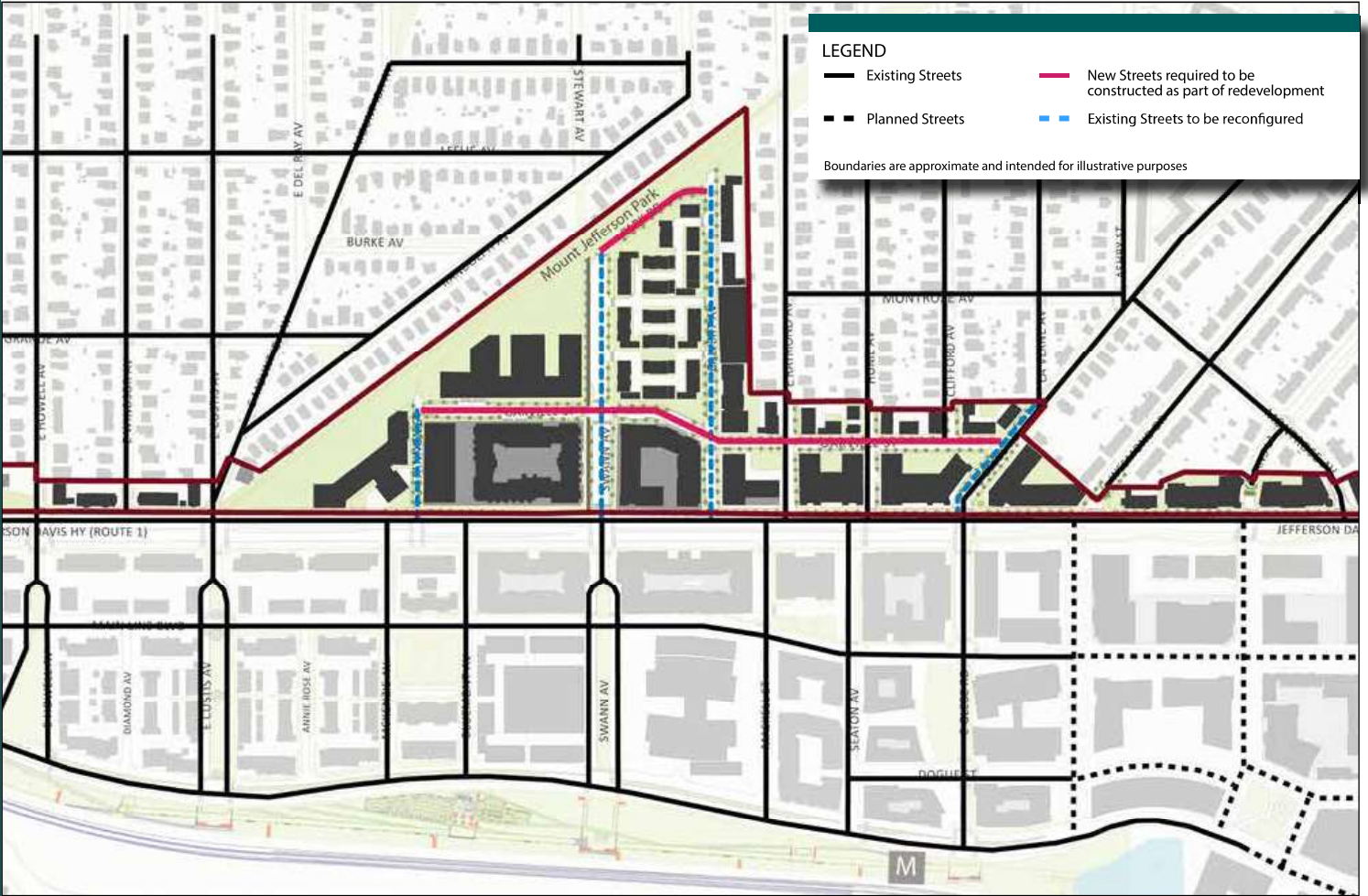
8

STREETS



STREETS

Figure 47: New and Reconfigured Streets



INTENT OF NEW/RECONFIGURED STREETS

One of the measures to ensure the Oakville Triangle/Route 1 Corridor will develop as an urban, pedestrian-oriented series of neighborhoods is to require urban, human scaled block sizes for each of the neighborhoods. Through the placement of the required framework streets, the block sizes are generally the size of blocks within the surrounding communities of Old Town, Lynhaven and Del Ray, which are used as national planning models due to their block size and associated walkability. New and reconfigured streets shall comply with the cross-sections herein and with the standards of the City's Complete Streets Design Guidelines.



STREET CROSS-SECTION STANDARDS

- 8.1 All new streets within the Plan area shall be consistent with the attached street cross-sections.
- 8.2 As part of the development review process, all utilities for new and existing streets shall be located below grade. Accommodation for transformers and vaults shall be identified as during the preliminary development review process.
- 8.3 All streets within the Plan are intended to be public streets, dedicated to the City or *private with public access easement* unless otherwise noted the property line is assumed to be at the edge of the right-of-way (ROW) as defined herein.

A. ROUTE 1 STREETScape - COMMERCIAL - RETAIL USES

TYPOLOGY: MIXED USE BOULEVARD



INTENT

The intent of the Route 1 streetscape, given the scale of the buildings and width of the street, is to provide an appropriate setback for the buildings and a comfortable area for pedestrians. The sidewalk is 15 ft. and will be located adjacent to the building for commercial-retail uses. The 10 ft. landscape strip is intended to provide an adequate area for the trees and setback for pedestrians.

STREETS

ROUTE 1 STREETScape - RESIDENTIAL (MULTI-FAMILY) TYPOLOGY: MIXED USE BOULEVARD



INTENT

The intent of the Route 1 streetscape given the scale of the buildings and width of the street is to provide an appropriate setback for the buildings and a comfortable area for pedestrians when residential is the primary use at the ground floor. The streetscape provides a 10 ft. landscape strip for the street trees and a setback for the pedestrians. A landscape strip planting area is also provided adjacent to the residential building, with a 10ft. sidewalk for pedestrians.

ROUTE 1 STREETScape - RESIDENTIAL (TOWNHOUSE)

TYPOLOGY: MIXED USE BOULEVARD

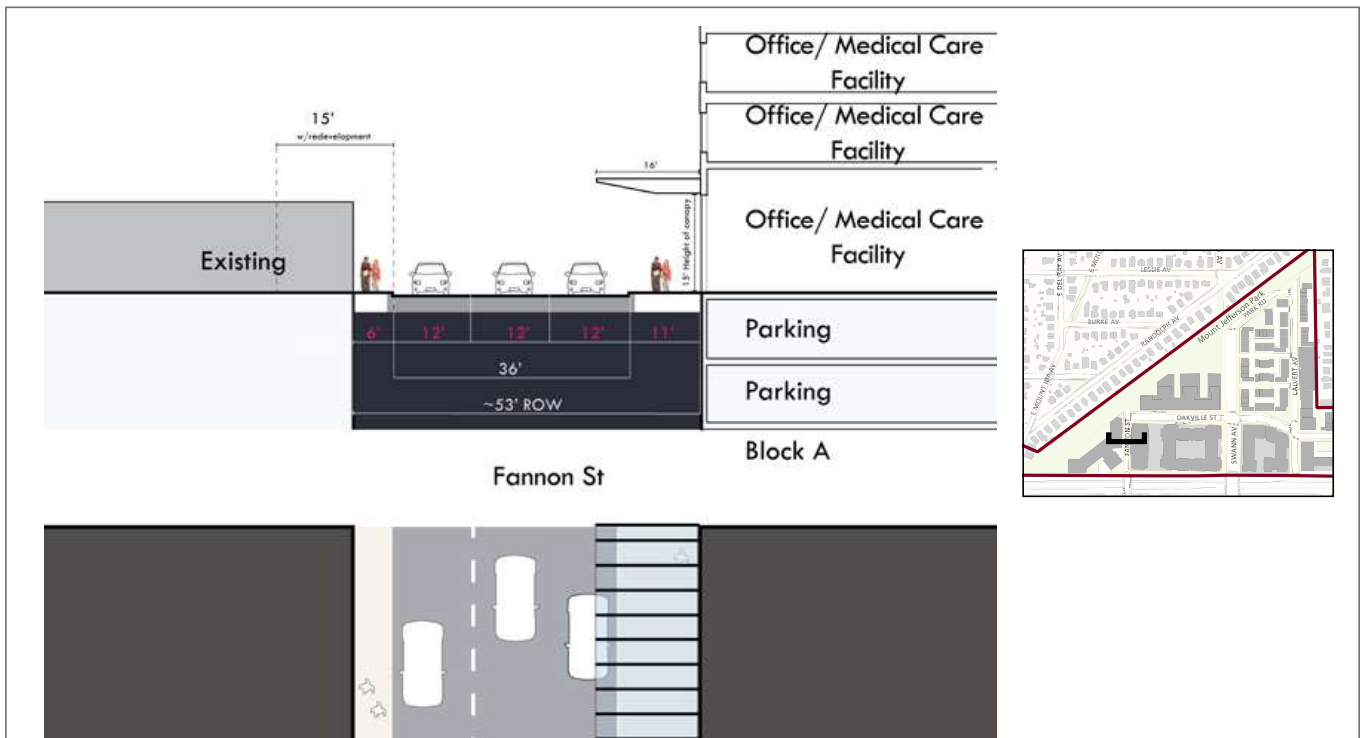


INTENT

The intent of the Route 1 streetscape is to provide an appropriate setback for the buildings and a comfortable area for pedestrians when residential is the primary use at the ground floor. The streetscape provides a 10 ft. landscape strip for the street trees, a 7 ft. sidewalk, and a 8 ft. landscape strip planting area adjacent to the building.

B. FANNON STREET (INTERIM)

TYPOLOGY: MIXED USE BOULEVARD



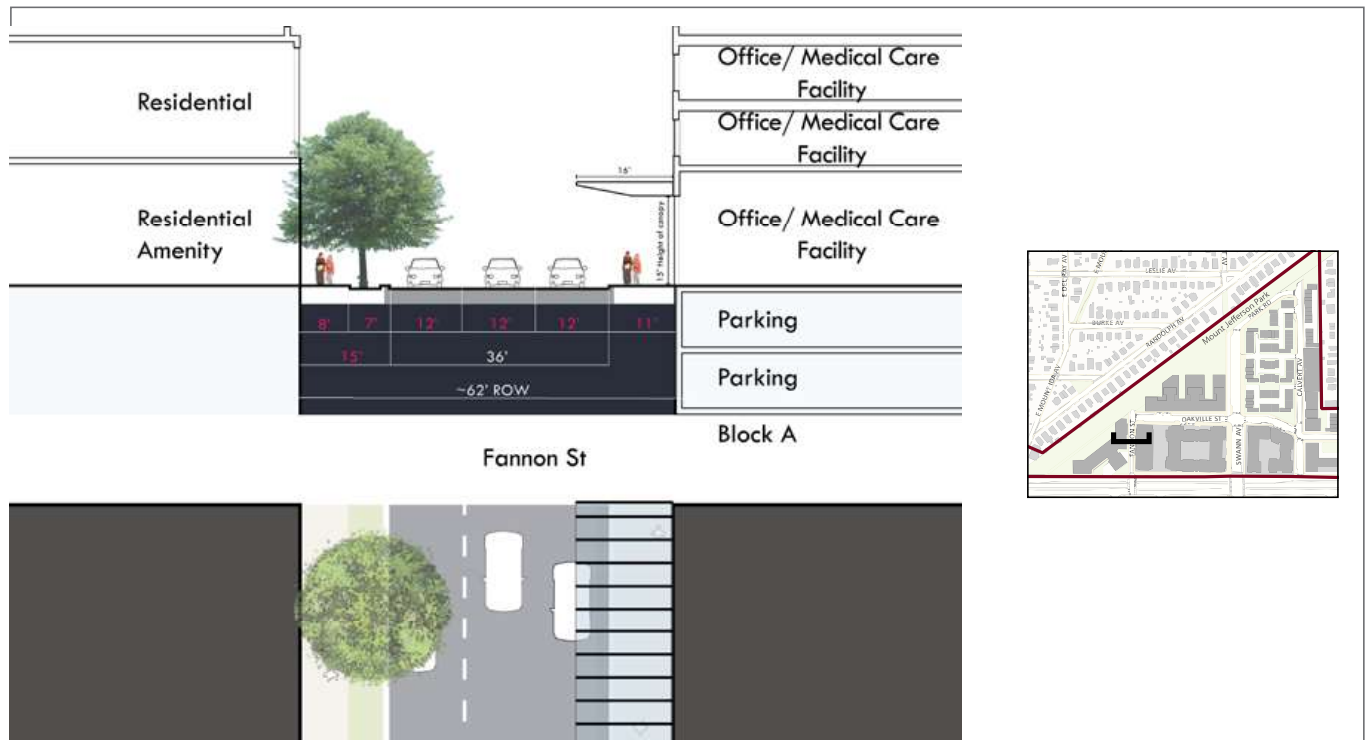
INTENT

The intent of this street is to serve as a predominantly residential street, which also provides pedestrian access to the Mount Jefferson Park. A 15 ft. wide sidewalk is required on each side of the street.

* The sidewalk on the south side will have an interim 6 ft. sidewalk that will be widened to 15 ft. when the adjoining buildings are redeveloped.

FANNON STREET (WITH REDEVELOPMENT)

TYPOLOGY: MIXED USE BOULEVARD

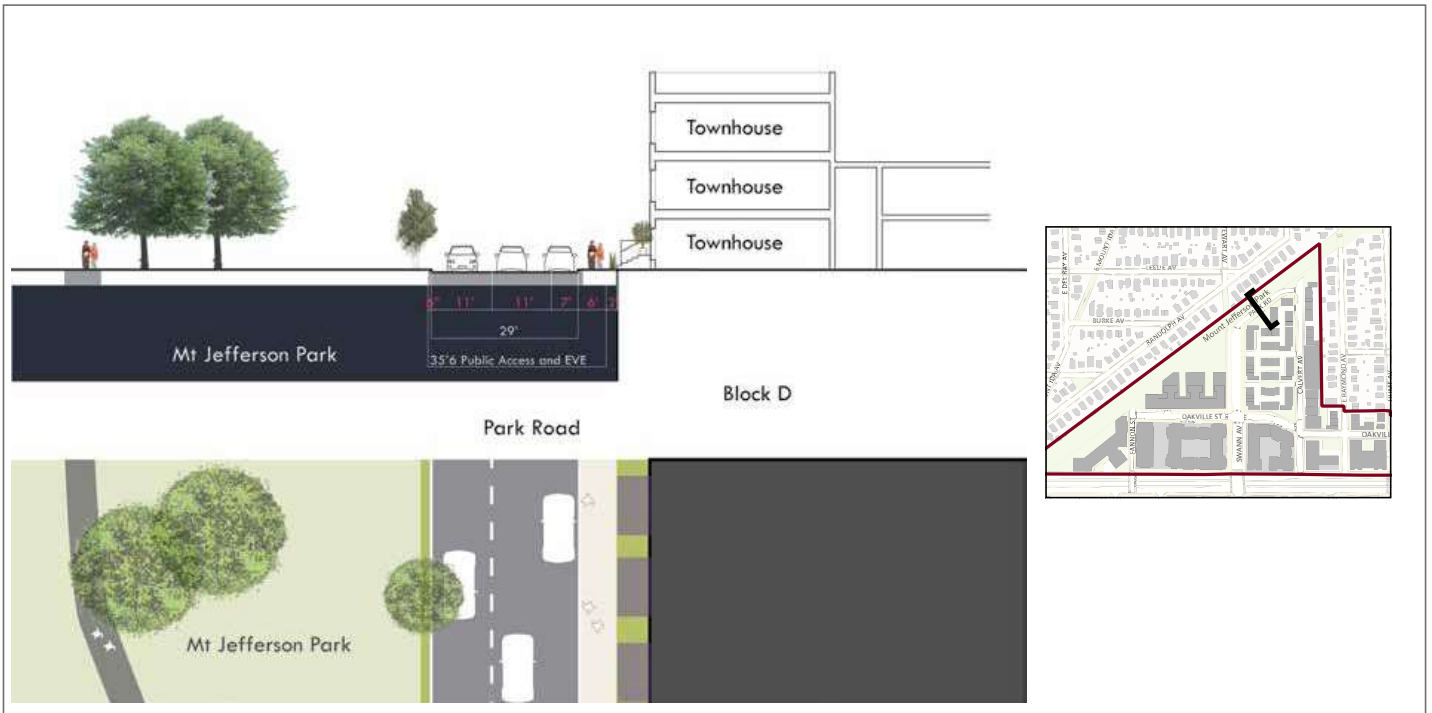


INTENT

The intent of this street is to serve as a predominantly residential street, which also provides pedestrian access to the Mount Jefferson Park. A 15 ft. wide sidewalk is required on each side of the street with redevelopment.

C. PARK ROAD

TYPOLOGY: PARK ROAD

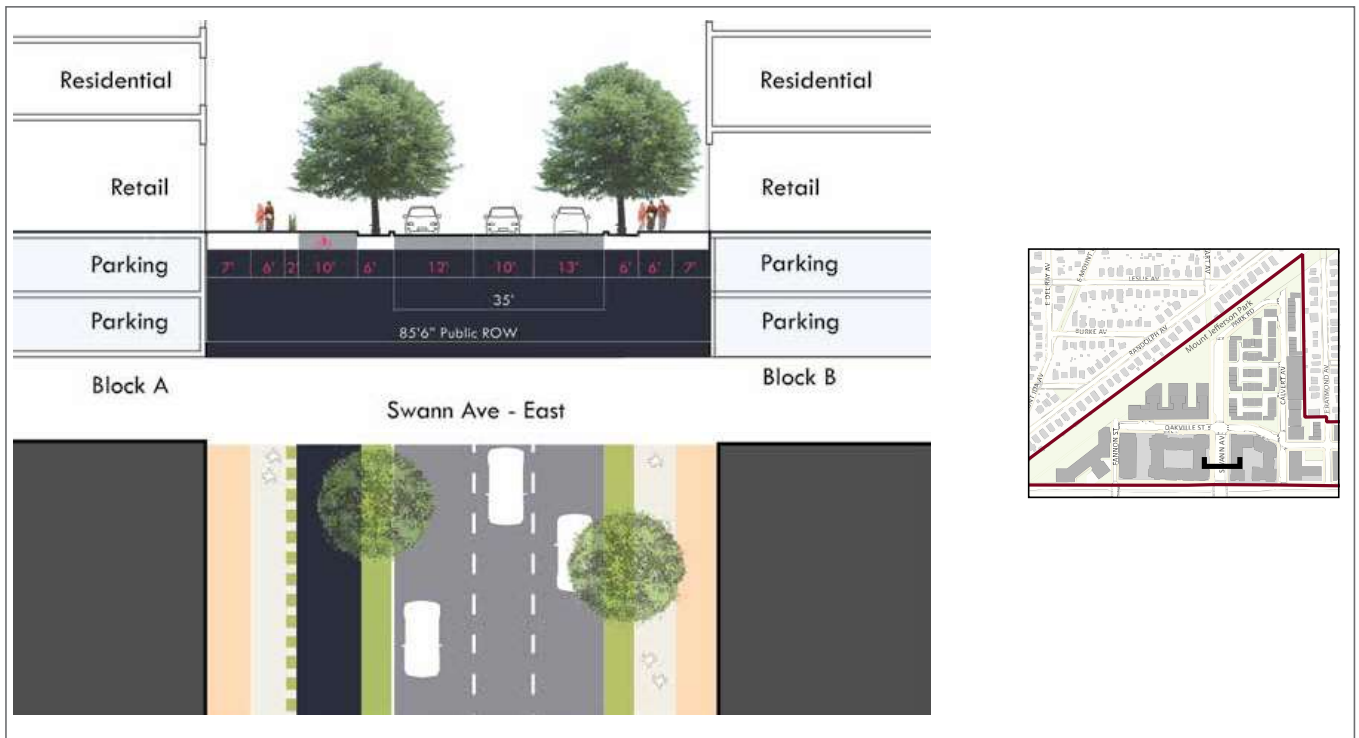


INTENT

This road is a residential street adjacent to the Mount Jefferson Park. The street and streetscape are intended to provide a landscaped edge adjacent to the park with a continual row of street trees, small front yards and porches. The streets also provides parallel parking adjacent to the new buildings and not Mount Jefferson Park.

D. SWANN AVENUE (EAST)

TYPOLOGY: MIXED USE BOULEVARD

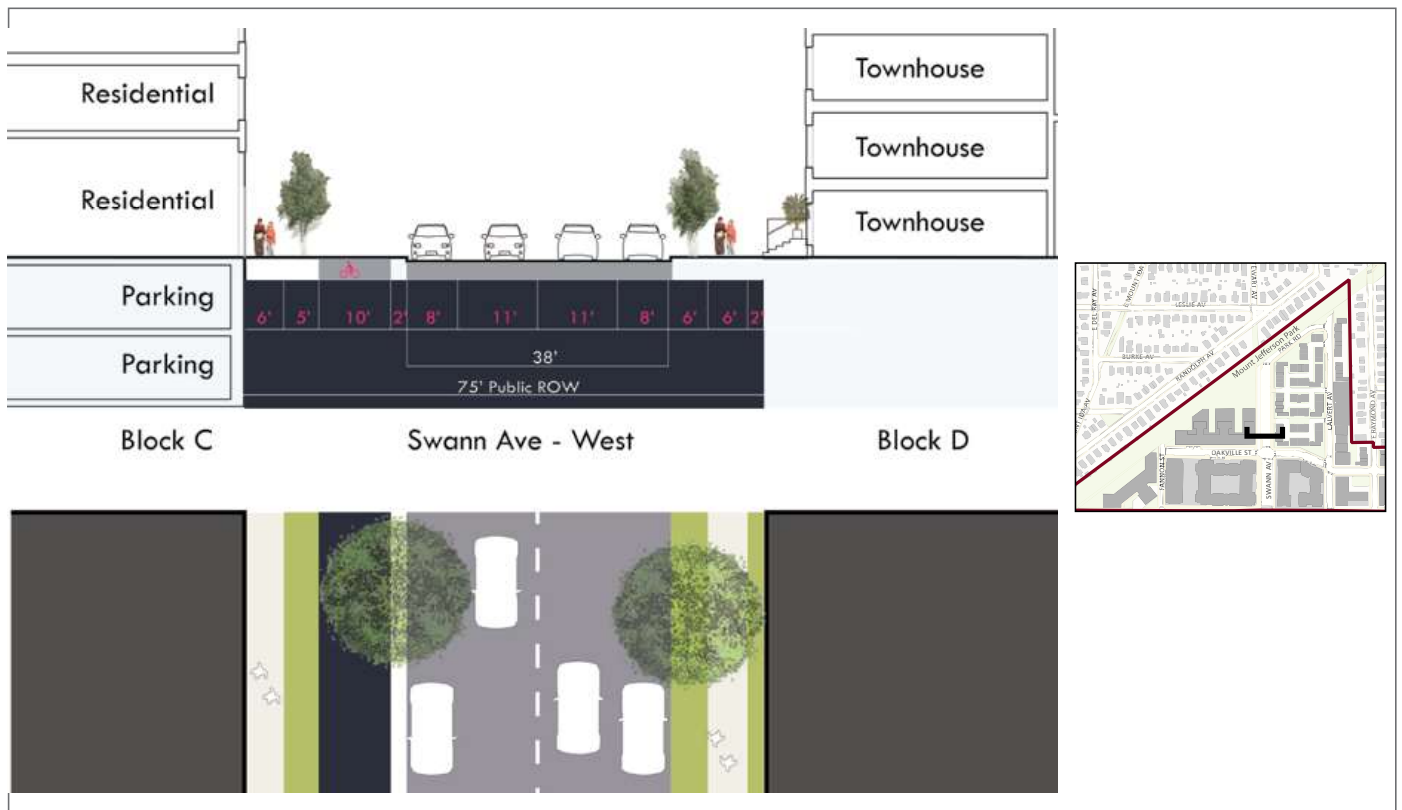


INTENT

The street will serve as the primary entrance to the retail on Swann Avenue. The sidewalks are wide (18 ft.) to accommodate retail and restaurants. A side path on each side of Swann Avenue is provided to accommodate bicycles. The street will have continuous rows of street trees and a landscape median with trees.

SWANN AVENUE (WEST)

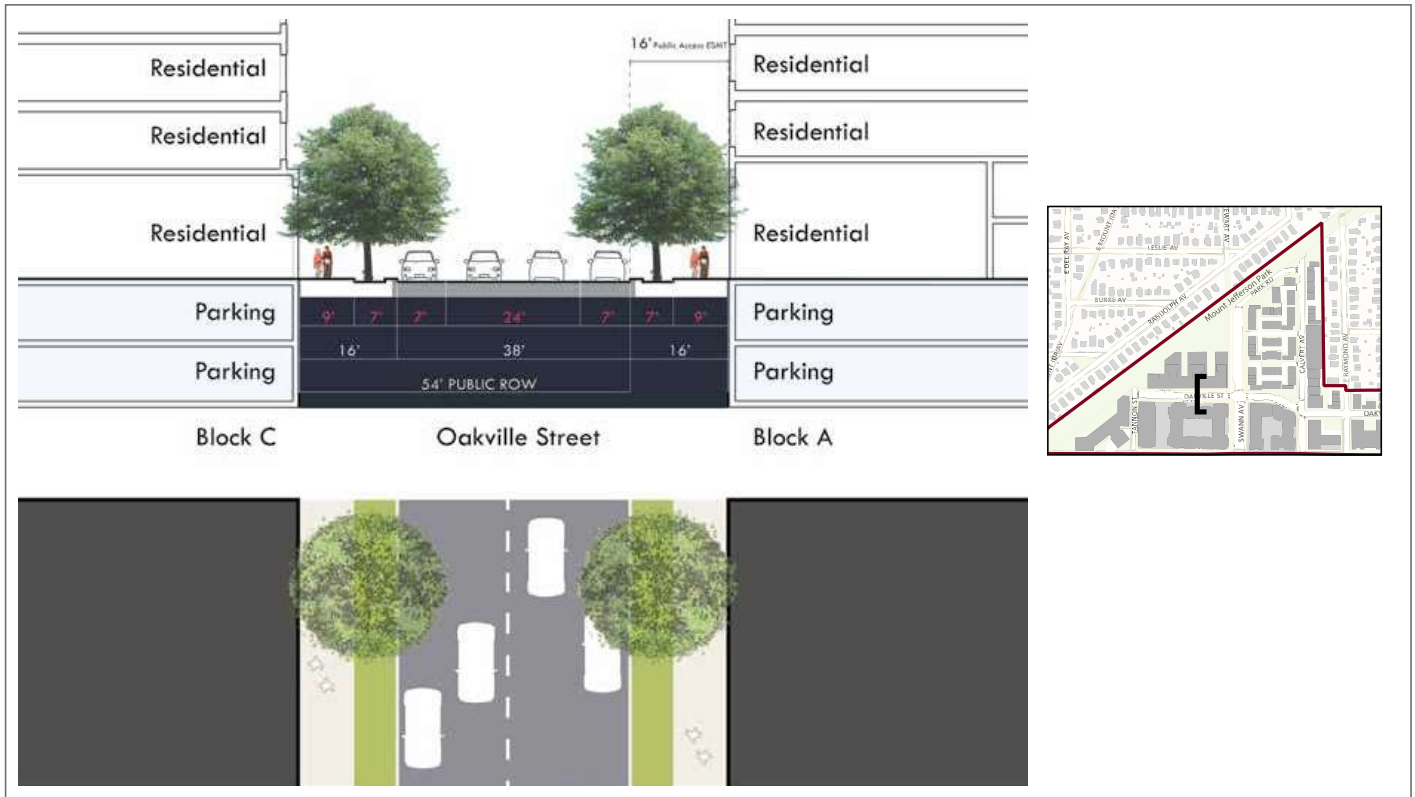
TYPOLOGY: SHARED STREET



INTENT

This area is intended to serve as a shared curb-less urban plaza, used by pedestrians, bikes and cars. The final design of the shared space will occur as part of the development review process.

E. OAKVILLE STREET (FANNON STREET TO CALVERT AVENUE) TYPOLOGY: MIXED USE BOULEVARD



INTENT

This street will have both residential and retail frontages. The street will have generous parking on each side and 16 ft. sidewalks on each side of the street.

STREETS

F. CALVERT AVENUE (WEST - INTERIM)

TYPOLOGY: MAIN STREET

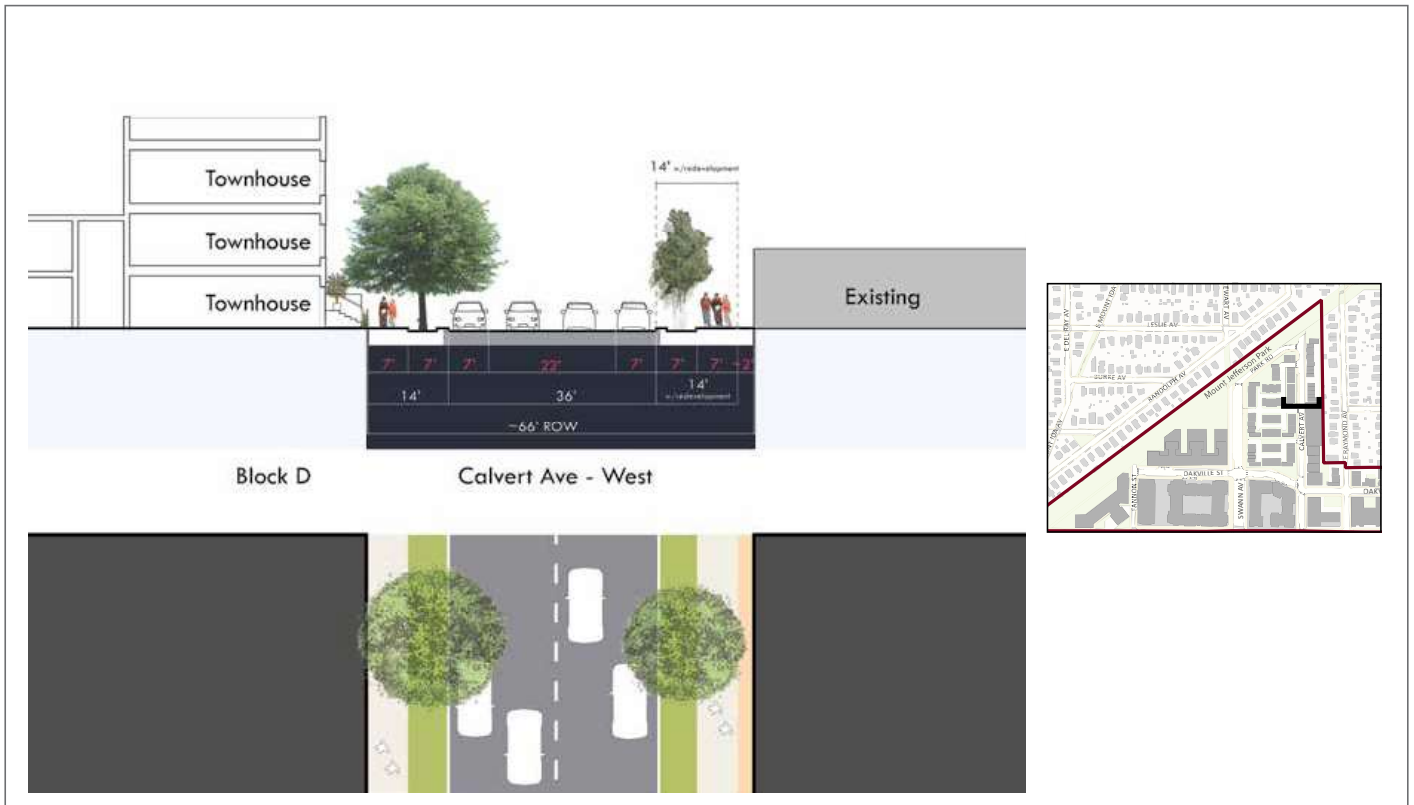


INTENT

This street is intended to be a more intimate street with parallel parking for portions of the street. An interim 11ft. sidewalk will be widened to 14 feet on the north side when the property redevelops.

CALVERT AVENUE (WEST - WITH REDEVELOPMENT)

TYPOLOGY: MAIN STREET



INTENT

This street is intended to be a more intimate street with parallel parking on both sides for a portion of the street. A 14 ft. wide sidewalk is required on each side of the street with redevelopment. The interim 11 ft. sidewalk that will be widened to 14 feet when the adjoining buildings are redeveloped.

CALVERT AVENUE (EAST)

TYOLOGY: MIXED USE BOULEVARD

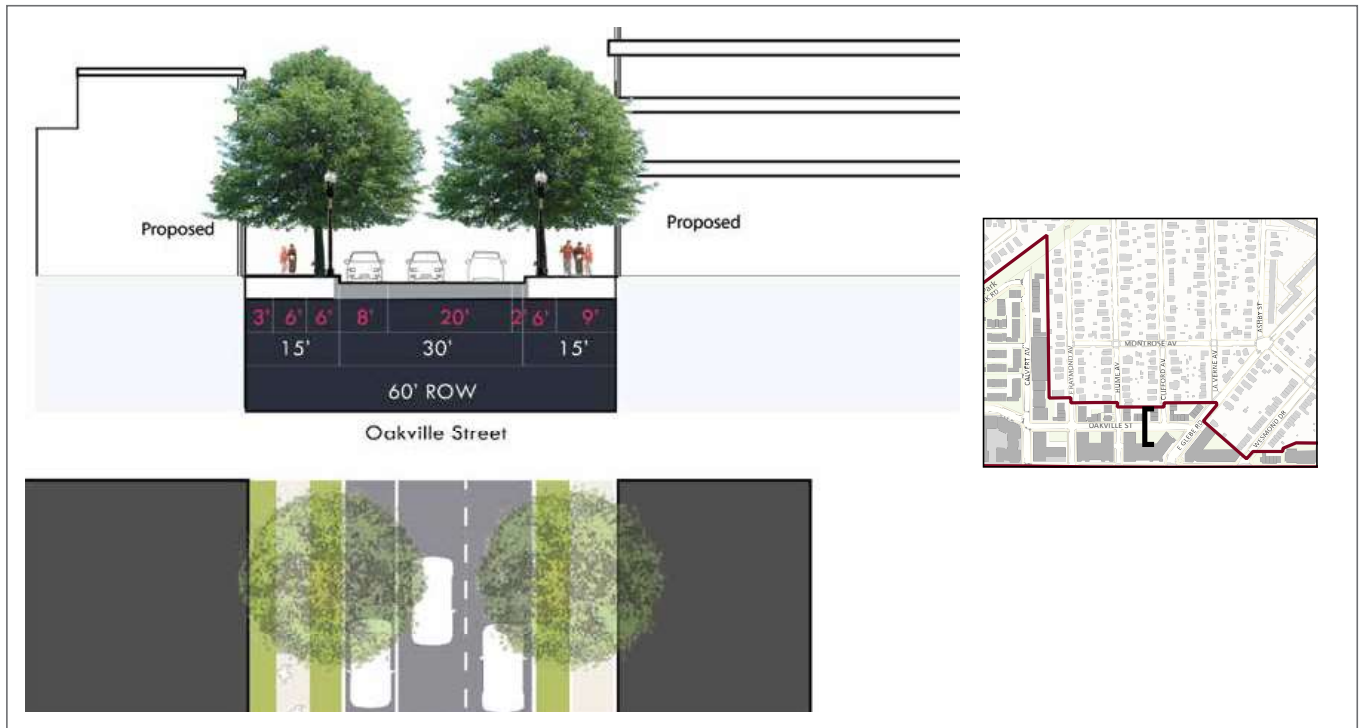


INTENT

This street is intended to be a more intimate street with parallel parking for portions of the street.

G. OAKVILLE STREET (CALVERT AVENUE TO EAST GLEBE ROAD)

TYPOLOGY: MAIN STREET

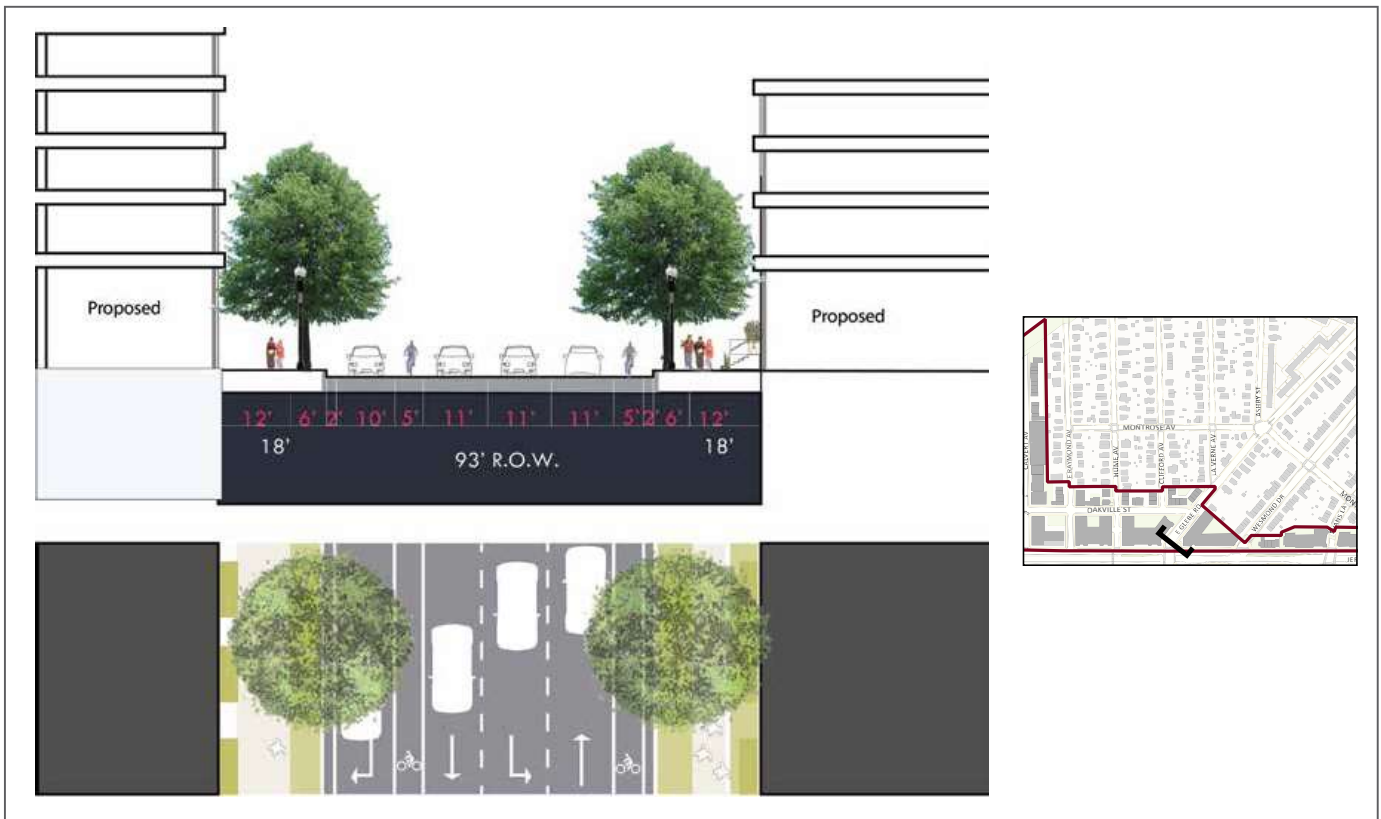


INTENT

This street will connect Calvert Street to East Glebe Road. Parallel parking will be provided on one side. Buildings fronting the street will be a combination of townhouses and multi-family residential buildings.

H. EAST GLEBE ROAD INTERSECTION - RECONFIGURED

TYPOLOGY: NEIGHBORHOOD CONNECTOR



INTENT

This street provides more lanes than the other streets due to the role of this street in the Plan area and the City. This cross-section is only intended for the portion of East Glebe Road within the Plan area. The cross-section also provides on-street bike lanes for cyclists, and wider 18' sidewalks for pedestrians.

INTENTIONALLY LEFT BLANK

9

SUMMARY OF RECOMMENDATIONS & DESIGN STANDARDS



PLAN RECOMMENDATIONS

- 1.1 Establish Design Standards and Guidelines to support the expectation of high quality architecture and to ensure compatibility with the adjoining neighborhoods.
- 1.2 Ensure that new development complies with the maximum and minimum height limits and appropriate building height transitions. (Figures 26 & 27, and 28.)
- 1.3 Require that the redevelopment of the Oakville Triangle site fund the final design and implementation of the approved Plan for Mount Jefferson Park.
- 1.4 Require new development to provide ground level open space (publicly accessible where feasible) and rooftop amenity space within redevelopment sites as specified in Design Standards and Coordinated Development District (CDD) zoning for the Plan area.
- 1.5 Expand Ruby Tucker Park within the City's existing right of way on Lynhaven Drive.
- 1.6 The existing right-of-way located on Bellefonte Avenue and Route 1 will remain as right-of-way but will be used as open space and streetscape improvements. The City will explore acquiring the vacant property on Bellefonte Avenue, adjacent to Route 1, for open space.
- 1.7 Ensure high quality design and architecture and building materials. Encourage integration of the area's railroad and industrial heritage into new building, park and streetscape design. Encourage uses that will activate the streetscape.
- 1.8 Ensure that new buildings are designed as a collection of compatible but different buildings in scale, materials and architecture.
- 1.9 With redevelopment of sites on Route 1, require undergrounding of utilities and construction of a 25-foot streetscape. For the limited sites where this is potentially not feasible (see Figure 17), funding for these improvements will be part of the plan-wide implementation.
- 1.10 Concentrate taller building heights at the locations of transit stations at Swann Avenue/Route 1 and East Glebe Road/Route 1, subject to the standards for required height transitions to the adjoining neighborhoods.
- 1.11 A transportation network that includes a new street grid to distribute vehicular traffic, improve traffic flow, and improve pedestrian, bicycle and transit connectivity.
- 1.12 A new north-south road between Fannon Street and East Glebe Road (extension of Oakville Street).
- 1.13 A pedestrian and bike connection between Swann Avenue and Stewart Avenue to provide connectivity between the neighborhoods and the future Potomac Yard Metrorail station. ~~The bike lane within the central urban plaza within the Oakville Triangle site will be designed as a shared space.~~
- 1.14 An improved pedestrian network that includes safe and accessible sidewalks along all streets within the plan area that connect to parks, retail, transit and trails.
- 1.15 Improvements to the Route 1/East Glebe Road intersection.
- 1.16 A new signal at the intersection of Route 1 and Montrose Avenue to improve east-west connectivity, to be coordinated with traffic calming improvements along Montrose Avenue and operational improvements at the intersection of Montrose Avenue at East Glebe Road/Ashby Street.
- 1.17 ~~An additional signalized pedestrian crossing across Route 1 between East Custis Avenue and East Glebe Road to improve pedestrian access between Oakville Triangle and Del Ray/Lynhaven and the future Potomac Yard Metrorail station.~~ An additional signalized crossing across Route 1 at Fannon Street, including pedestrian and intersection improvements, to improve pedestrian access between Oakville Triangle and Del Ray/Lynhaven and the Potomac Yard Metrorail station and provide efficient access to the medical care facility.
- 1.18 An improved bicycle network that includes bike lanes along Swann Avenue to connect Potomac Yard, the Mt. Jefferson Trail, and the Del Ray neighborhood.

- 1.19 Bicycle parking and opportunities for bike sharing.
- 1.20 Enhanced parking management, including performance parking, smart parking technology, and shared parking.
- 1.21 Transportation Management Plan (TMP) strategies such as a TMP District, transit incentives, vanpool and carpool sharing, car share, electric vehicle charging stations, and TMP monitoring.
- 1.22 Require a mix of land uses as depicted in Figure 25 and taller first floors heights where required. Parking, loading, refuse, and noise will be addressed in the design of the new buildings as part of the development review process.
- 1.23 Consistent with the policies and goals of the Housing Master Plan, encourage the inclusion of on-site affordable rental and home-owner housing opportunities and voluntary contributions to the Affordable Housing Trust Fund with each redevelopment in the Plan area.
- 1.24 Ensure a minimum of 65 affordable units within the Oakville Triangle site.
- 1.25 Explore the provision of potential ARHA replacement units in the Plan area.
- 1.26 Encourage universal design to allow residents to age-in-place.
- 1.27 Encourage a variety of housing types within the Plan area.

GENERAL DESIGN GUIDELINES & STANDARDS

GENERAL STANDARDS

- 2.1 Projects are required to comply with all applicable Design Standards herein, and comply with the applicable approvals, zoning requirements, and existing City Plans and policies, such as the Complete Street Design Guidelines, Green Building Policy, Housing Master Plan, etc. These Standards and Guidelines ensure high quality design and architecture within the Plan Area. Standards shall require a higher level of review and the expectation is that new development will be required to comply with these Standards. Any deviation from the standards contained herein shall be evaluated and determined through the Development Special Use Permit (DSUP) process. Guidelines are advisory and new development is encouraged to incorporate them as appropriate.
- 2.2 Provide taller building height adjacent to the transit stops along Route 1 and in close proximity to the Potomac Yard Metrorail station.
- 2.3 Improve and enhance the Route 1 frontage by locating the utilities below grade and providing a 25 ft. setback-streetscape for the buildings adjacent to Route 1.

OPEN SPACE

- 2.4 Improve Mount Jefferson Park consistent with the Plan approved by the Parks and Recreation and Planning Commissions including the at-grade trail extension at the southern end of the Park.
- 2.5 Provide a *new* central public ~~urban plaza~~ *open space* within Oakville Triangle that includes usable amenities that help to meet the recreational needs of new residents.
- 2.6 Provide ~~five~~ *four* new ground level public green spaces adjacent to Mount Jefferson Park.
- 2.7 Expand the existing Ruby Tucker Park.

STREETS

- 2.8 Develop a generally orthogonal street grid pattern, including the introduction of several new streets.
- 2.9 Provide a hierarchy of streets that identify the character and function of each street.

- 2.10 Provide on-street parallel parking where feasible for the new streets (excluding Route 1).
- 2.11 New streets are designed to be low speed, local serving, pedestrian-oriented to encourage bicycle and transit use.
- 2.12 New streets should accommodate stormwater management.
- 2.13 Where possible, new streets should include safe, separate, lanes for bicycle traffic as recommended in the Transportation Master Plan.

PUBLIC REALM

- 2.14 Create pedestrian-oriented streetscapes.
- 2.15 Incorporate the provision of safe, efficient, and convenient pedestrian and bicycle circulation systems that connect neighborhoods, transit, and open spaces.
- 2.16 Provide careful attention to sidewalk design and detailing to support the walkability and sustainability of the Plan area.

BUILDINGS

- 2.17 Create an urban building scale and relationship between buildings, streets and open spaces that ensure and maximize walkability, create compact development and maximize the use of transit.
- 2.18 Create a variety of building massing (footprint and height) for the townhouses, multi-family, office and hotel uses.
- 2.19 Provide appropriate transitions to the existing single-family homes and townhouses to the west of the Plan area through scale transitions, setbacks and landscape buffers.
- 2.20 Provide high quality architecture and appropriate building materials, textures, façades, and treatments to establish a high quality urban environment.

PLAN FRAMEWORK

FRAMEWORK STREETS STANDARDS

- 4.1 The intersection of Glebe Road and Route 1 will need to be reconfigured as generally depicted within the attached cross-sections. ~~The timing and phasing of the improvements will be established as part of the CDD zoning for the Plan area.~~ Funding will be proposed as part of the Oakville Plan CDD for the improvements.
- 4.2 A hierarchy of streets, as depicted in Figure 20A, is required to maintain a high-quality street environment and address a variety of needs and functions.
- 4.3 The streets are required to be constructed in the locations generally depicted in Figure 19 and in the dimensions configured in the street cross sections required herein.
- 4.4 "A" Streets: Curb cuts, entrances to parking garages and service bays are prohibited.
 - i. Buildings shall front the street.
 - ii. Active uses shall be located on all street frontages for each level of the building.
 - iii. The higher quality of architectural façade and streetscape treatment shall be used within the street hierarchy.
- 4.5 "B" Streets
 - i. Buildings shall front the street.
 - ii. Active uses shall be located on all street frontages for each level of the building
 - iii. A maximum of one curb cut per block face shall be permitted on each side of the street. To the extent possible, curb cuts should be aligned with curb cuts on the opposite side of the street. Offset curbs may be permitted, where it is deemed necessary. Additional curb cuts may be considered as part of the DSUP process.

iv. Main building and pedestrian entrances shall be located along “B” street frontages unless adjacent to an “A” street. A high quality of architectural façade treatment is required.

4.6 “C” Streets

v. Curb cuts for internal alleys and service shall be located on these streets, unless it can be determined that it is infeasible to do so.

4.7 Curb cuts shall be limited to the minimum necessary in number and width.

4.8 Residential entrances are encouraged on all streets. Where ground floor retail is provided or required, residential lobby entrances should be minimized but are not prohibited. The final location of residential entrances where retail is provided will be determined as part of the development review process.

BIKE AND PEDESTRIAN NETWORK STANDARDS

4.9 The bike and pedestrian network as generally depicted in Figure 21 shall be implemented as part of the redevelopment within the Plan area.

4.10 A signalized pedestrian only crossing shall be provided as generally depicted in Figure 21.

4.11 The bike and pedestrian areas will be implemented consistent with the attached street cross-sections and the design of the Mount Jefferson Park Plan.

FUELING/CHARGING STATION GENERAL STANDARDS

4.12 Fueling/charging stations, if provided, shall be part of the redevelopment that implements the requirements of the Vision Plan and Design Standards and Guidelines.

4.13 Fueling/charging stations shall be integrated into the design of the building and not a secondary element.

4.14 If a canopy is provided, the design of the roof shall be considered from the adjoining building(s) and uses and shall include elements such as a green roof or comparable treatment. Canopy lighting shall be designed to minimize impact/visibility from adjoining neighborhoods.

4.15 Service components, such as payment, shall be integrated into the building to the maximum extent feasible.

4.16 The fueling/charging station will be subject to all applicable building, environmental, and zoning requirements and the CDD zoning(s) for the Plan area.

OPEN SPACE STANDARDS

4.17 Mount Jefferson Park shall be completed as part of the redevelopment of the Oakville Triangle site and shall be consistent with the park improvements approved by the Parks and Recreation and Planning Commissions.

4.18 The ~~five~~ *four* new open spaces adjacent to Mount Jefferson Park shall be constructed as generally depicted in Figure 30, as part of the redevelopment adjacent to the Mount Jefferson Park. The at-grade trail (lower trail) extension on the northern portion constructed by the adjoining property owner.

4.19 The central open space within Oakville Triangle shall be ~~hardscape with~~ *have* appropriate plantings, shade options, and lighting and shall contain high-quality materials and finishes, as well as the inclusion of public art or other focal features. The open space/plaza shall be accessible to the public through the provision of a public access easement.

4.20 The Ruby Tucker Park shall be expanded by ± 4,000 sq. ft by utilizing the existing Lynhaven Drive right-of-way.

4.21 New townhouse developments shall provide a minimum of 15% ground level open space. Roof-top amenity space is strongly encouraged.

- 4.22 New multifamily developments shall provide a minimum of 25% ground level open space and 15% roof top amenity space.
- 4.23 New mixed-use projects (with ground floor retail-commercial) shall provide a minimum of ~~15%~~ 20% ground level open space ~~and 25% roof top amenity space~~ with a total of 40% on-site open space to include rooftop-amenity space as part of redevelopment.
- 4.24 For office and hotel roof top open spaces or courtyards are encouraged.
- 4.25 Spaces shall be designed for their intended function; for example, plazas should be designed with adequate amounts of hardscape, electrical and water connections to accommodate public gatherings; large greens or parks should minimize hardscape areas that will detract from their intended appearance as a green oasis dominated by native vegetation, some lawn areas, and trees. Pedestrians-only and shared pedestrian/vehicular areas shall be designed to withstand the intended loading on paved or green surfaces.
- 4.26 With the exception of designated urban plaza on Swann Avenue, the Plan's open space shall incorporate significant green and pervious elements, offer shade relief and contribute to the City's tree canopy goals where possible.
- 4.27 Spaces should be designed with consideration for climate and sun exposure during different seasons of the year. Opportunities for shade or sun, with water elements to offer a sensory change and softening of urban noise and wind protection, should be considered where appropriate.
- 4.28 Materials shall be selected that are durable and appropriate for the scale and context of the Plan area. Materials should be typical of the types used in the construction of urban spaces. Although materials must be suitable for significant pedestrian use, their quality and appearance shall reflect their importance as open space within the public realm.
- 4.29 Garden screen walls and/or retaining walls should be constructed of brick, stone, architectural precast or other highly finished appropriate material.
- 4.30 Pavement in open space shall be brick, stone, concrete pavers, or concrete. Large expanses of concrete without details, scoring patterns, or brick/stone banding are prohibited.
- 4.31 Children of all ages should have easy access to appropriately located, designed, and landscaped outdoor play areas suited to their development and play needs.
- 4.32 Landscapes shall be designed with sustainable plant selections that are horticulturally acclimatized to the Mid-Atlantic and DC National Capital Region, that require minimal maintenance and non-organic treatment, that utilize manipulation of rainwater for natural irrigation, and that provide natural pest control.
- 4.33 Rooftop amenity space areas on buildings in close proximity to the adjoining neighborhoods will need to be designed in a compatible manner to prevent adverse affects of noise and light.
- 4.34 As part of the new multi-family, office, medical care facility, or hotel buildings, explore providing a community meeting space within each building.

GATEWAY ELEMENTS/SIGNATURE FACADES STANDARDS

- 4.35 Gateway elements and signature facades shall be provided at locations as depicted in Figure 31.
- 4.36 Gateway elements and signature facades shall be proportional to the size and scale of the building.
- 4.37 Gateway element(s) shall provide distinctive three-dimensional forms, and unique shapes and materials to reinforce the significance of each location. Architectural features, such as towers, cupolas and lanterns should be used to address highly visible corners or terminated vistas. Gateway elements should provide special elements at street terminations to frame views.

- 4.38 Signature façades shall be designed to reflect their visually prominent location.
- 4.39 The gateway element will vary in height from the primary height of the building by being lower or slightly taller as permitted herein, or through the architectural treatment of the gateway element.

URBAN DESIGN CHARACTER

BUILDING STREETWALL STANDARDS

- 5.1 The streetwall height for each street frontage shall be a minimum of 35 feet on Route 1 and 25 feet for other street frontages, which shall be located parallel to the adjoining street or open space. In addition to the streetwall requirements, buildings are also subject to the height requirements herein. Where the building streetwall is taller than the minimum, pedestrian elements such as cornice and color shall be provided to ensure pedestrian scale buildings.
- 5.2 The streetwall shall generally be parallel to the street and at a minimum height as required herein. The streetwall for each block frontage shall be a minimum of 75% for residential, office, *medical care facility*, and hotel. For residential buildings where courtyards are provided, the streetwall shall be a minimum of 50%. A courtyard shall be limited to a maximum of two block frontage. Ground floor retail and maker uses shall provide a minimum of 85% continual streetwall.
- 5.3 The streetwall shall be setback a maximum of 30 feet from the property line. This shall be in conjunction with the massing requirements herein.
- 5.4 While a generally continual streetwall is required for each building, the streetwall shall incorporate articulation to ensure variety of the building as required herein.

BUILDING HEIGHTS - VARIETY STANDARDS

- 5.5 Each multi-family building (excluding 2/2 stacked townhouses) shall provide a minimum of 25% of the building footprint below the maximum provided height (Figure 33). The specific allocation of the 25% shall be determined as part of the development review process. Building Breaks provided shall count toward the minimum requirement of this section. Office and hotel building shall provide a variety of height which shall be determined through the development review process.
- 5.6 For townhouses and 2/2 stacked townhouses a variety of heights shall be provided within each row of townhouses. The difference in height shall be a minimum of one level (story) variation between the townhouses. This can be achieved through variation in roof form, setbacks and height and the amount of variation shall be determined as part of the development review process.
- 5.7 Some buildings, at locations as depicted in Figure 31, where gateway elements are required may be permitted to exceed the maximum height by one full level (story), in an amount not to exceed 3,000 sq. ft. The locations shall be limited to locations depicted in Figure 31 - Gateway Elements.

BUILDING MASSING STANDARDS

- 5.8a At least 25% of the total multi-family building perimeter must be setback between 8-10 feet at the building face on at least 2 facades. Steppbacks may occur at the ground floor or above the streetwall where retail and/or maker space is provided on the ground floor.
- b For office and hotels, at least 15% of the total building perimeter must be setback between 2-8 feet at the building face on at least 2 facades. Steppbacks may occur at the ground floor above the streetwall where retail and/or maker space is provided on the ground floor.
- 5.9 Where bays, pavilions, recesses, hyphens and screens or compatible elements are provided they shall comply with Figure 35.

BUILDING TRANSITION STANDARDS

I. TOWNHOUSES

- 5.10 A - Where townhouses in the Plan area are adjacent to existing homes, the new townhouse shall a maximum of one additional level or no more than 14 feet taller than the existing homes whichever is less. More than one unit of the townhouses may be required to comply with this requirement if deemed necessary as part of the development review process.
- 5.11 B - Townhouses shall provide a side yard setback of a minimum of 15 feet or a 1:3 floor to height ratio from the property line, whichever is greater, adjacent to the existing single-family homes or townhouses or buildings adjoining the Plan area. This area shall be landscaped.
- 5.12 C - Where new townhouses are constructed on new streets, they shall generally be oriented to face the existing streets, where feasible. Front yard setbacks for new townhouses on existing streets shall be compatible with the existing neighborhood. For new streets, the setback will be consistent with the attached cross-sections or a minimum of 4 ft, whichever is greater.
- 5.13 D - For four level townhouses adjacent to existing homes, the fourth level shall be setback a minimum of 15 ft. The third level shall generally be oriented to the existing homes.
- 5.14 F - In the rear, a setback-landscaped buffer of a minimum 8 feet shall be provided adjacent to the single-family homes and townhouses. Where feasible a larger rear setback shall be provided. A fence or wall may be required within the landscape buffer or setback as part of the development review process.

II. MULTI-FAMILY (BUILDING TRANSITIONS)

- 5.15 A - Each multi-family building or office building shall provide a building shoulder as generally depicted in Figure 37. The shoulder shall provide a minimum setback of 15 ft. The building shoulder shall be no taller than 15 feet taller than the existing buildings-homes or a maximum of 45 feet whichever is less. The length, width, and depth of the building shoulder shall be determined as part of the development review process.
- 5.16 B - In addition to the building shoulder required herein, a landscaped setback a minimum 15 ft. shall be provided. Where an additional setback is feasible it shall be provided, as determined through the development review process. A fence or wall may be required within the landscape buffer or setback as part of the development review process.
- 5.17 To the extent feasible, the setback of the multi-family buildings (excluding Route 1) on existing streets shall be compatible with the existing neighborhood.

BUILDING TYPE STANDARDS

I. TOWNHOUSES

1. Parking

- 5.18 The parking for each townhouse shall be provided from a rear alley. Front loaded townhouses are prohibited. Detached parking garages are encouraged.
- 5.19 Permanent surface parking lots are prohibited.

2. Streetscape Level – First Level

- 5.20 The first level shall be designed with the highest quality material and detailing.
- 5.21 Each unit shall be subject to the residential uses at grade requirements as required herein.

- 5.22 Building designs shall incorporate modulation and articulation such as massing reveals, changes of textures, materials, and/or colors, or shifts of the façade plane in order to create a pedestrian scaled façade. Shifts in the footprints shall be a minimum of 2-5 ft. to provide variety in the façade plane.
- 5.23 Each ground floor residential unit must have an individual entry door directly from the adjoining street. Mews units and configuration is prohibited. Where stoops are required or provided, they shall be oriented parallel to the adjoining sidewalk.
- 5.24 All units must include a minimum depth of 25 feet of occupied habitable space for each level unless less depth is required for variation in the streetwall.
- 5.25 A minimum of 50% of each group of townhouses for each project shall provide a front porch. The porches shall comply with the applicable requirements herein. *Final location of porches will be determined during the DSUP process.*
- 5.26 Townhouses shall provide a 2 to 10 ft. setback from the required sidewalk to provide space for individual front yards, plantings, porches, stoops and similar elements.
- 5.27 For each grouping of liner townhouses exceeding 120 feet in length, a setback of a minimum of 8 feet shall be provided or a building break, between the adjacent units. The final width shall be determined as part of the development review process.
- 5.28 For freestanding townhouses, a building break shall be provided to ensure that groupings of townhouses do not exceed 120 feet in length.

3. Building Character and Materials

- 5.29 Upper floor exterior terraces or balconies are permitted at the rear facade of the building. These may also be permitted on the front facade of a building at the discretion of the Director of Planning and Zoning as part of the Development Review Process.
- 5.30 Units shall be architecturally differentiated through the use of color and materials within each block. This is not intended to require variety for each unit, but rather within each group of townhouses.
- 5.31 All building materials shall be used to express their specific purpose and express the tectonic nature of the materials; for example, heavier materials support lighter materials.
- 5.32 Building materials for each façade shall consist of the following:
 - Brick, glass, stone, wood, precast and/or metal
 - Stone, cast stone, metal or similar durable materials for trim
 - Fiber cement siding and panels (or comparable) may be provided at limited locations.
 - Prohibited materials include synthetic stucco, regular ground or split face CMU, and any masonry units with an expressed size of 8" x 16" and vinyl siding and any material not outlined above.
- 5.33 Sides and rears of buildings visible from a street or park shall use the same architectural treatment as the primary façade.
- 5.34 Blank façades shall be prohibited for street or park frontages.

4. Building Massing

- 5.35 Each townhouse shall comply with the maximum height (Figure 27), minimum height (Figure 28), and the required height transitions (Figures 26, 36, 37).
- 5.36 To comply with the applicable height requirements, the roof may be a flat or gable roof.

5. Building Fenestration

The size, frequency, and location of windows will be one of the primary visual characteristics of each building. All townhouses are subject to the following:

- 5.37 At least 25% of the each facade adjacent to a street or open space shall be devoted to transparent windows and doors to allow maximum visual interaction between sidewalk areas and the unit. The use of dark or mirrored glass is not permitted. A higher percentage is encouraged where feasible
- 5.38 Buildings shall generally provide a vertical fenestration pattern.
- 5.39 Mirrored reflective, frosted reflective or darkly tinted glass is prohibited.
- 5.40 Windows shall be used as an element that helps to articulate the character of a façade, and designed to reveal the thickness/depth of the façade wall. Windows shall be well-proportioned and operable.

II. MULTI-FAMILY BUILDINGS

1. Parking

- 5.41 Parking for each building shall be located entirely below grade or entirely screened with an active use. The screening of the parking with active uses shall be provided for each level of the entire perimeter of each street and/or park, courtyard, and/or open space frontage or where visible from an adjoining street or open space.
- 5.42 Permanent surface parking lot(s) are prohibited.

2. Streetscape Level – First Level

- 5.43 Active uses shall be provided for a minimum depth of 25 ft. for each street frontage and all street, open space, courtyard, and park frontages for each level and the entire length of the building frontage excluding necessary curb cuts and loading areas.
- 5.44 Building designs shall incorporate modulation and articulation such as massing reveals, changes of textures, materials, and/or colors, or shifts of the facade plane in order to create a pedestrian scaled façade
- 5.45 Unless required for the function of the building, blank walls in excess of 30 ft. in length or height are prohibited.
- 5.46 Individual and functional entries and “townhouse-scale” elements are required for the multi-family buildings at 20 to 30 ft. intervals, where ground floor commercial, retail uses or maker spaces are not provided where stoops are required or provided, they shall be oriented parallel to the adjoining sidewalk.

3. Building Break

- 5.47 A building break shall be provided for larger multi-family buildings such that the longest expressed element does not exceed 200 feet in length. The building break shall be a minimum of 30 feet in width.
 - a. Where retail-commercial use is provided or required on the ground floor the building break is required above the first floor retail-commercial use.
 - b. There may be a connector between the building break.
 - c. As part of the development review process, a building break may not be required if a level of façade variation is provided comparable to the building break required above. In addition, if a building break is not required, the façade variation shall include variation in color and materials.

- d. In the event that the predominant portion of the building is setback a minimum of 40 feet, a building break is not required. However the expression of a building break is required at a distance not to exceed 200 feet in length, which shall be expressed through a building recess of a minimum depth of 10 feet and through the use of materials and color. The width of the recess shall be determined as part of the development review process.

4. Building Character and Materials

- 5.48 Buildings shall be architecturally differentiated through the use of color and materials within each block.
- 5.49 All building materials shall be used to express their specific purpose and express the tectonic nature of the materials; for example, heavier materials support lighter materials.
- 5.50 Building materials for each façade shall consist of the following:
- Brick, glass, stone, wood, precast ceramic panels and/or metal
 - Stone, cast stone, metal or similar durable materials for trim
 - Fiber cement board and/or siding and/or panels (or comparable) shall be limited to a maximum of 20% of the materials used on the building facade visible from a street or park/open space.
 - Prohibited materials include synthetic stucco, regular ground or split face CMU, and any masonry units with an expressed size of 8" x 16" and vinyl siding.
- 5.51 HVAC and mechanical equipment shall be integrated into the overall building design and not be visible from an adjoining street and/or park. Wall units or vents shall be prohibited, unless recessed within a balcony or shall be integrated with the design of the building.
- 5.52 Sides and rears of buildings that are visible from an adjoining street and/or park shall be designed in a compatible manner utilizing a similar architectural treatment as the primary façade. Blank façades shall be prohibited for any street frontage.
- 5.53 The solid to void ratio shall consist of a minimum of 30% void for each building which shall exclude ground floor commercial-retail areas where provided.
- 5.54 Buildings shall generally provide a vertical fenestration pattern.
- 5.55 Mirrored reflective, frosted reflective or darkly tinted glass is prohibited.
- 5.56 Windows shall be used as an element that helps to articulate the character of a façade, and designed to reveal the thickness/depth of the façade wall.
- 5.57 Windows shall be well-proportioned and operable, if feasible.
- 5.58 Windows shall be grouped to establish rhythms across the façade and hierarchies at important places on the façade.

5. Building Fenestration

- 5.59 Window and door placement shall provide a high degree of transparency at the lower levels of the building, maximize visibility of pedestrian active uses, provide a human-scaled architectural pattern along the street and establish a pattern of individual windows and exterior openings within building façades that provides a greater variety of scale through material variation, detail and surface relief.

III. OFFICE AND HOTEL BUILDINGS

1. Parking

5.60 Parking for each building shall be located entirely below grade or entirely screened with an active use. The screening of the parking with active uses shall be provided for each level of the entire perimeter of each street and/or park, courtyard, and/or open space frontage or where visible from an adjoining street or open space.

5.61 Permanent surface parking lot(s) are prohibited.

2. Streetscape Level – First Level

5.62 Window and door placement shall provide a high degree of transparency at the lower levels of the building, maximize visibility of pedestrian active uses, provide a human-scaled architectural pattern along the street and establish a pattern of individual windows and exterior openings within building façades that provides a greater variety of scale through material variation, detail and surface relief.

3. Building Character and Materials

5.63 Buildings shall be architecturally differentiated through the use of color and materials within each block.

5.64 All building materials shall be used to express their specific purpose and express the tectonic nature of the materials; for example, heavier materials support lighter materials.

5.65 Building materials for each façade shall consist of the following:

- Brick, glass, stone, wood, precast and/or metal
- Stone, cast stone, metal or similar durable materials for trim
- Prohibited materials include synthetic stucco, regular ground or split face CMU, and any masonry units with an expressed size of 8" x 16"

5.66 Sides and rears of buildings that are visible from an adjoining street and/or park shall be designed in a compatible manner utilizing a similar architectural treatment as the primary façade. Blank façades shall be prohibited for any frontage.

5.67 The solid to void ratio shall consist of a minimum of 30% void for hotel buildings and 35% void for office buildings and may include spandrels.

5.68 A minimum of 50% of the linear ground floor retail façade (as measured from floor-to-floor) and any second floor retail façade shall be constructed of transparent materials.

5.69 Buildings shall generally provide a vertical fenestration pattern.

5.70 Mirrored reflective, frosted reflective or darkly tinted glass is prohibited.

5.71 Windows shall be used as an element that helps to articulate the character of a façade, and designed to reveal the thickness/depth of the façade wall.

5.72 Windows shall be well-proportioned and operable, if feasible.

5.73 Windows shall be grouped to establish rhythms across the façade and hierarchies at important places on the façade.

5.74 Strip and/or ribbon windows shall be prohibited on all building façades.

PORCH STANDARDS

The porches for the townhouses as required herein will project from the primary façade and generally have a small-to-medium setback from the sidewalk. The projecting porch shall be open on three sides.

5.75 Front porches shall be provided for a minimum of 50% of all new townhouses constructed as part of each project. Front porches shall remain open, not closed.

- 5.76 Where porches are provided for multi-family buildings, the porches shall comply with the applicable requirements herein. Front porches shall remain open, not closed.
- 5.77 If provided, second floor balconies shall have a minimum depth of three feet and a minimum underside clearance of 8 feet. Exceptions shall include Juliette balconies.
- 5.78 Although not required for multi-family buildings, porches or stoops are encouraged.
- 5.79 The material of the porch shall be compatible with the design of the building. Materials shall be wood and/or metal.
- 5.80 New porches shall comply with the requirements of Figure 39A.

BUILDING ENTRY STANDARDS

- 5.81 Building entrances shall be given prominence on the street frontage, sized and scaled appropriately for the scale of the building and have a change in material, wall plane, and/or color.
- 5.82 The primary pedestrian entrance shall front the adjoining street.
- 5.83 Enhanced level of architectural design and treatment are required, and, where appropriate, landscape treatment should emphasize the primary entrance.
- 5.84 Differentiate architecturally between residential and commercial entrances in mixed-use buildings.
- 5.85 Entries shall provide protection from the elements, with canopies, recesses, or roof overhangs to reinforce the pedestrian scale.

- 5.86 Unless ground floor retail is provided, buildings that have frontage on more than one street shall provide their primary entry based upon street hierarchy (ex: primary entry provided on "A" street vs. "B" street) and secondary entrances on the remaining streets. Corner entrances are permitted.
- 5.87 For required retail frontages, the width of residential and/or office lobbies shall be the minimum necessary.
- 5.88 Encourage the provision of entrances to retail, residential and other active ground level uses generally every 20 to 75 ft. along the street frontage.
- 5.89 Explore the provision of as many entries as possible at the street frontages.
- 5.90 For residential frontages, the frequency of the entries must relate to the size of the unit fronting the street, and shall occur on average every 20 to 30 feet along public rights-of-way. Two entries may be grouped together.
- 5.91 Handrails for all building types shall not be permitted within the required/provided sidewalk.

BUILDING ROOF STANDARDS

- 5.92 Rooftop equipment (including elevator equipment, HVAC equipment, etc.) shall be concealed in penthouse structures and designed as an integral part of the building and/or screened with a parapet. Mechanical penthouses and roof top equipment shall be designed as an extension of the building, employing building materials and design treatments consistent with the exterior façades of the building when visible from a public street or open space.
- 5.93 To the extent where visible from the street, roof penetrations such as vents, attic ventilators, flues, etc. shall be placed to limit their visibility from the street and designed in material and color to match the color of the roof, except those made of metal, which may be left natural.
- 5.94 Sloped roofs shall be metal, slate, tile, or other comparable high quality material.

WALLS/FENCES STANDARDS

- 5.95 The height, length, and visual impact of walls and fences shall be pedestrian scale and in no case shall exceed 3 ft. in height in the front or side yards. In the rear yards, 6 ft. fences may be provided, if approved as part of the development review process.
- 5.96 Materials for walls shall be brick and/or stone. Garden screen wall and/or retaining walls should be constructed of brick, stone, architectural precast or other highly finished appropriate material
- 5.97 Materials for fences shall be decorative metal or wood.
- 5.98 Greenwalls and living walls are strongly encouraged.

PUBLIC REALM - STREETScape STANDARDS**1. Sidewalks**

- 5.99 Refer to attached street cross-sections in Section 8.

2. Street Trees

- 5.100 Refer to Complete Streets Guidelines.

3. Street Furniture

- 5.101 Each project shall provide street and on-site furniture and amenities for public use. Street furniture shall include benches, bicycle racks, and trash receptacles.

Benches

- 5.102 Benches shall be located on public streets and shall be the Victor Stanley Classic Series CR-96 or similar as approved by the City of Alexandria. A minimum of two benches shall be provided for each block in appropriate locations based on the specific ground floor use and the location of bus stops and public open space.

Bike racks

- 5.103 To encourage and facilitate biking as a means of transportation, bike racks that conform to the City's bike rack standards shall be provided and placed in groups at convenient, safe, well lit paved areas in the building or curb zone. Bike racks shall also be provided in parking garages and at appropriate park amenities.

Trash/Recycling Receptacles

- 5.104 The trash receptacle to be used throughout the area is the Iron Site Bethesda Series Receptacle with domed lid (model SD-42) by Victor Stanley with black, powdercoat finish (or equal as approved by the City of Alexandria). Trash receptacles shall also include accommodations for recycling. One trash receptacle shall be located at each intersection.

4. Lighting

- 5.105 Street light fixtures shall be single black Dominion Virginia Power acorn lighting fixtures for all streets (except Route 1) with a standard black finish.
- 5.106 The street light fixtures on Route 1 shall be double acorn with a standard black finish.
- 5.107 All street lights shall be placed to avoid conflict with street trees, and shall not be located within the sidewalks but rather shall be placed between the street trees.
- 5.108 All street lights shall be designed to minimize light spillover. Where located next to residential uses, street lights should include shielded as needed to prevent lighting from directly entering residential windows or adjoining public parks.

REAR OR BUILDINGS - ALLEYS STANDARDS

- 5.109 Use same material on rear facades as the front and side of townhouses.
- 5.110 Paving material should be designed for durability. Change paving materials, colors in alleys to minimize visual expanse the asphalt paving of the alley

- 5.111 Add elements such as porches and bays where feasible to soften the rear facades and alleys.
- 5.112 Add landscaping and trees to minimize the visual impact to the adjoining homes.

GROUND FLOOR USES

RESIDENTIAL USES AT GRADE STANDARDS

- 6.1 Residential buildings shall provide a front setback of 2-10 feet from the required sidewalk to provide space for individual front yards, plantings, landscaping, fences, stoops, and similar elements.
- 6.2 Ground floor levels for all residential uses shall be elevated a minimum of 12 inches and maximum of 4 feet above the adjoining sidewalk. 2-3 feet is desired.
- 6.3 For multi-family buildings (where ground floor commercial space is not provided) individual and functional entries shall be provided at 20-30 ft. intervals.
- 6.4 Where at-grade accessible units are appropriate, alternatives shall be considered to the satisfaction of the Director of Planning and Zoning.

RETAIL USE AND RETAIL STOREFRONT STANDARDS

- 6.5 The retail frontages shall be designed to create a comfortable yet highly animated pedestrian environment utilizing a rhythm of multiple retail frontages architecturally articulated through materials, colors, numerous entrances, display windows, canopies and signage.
- 6.6 Building materials shall be high-quality and contribute to a human-scaled public realm. Blank walls shall be prohibited.
- 6.7 To establish pedestrian-scaled design on the ground floors of larger buildings, use window groupings, material changes, or columns on the principal façade to accentuate individual storefronts and denote a smaller increment of building bays.

- 6.8 For retail, generally provide transparent windows for a minimum of 70% of the retail area. Flexibility may be considered based on creativity and the overall compatibility and character of the storefront design, meets the intent of the Standards, and is approved by the Director of P&Z.
- 6.9 Corner retail storefronts shall extend at least 40 ft. along the side street and/or park-open space, and shall also be expressed in the architecture.
- 6.10 The design of the storefront shall be appropriate to the scale and architectural design of the building.
- 6.11 The design and construction materials of the ground-level storefronts shall be appropriate for a retail street, to help contribute to an active pedestrian-oriented street. These shall include: how the storefront fits into the architecture of the buildings; relationship to varying grades along the storefronts, and the flexibility to adjust store entries; visibility of storefronts (including clear glass); sidewalk spaces for outdoor retail displays or dining; sign and logo requirements; and the design, materials and colors of awnings or canopies to protect pedestrians and windows.
- 6.12 The materials for the retail storefront shall consist of stone, metal, glass and/or wood. Construction detail and finish shall adhere to craftsman's standards. Durable materials such as these are especially critical at the street level where pedestrian contact will be considerable. Storefronts should be predominantly glass to provide views into the store. Translucent composite materials may be acceptable and reviewed as part of the development review process.
- 6.13 The design of the retail storefronts shall be administratively approved subject to the standards required herein.

MAKER SPACE STANDARDS

- 6.14 Each maker space shall provide a minimum of 40% transparency (garage doors, doors and windows) at the street level.
- 6.15 A garage door or comparable sized opening shall be provided for each space or approximately every 20-30 feet.

- 6.16 Garage and/or roll up doors shall be glass and metal.
- 6.17 Signage shall comply with the applicable signage provisions herein.
- 6.18 The uses shall be subject to all applicable requirements of the CDD zoning and associated requirements.
- 6.19 The floor to ceiling height shall be a minimum of 15 ft., with 18 ft. preferable. The minimum depth of each space shall be a minimum of 35 feet.
- 6.20 Adequate loading, access, refuse collection, and noise attenuation shall be addressed during the development review process.
- 6.21 Flexibility may be granted for exhaust, fans, and vents on primary building facades that support the building function/use. Final location and treatment will be determined as part of the development review process.
- 7.3 Hotel uses shall be permitted a maximum sign area of 2.5 sq. ft. per linear foot of building frontage not to exceed 75 sq. ft. For purposes of calculating sign area, building frontage shall be limited to frontage on a public street. Hotels located on a corner cannot use total allowed sign area on one frontage.
- 7.4 Retail, residential, hotel and office signs shall be limited to a maximum height of 20 ft. above the grade of the adjoining sidewalk. The Director of P&Z may permit hotel signage above 20 ft. above the grade of the adjoining sidewalk provided that the illumination does not have an adverse impact on adjoining residential uses or. However, in no case shall signage exceed 50 ft. above the grade of the adjoining sidewalk. The intent is to minimize visibility from adjoining neighborhoods and Mount Jefferson Park.
- 7.5 Awnings shall be permitted to project up to 4 ft. from the building; greater projections require approval of the Director of P&Z. Greater projections which encroach into the public street may require City Council approval of an encroachment.

SIGNAGE

RETAIL USE AND MAKER USE SIGNAGE STANDARDS

- 7.1 Each retail tenant shall install a minimum of one sign for each retail street frontage appropriate to the scale of each façade. In addition, each retail tenant shall provide a second pedestrian-oriented sign such as a projecting sign, blade or window sign for each street frontage.
- 7.2 Retail tenants shall be allowed a maximum of 1 sq. ft. of sign area per linear foot of tenant storefront and a maximum of 50 sq. ft. of signage. The Director of P&Z may approve signage for retail uses up to 2 sq. ft. per linear foot of frontage for exceptional design.
- 7.6 Projecting signs are required for each retail use and shall be appropriately sized and proportional to the building and/or storefront. Signs projecting over the sidewalk shall be a minimum of 8 ft. above the sidewalk. Projecting signs may be internally illuminated if approved by the Director of P&Z. Externally illuminated projecting signs shall have lighting fixtures that are complementary and integrated into the storefront design.
- 7.7 Retail tenants may incorporate window graphics; however, at no time shall the window graphics exceed 20% of the window area. The Director of P&Z may approve a maximum up to 40% if the design is consistent with the intent of the Standards.
- 7.8 Signage shall be located to not obscure architectural design elements such as projections, cornices, or change of building material or pattern.
- 7.9 Each maker tenant shall install a minimum of one sign for each street frontage appropriate to the scale of each façade.

7.10 A-Frame and similar signage shall comply with the findings of the Ad hoc Workgroup on A-Frame Signs and applicable City requirements and policies.

AWNINGS SIGNS AND BANNERS (RETAIL AND MAKER SPACES)

- 7.11 Awnings, when projecting from the building face, shall allow a clearance of 9 ft. from the grade of the adjoining sidewalk.
- 7.12 Fixed lightweight metal and glass structures are acceptable.
- 7.13 Awning or canopy material shall be a woven fabric or other material that conveys the aesthetic of the natural material of canvas, metal, glass etc.

RESIDENTIAL (MULTI-FAMILY BUILDINGS)

7.14 Signage shall be located to not obscure architectural design elements such as projections, cornices, or change of building material or pattern. Signs shall be limited to a maximum height of 20 feet above the grade of the adjoining sidewalk. Signage taller than 20 feet may be permitted if approved by the Planning Commission and City Council as part of a Coordinated Sign Special Use Permit. The size of the signs shall be limited to 50 sq. ft. The signage shall be located to minimize impacts on adjoining neighborhoods and parks.

MATERIALS – CONSTRUCTION (ALL SIGNS)

- 7.15 Signs shall be in the form of a painted dimensional sign, flat sign, blade sign, illuminated sign, fabricated dimension sign or awnings.
- 7.16 Materials shall be durable natural materials such as cast, polished or painted metal; glazed and ceramic tile; etched, cut or stained glass; cast stone and carved natural stone.

7.17 All methods of attachment including fasteners, mounting brackets and other mechanisms must be concealed from view.

- 7.18 Letters and graphics mounted directly onto building façades shall be pin mounted at least 2 inches from the surface onto which the sign is mounted and provide dimensional returns.
- 7.19 Neon signs, signs painted directly on building storefronts, and wall murals may be considered based on creativity and the overall compatibility and character of the tenant storefront design, if approved by the Director of P&Z, and meets the intent of the Standards.

ILLUMINATION- LIGHTING

- 7.20 Back-lit, halo-lit illumination or reverse channel letters with halo illumination.
- 7.21 All illuminated signs and exterior lighting shall be controlled by a time clock, which shall coincide with the normal business hours.
- 7.22 Blade signs shall generally be externally illuminated with decorative bracketed lighting. Internally illuminated blade signs may be considered based on design, if approved by the Director of P&Z and meets the intent of the Standards.
- 7.23 In general, lighting should be designed and located to accommodate public safety without creating glare or excessively high light levels. Fixtures should be chosen to control light trespass either vertically (toward the sky) or horizontally onto neighboring properties.
- 7.24 High pressure sodium vapor (yellow orange) lighting is prohibited for exterior use including buildings, parking facilities, service areas, signage, etc. Such lighting is also prohibited inside parking garages or building entries where it would be visible from the outside.
- 7.25 For any building or project, exterior light fixtures – their design, size, finish, location, etc. - should be compatible with, and appropriate for, the building architecture, materials and colors.

PARKING SIGNS

7.26 All parking signage shall comply with the City Wayfinding Guidelines and shall include garage identification and directional signs.

PROHIBITED SIGNS – FOR ALL USES AND BUILDING TYPES

7.27 Box signs and signs employing flickering rotating or moving lights shall be prohibited.

7.28 Panel box signs shall be prohibited. External raceways are discouraged.

7.29 Storage cabinets, carts, window signs, posters, shelving, boxes, coat racks, storage bins, closets, and similar items shall not block the visibility of the interior of the store from the street. This condition, however, is not intended to prevent retailers from displaying their goods in display cases that are oriented towards the street frontage.

7.30 All window coverings shall be open as much as possible and provide some interior accent lighting when the business is closed.

7.31 All banners relating to commercial promotions, leasing, hiring or advertising shall be prohibited.

7.32 Vinyl or plastic awnings, translucent acrylic or comparable shall be prohibited.

7.33 Building signage on the western portions of the proposed buildings or visible from Del Ray and/or Lynhaven or Mount Jefferson Park shall be prohibited.

PROCESSING – REVIEW

7.34 Each retail, multi-family, office and hotel tenant proceeding with permitting and/or fabrication shall submit detailed drawings and samples to be approved by the Department of P&Z.

7.35 Each sign(s) shall require a separate sign permit.

7.36 For larger/more prominent signs, the Director of P&Z may require a full-size mock up (constructed from foam core or illustration board) and/or a photomontage image.

WAYFINDING SIGNAGE

7.37 All new development sites shall provide wayfinding signage consistent with the City's Wayfinding Design Guidelines Manual.

STREETS**STREET CROSS-SECTION STANDARDS**

8.1 All new streets within the Plan area shall be consistent with the attached street cross-sections.

8.2 As part of the development review process, all utilities for new and existing streets shall be located below grade. Accommodation for transformers and vaults shall be identified as during the preliminary development review process.

8.3 All streets within the Plan are intended to be public streets, dedicated to the City or private with public access easement ~~unless otherwise noted the property line is assumed to be at the edge of the right-of-way (ROW) as defined herein.~~

A PPENDIX



A P P E N D I X

1. PLAN AREA CONTEXT AND HISTORY

- a. Plan Boundary Map
- b. Historic Maps and Historic Photos
- c. Existing Zoning Map
- d. Variety of Ownership Map
- e. Potomac Yard Approved and As-Built Heights
- e. Excerpts from Potomac West SAP 1992
- f. Approvals and Changes to the Potomac West SAP and Potomac Yard SAP since 1992

2. WHY A NEW PLAN?

- a. Intro
- b. Advisory Group and Community Planning Process
- c. Existing City Plans and Policies

3. DEMOGRAPHICS AND STUDENT GENERATION

- a. Demographic Information
- b. Student Generation
- c. Redistricting Program
- d. Long-Range Educational Facilities Plan
- e. Capital Improvements Planning
- f. Additional School Facilities

4. OPEN SPACE & PARKS

- a. Open Space Context Map
- b. Existing Conditions Mapping

5. SOLAR STUDIES

6. PLAN AREA BUILDING HEIGHTS & TOPOGRAPHY STUDY

7. MULTIMODAL TRANSPORTATION STUDY

- a. Multimodal Transportation Study March 2015
- b. Multimodal Transportation Study Appendices March 2015

8. RETAIL MARKET ANALYSIS

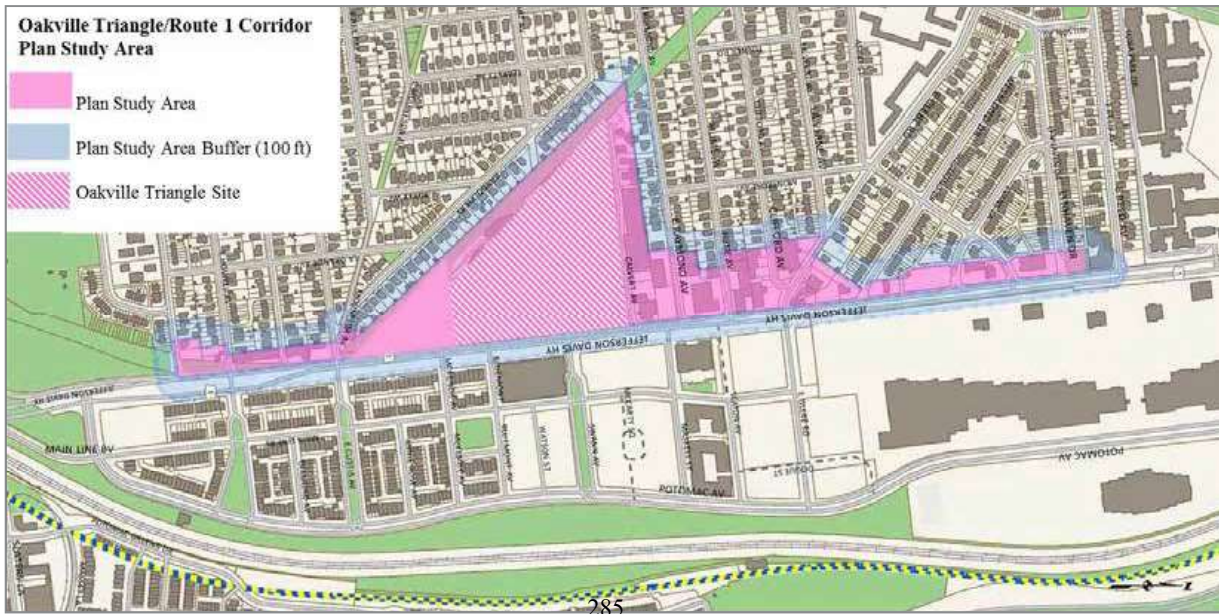
PLAN AREA CONTEXT AND HISTORY

A. OAKVILLE TRIANGLE/ROUTE 1 PLAN STUDY AREA MAP

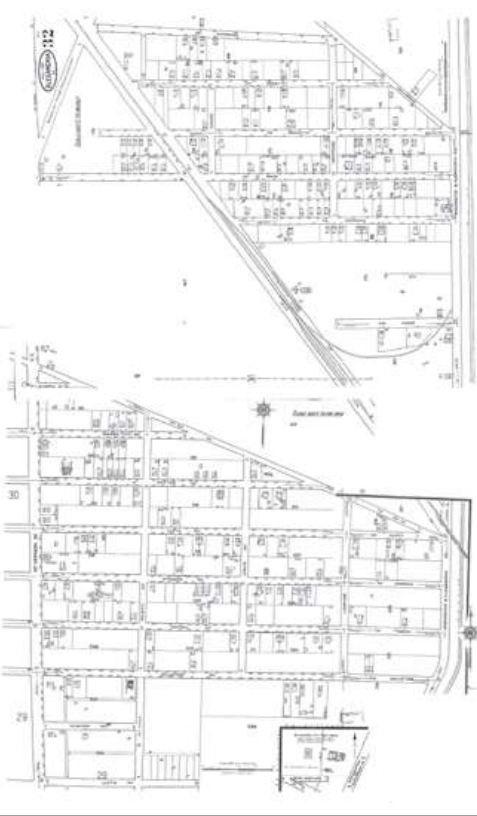
As shown on the Plan Boundary map, the Oakville Triangle/Route 1 Corridor planning area (shown in pink), consists of the I/Industrial and CSL/Commercial Service Low zoned properties on the west side of Route 1 between Simpson Fields and Ruby Tucker Park. The 100-ft buffer (shown in blue) indicates the sensitive residential and other non-commercial uses directly adjacent to the planning area that require appropriate building transitions with redevelopment. The entire plan area is about

19 acres, located within the Potomac West Small Area Plan area. The Oakville Triangle site is approximately 13 acres of I/Industrial land within the plan area, and is characterized by one and two story brick and metal warehouse structures occupied by a variety of businesses, including self-storage, auto and auto body repair, fitness, dog care, contractors, neighborhood-serving business, and a plant nursery. The Route 1 frontage in the plan area also serves a variety of businesses, such as auto related service and sales

businesses, carry-out restaurants, convenience stores, neighborhood serving uses, and two gas stations. Across Route 1 from the Plan area is Potomac Yard, a 235 acre site which was one of the largest freight rail yards on the East coast of the US until it closed in 1989. Currently in the process of redevelopment starting at the south end, Potomac Yard will be home to a future new Metrorail Station, and a combined 10 million square feet of office, retail, and residential development, as well as open space.



B. HISTORIC MAPS AND HISTORIC PHOTOS



1921 Sanborn Map



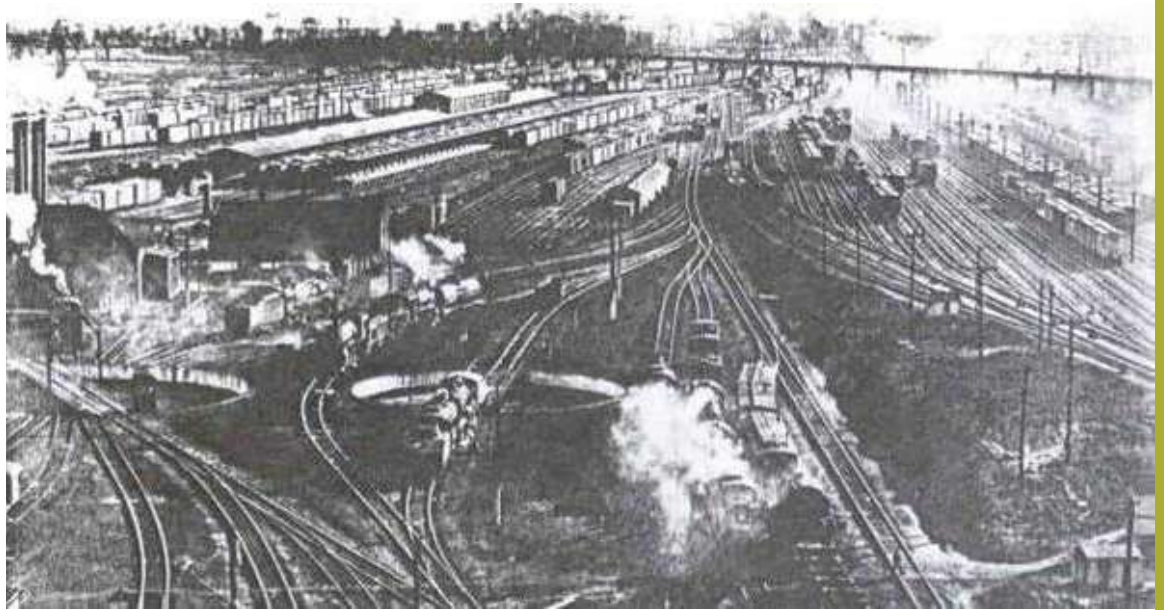
1927 Aerial photo



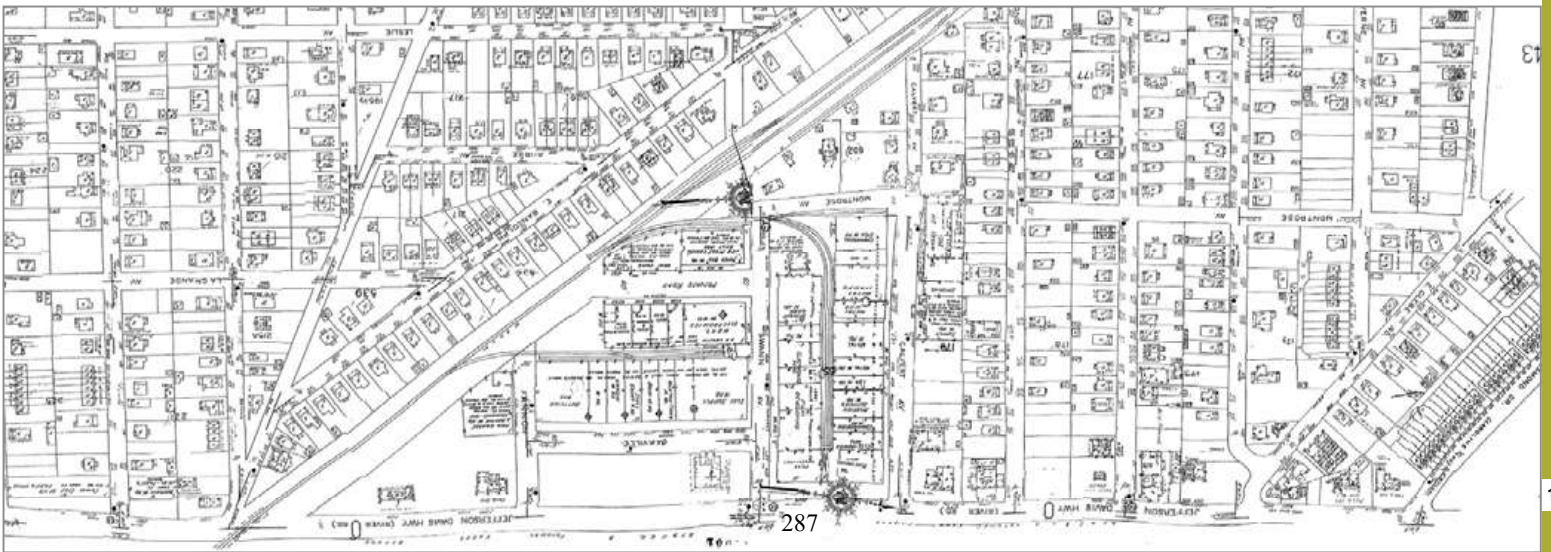
1941 Sanborn Map

APPENDIX

Historic photo of W&OD railroad line, now Mount Jefferson Park, across what is now Route 1 and Potomac Yard to Old Town North.



1958 Sanborn Map

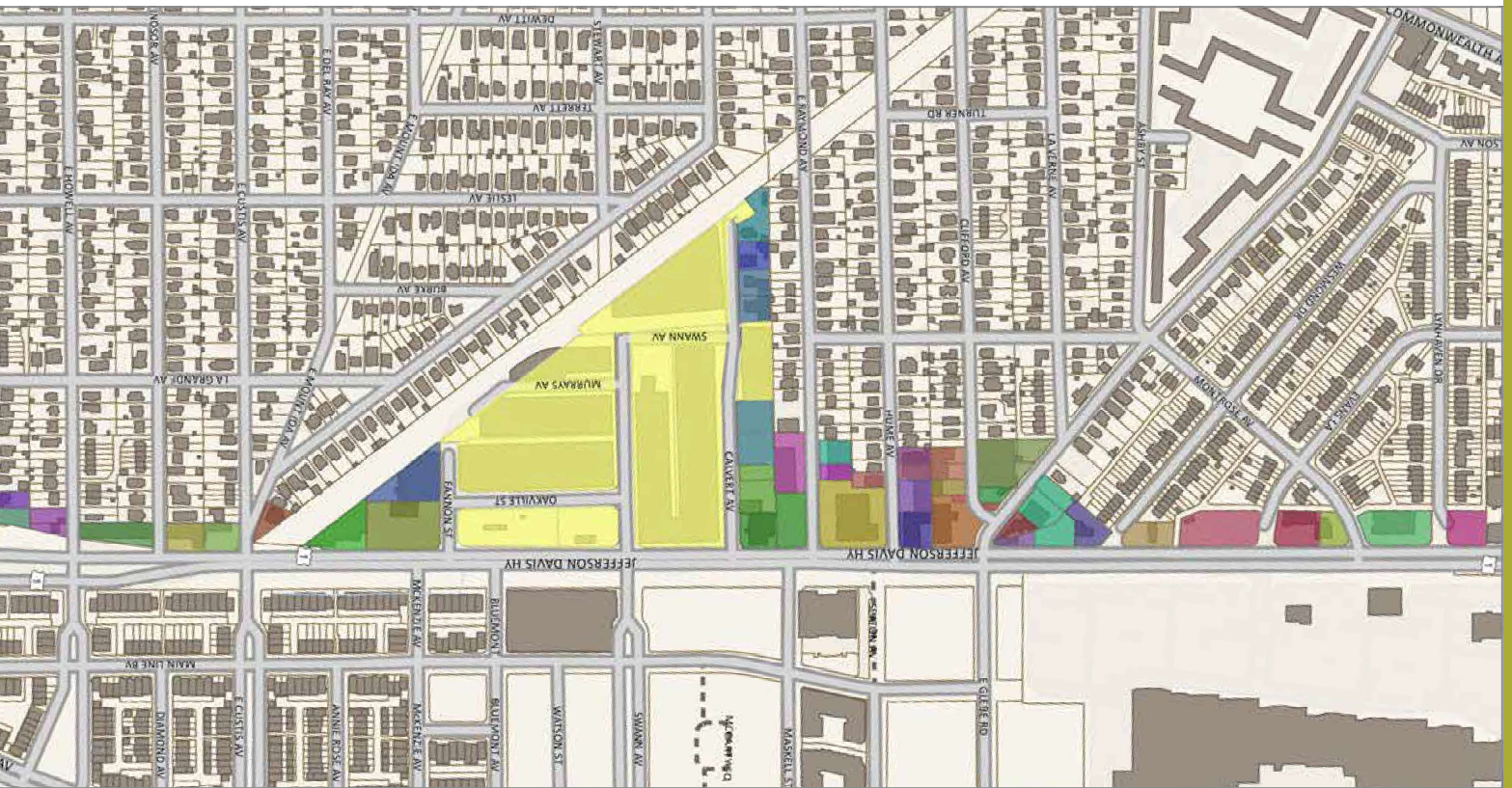


C. EXISTING ZONING MAP



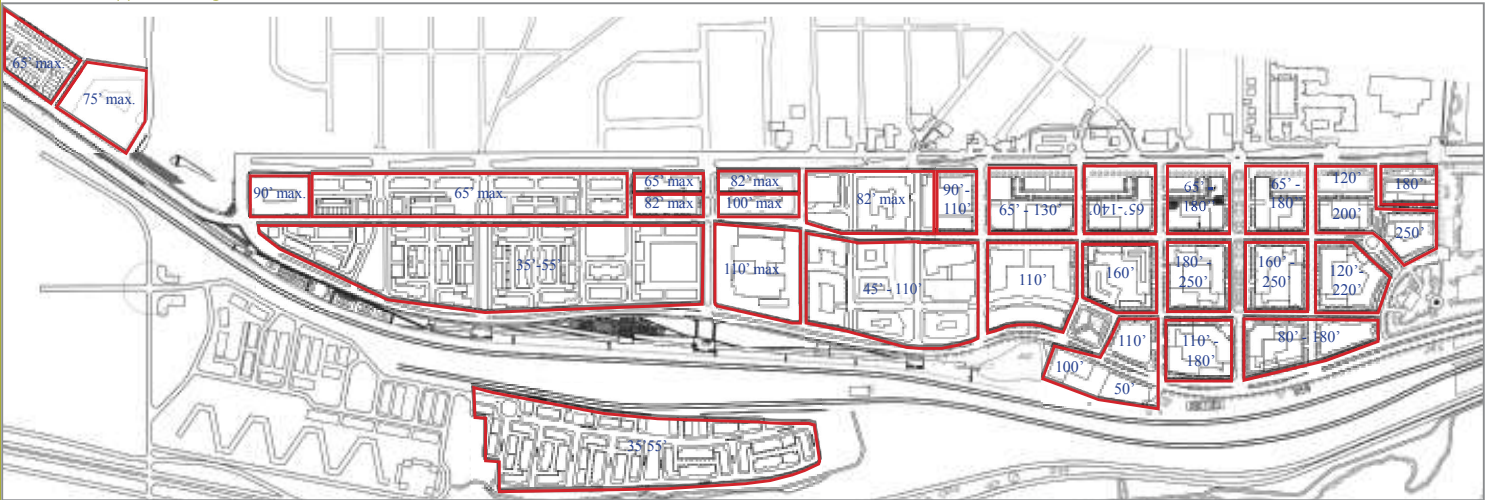
APPENDIX

D. VARIETY OF OWNERSHIP MAP

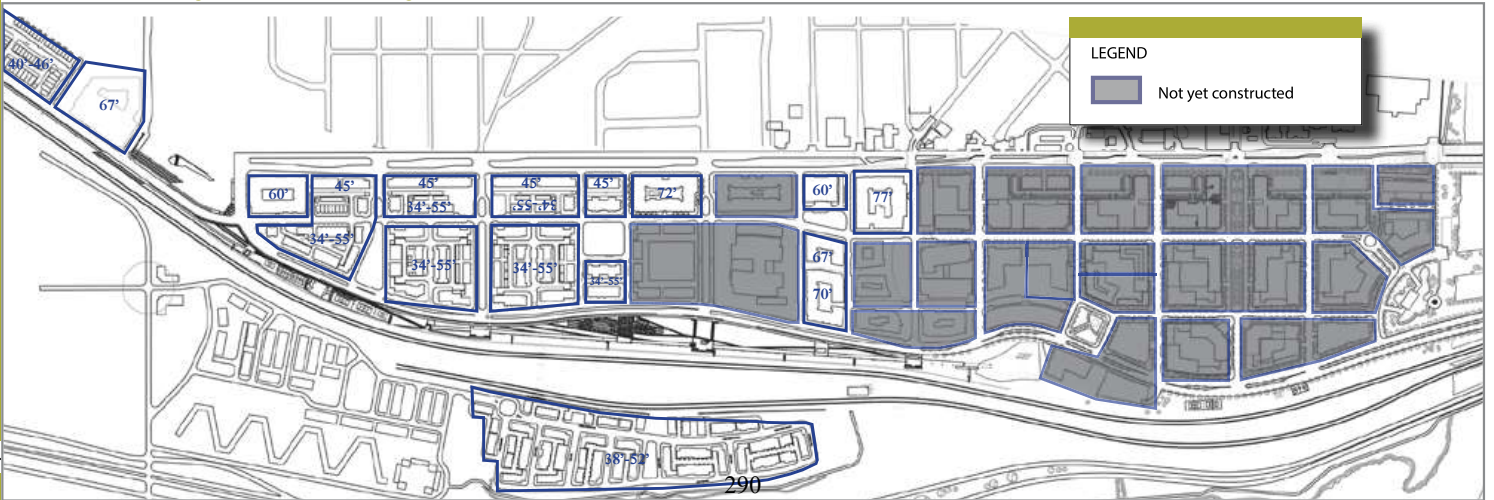


E. APPROVED AND AS-BUILT HEIGHTS IN POTOMAC YARD

Approved Height Maximums



As-Built Heights of Constructed Buildings (2015)



F. EXCERPTS FROM 1992 POTOMAC WEST SMALL AREA PLAN
REFERENCES TO OAKVILLE TRIANGLE

To view the 1992 Small Area Plan in its entirety, visit: <http://www.alexandriava.gov/planning/info/default.aspx?id=44614>

PAGE 22: LAND USE - INDUSTRIAL LAND USE

The 24.3 acre Oakville Industrial Triangle (bounded by US Route 1, the abandoned W & OD right-of-way and Raymond Avenue) is the principal location of industrial activity in Potomac West.

The Triangle consists of one and two story brick and metal warehouse structures occupied by a variety of firms including printing, metal fabrication, equipment and construction materials supply companies, and auto related businesses. With the limited amount of industrial land in the City, this area continues to be fully-leased and its long-term viability should be considered as part of the future City-wide industrial uses study.

In addition to the Oakville Triangle, much of the frontage along the US Route 1 is industrial in character with little transition between the industrial uses and the abutting residential neighborhoods. Many of these uses include auto repair, services and sales. Additionally, there are a few restaurants, carry-outs and convenience stores.

PAGE 23: SUMMARY- LAND USE

- Land use within the Potomac West area is predominantly residential with commercial concentrated along the major thoroughfares. Industrial uses are found along the US Route 1 corridor.
- Both the Mount Vernon Avenue Business Area Plan and the Arlandria Neighborhood Plans encourage mixed use development that is pedestrian oriented and strengthens the “Main Street” type retail environment in these areas.
- There is very little vacant land available in the planning area, but there are areas that are underutilized. The City has implemented several planning tools to support the coordinated redevelopment of larger vacant or underutilized commercially zoned properties.

PAGE 25: EXISTING ZONING

Zoning in the Potomac West area essentially mirrors the land use pattern, with residential being the clearly predominant category. The majority of the commercial zoning is located along the major thoroughfares. Although there are uses along US Route 1 that are more industrial in nature, industrial zoning is concentrated in the Oakville Triangle. Throughout the study area, there are parcels zoned Public Open Space (POS) with the largest being Four Mile Run Park.

PAGE 27: INDUSTRIAL ZONING

Within the study area, only the Oakville Triangle is zoned industrial. Industrial zoning allows light to medium industrial use, such as service, distribution, manufacturing and wholesale and storage facilities at low densities. The intent is that these uses will not negatively impact adjacent neighborhoods. The maximum allowable height for a building is 50 feet with the maximum allowable FAR of 0.85, an increase to 1.25 is allowed with a SUP.

PAGE 27: SUMMARY -ZONING

- Zoning in the Potomac West area is predominantly low to medium density residential.
- Industrial zoning is only found predominantly in the Oakville Triangle. This area should be included in a future City-wide industrial uses study.
- The commercial zoning along Mount Vernon Avenue as well as the existing and proposed Coordinated Development Districts promote a mix of uses as well as pedestrian oriented commercial uses.

PAGE 30: POTENTIAL DEVELOPMENT AND REDEVELOPMENT SITES

Although Potomac West is largely built up, there are sites and areas that could be redeveloped over the next 10-20 years. The three most significant sites subject to major development or redevelopment are the Mt. Vernon Village Center and Birchmere sites, the Safeway/Datatel sites, and the Triangle sites. In addition, there are scattered individual sites along US Route 1, as well as

a collection of parcels along the north side of East Reed Avenue, with the potential for future redevelopment. (Map 8).

PAGE 32: US ROUTE 1 CORRIDOR

Numerous properties located on the west side of US Route 1 could be considered potential redevelopment sites. The massive Potomac Yard development, currently underway on the east side of US Route 1, will change the character of this major north/south route and may lead to the redevelopment of parcels on the west side of the road. Detailed evaluation of the future use and design to complement the Potomac Yard development is needed. Currently, most of the parcels along the US Route 1 corridor are in low-intensity commercial or retail use. Many of the developable parcels are zoned CSL (Commercial Service Low), while the properties located between Hume Avenue and Custis Avenue are zoned for industrial use. In many cases the commercial and industrial uses abut residential uses without adequate buffering or transition. In addition, there are residential developments at the north end of the corridor, and Potomac Yard on the east side of US Route 1 is approved for residential uses. It is also important that impacts on adjacent residential areas are minimized and may be achieved through more extensive landscaping, masonry walls, or other materials that would buffer any impacts from the commercial and industrial uses. This area offers numerous advantages for commercial or service-oriented development, with a location on a heavily-trafficked highway and excellent access to Crystal City and to Ronald Reagan Washington National Airport. This access is being enhanced by the road improvements through Crystal City and the planned realignment of the Monroe Avenue bridge.

PAGE 42: URBAN DESIGN

The principle industrial area is the Oakville Triangle and a strip along US Route 1. While the area has seen some multi-family residential redevelopment, the commercial and industrial frontage along US Route 1 could be generally characterized as the typical arrangement of highway oriented buildings ranging from retail and commercial service uses to manufacturing and light industrial activities. To some extent the relationship between the highway-oriented industrial uses and the abutting residential uses further west are

compatible. In other cases the transitions are awkward; industrial uses seem to encroach upon the residential character of a street such as Raymond Avenue. With the overall built form of Potomac West well established, the focus of the urban design analysis is on those elements within the area that are likely to change - the commercial uses along Mt. Vernon Avenue and potential growth sites - and how these elements might affect the character of Potomac West.

PAGE 48: SUMMARY- POLICY HISTORY

- The City has viewed Potomac West as mostly a built up non-growth area and has encouraged programs emphasizing the conservation and rehabilitation of existing uses.
- To further enhance and serve residential neighborhoods, the City has encouraged commercial revitalization along Mt. Vernon Avenue using a business loan program and capital improvements.
- Significant capital improvements in the forms of flood control, parks, street closings, streetscaping and the undergrounding of utilities have been made by the City to strengthen the residential and commercial components of the area.
- The City has adopted policies to preserve existing industrial areas.
- The City has designated five sites for coordinated development/redevelopment; Route 1 /East Reed Avenue Properties; Mt. Vernon Village Center/Birchmere Sites; Safeway/Datatel Sites; Triangle Sites; and Giant/CVS Sites.

PAGE 50: GOALS AND OBJECTIVES

The goals of the 1988 Potomac West Plan are to:

- to preserve and strengthen the residential area in Potomac West
- revitalize and support neighborhood oriented, small scale retail and office development
- to preserve existing concentrations of industrial uses and the job base these uses provide
- to encourage coordinated mixed use development on the Mt. Vernon Village Center and Route 1 Properties sites
- discourage the use of streets in Potomac West for through traffic

APPENDIX

- provide for the use of effective buffer areas between residential sites and adjacent higher-density and non-residential uses
- require any new residential development to be of the same scale and density as the surrounding residential neighborhood
- discourage the development of major office and commercial sites within Potomac West
- encourage the consolidation and revitalization of commercial areas that serve the Potomac West neighborhood, particularly along Mt. Vernon Avenue

These goals translate into the following specific objectives:

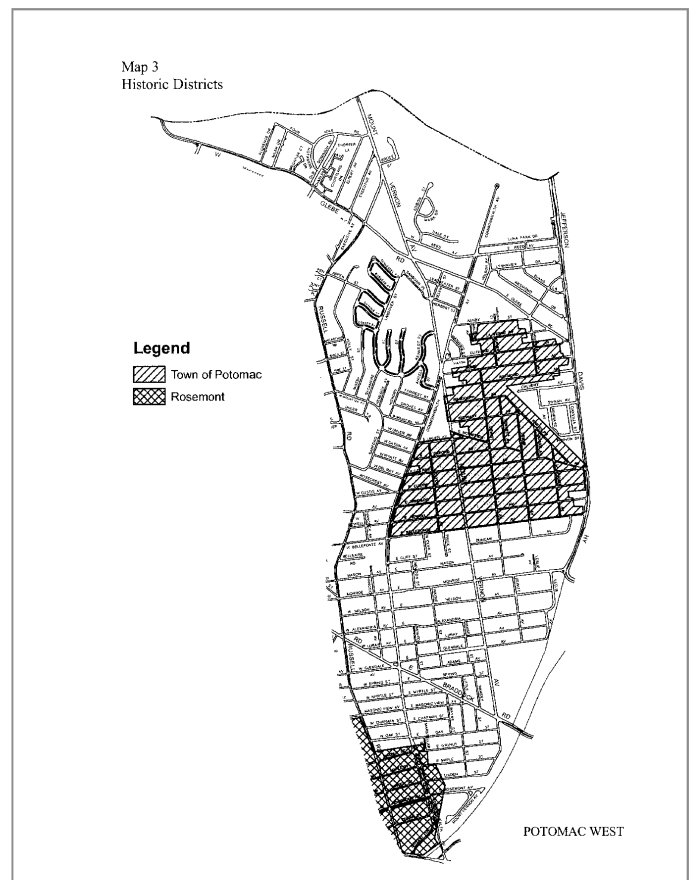
- provide a land use plan which reflects the City's policies to preserve and to protect existing uses and to indicate where coordinated mixed use is desired
- redesign the industrial zones to encourage retention of existing industrial uses and to prohibit high density office development
- reduce overall heights in Potomac West to reflect the low scale character of the area
- continue existing City capital improvement, housing and commercial revitalization programs
- develop guidelines and appropriate zoning controls and incentives for development of the Mt. Vernon Village Center and the Route 1 sites
- increase and coordinate public transportation services along Mt. Vernon Avenue.

PAGE 52: LAND USE RECOMMENDATIONS

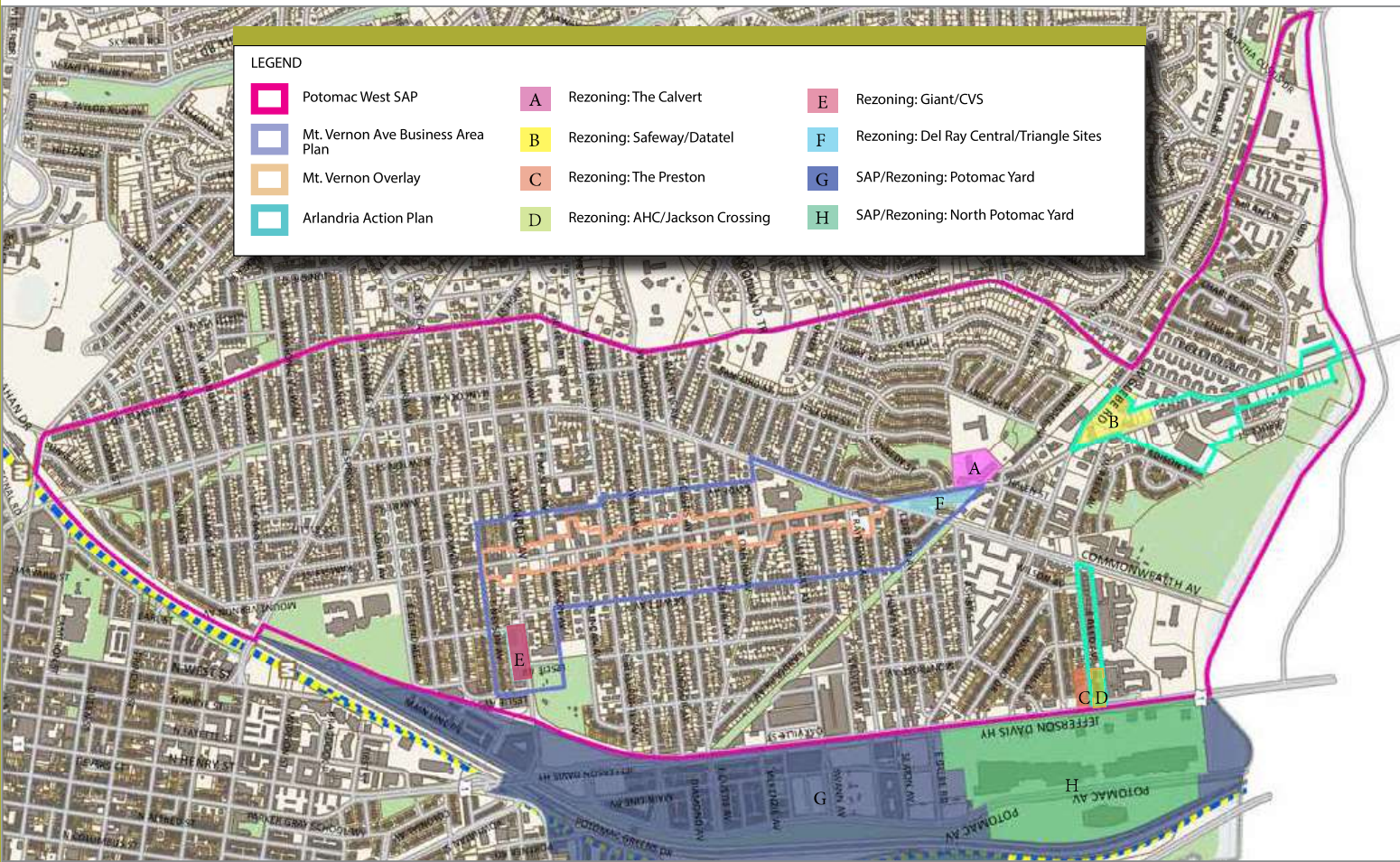
The SAP designates most of the residential areas in accordance with existing land use patterns; either residential low or medium with the mid to high rise apartments designated for residential high. Map 10 shows the proposed changes to the land use plan.

The intent of CL is to provide for the types of low scale retail, office and residential uses. The CSL designation or commercial service low, is designed for low scale commercial uses with some light industrial activity which would be compatible with nearby residential areas.

The I industrial category is designed for light industrial, warehouse, service or limited office or flex space. The Oakville Triangle should remain industrial. The City holds open the possibility of reevaluating the area in the future as development occurs in the Route 1 corridor. The area may be considered as a CDD in the future.



G. APPROVALS AND CHANGES TO THE POTOMAC WEST SAP AND POTOMAC YARD SAP SINCE 1992



WHY A NEW PLAN?

A. INTRODUCTION

As shown on the map on the preceding page, the Potomac West Small Area Plan has been updated a number of times since it was adopted in 1992. The Oakville Triangle/Route 1 Corridor plan area, located within Potomac West, has not been updated comprehensively since that time, and due to its proximity to the Transitway, the future Potomac Yard Metrorail Station, and the redevelopment currently underway in Potomac Yard, change is certain. As a result, the Plan was undertaken to establish a long-term (20 year) vision and framework for the future – for infrastructure, land uses, urban design, and open space, intended to serve as a guide for public and private investment. Developing the Plan proactively, in advance of redevelopment of the west side of the Route 1 Corridor, enables a more holistic approach that fosters connections – between people and their jobs, the urban and natural environment, between neighborhoods and services, and the planned Potomac Yard Metrorail station. The Plan builds on the strengths of the area, its industrial heritage, neighborhood-serving businesses, the character of the adjoining neighborhoods, the newly implemented Transitway and future Metrorail station, and the planned and existing uses within Potomac Yard.

B. ADVISORY GROUP AND COMMUNITY PROCESS

A nine-member Oakville Triangle and Route 1 Corridor Advisory Group was established by City Council Resolution on March 11, 2014, and membership was selected by the City Manager after a public nomination process. The group met monthly from April 2014 through November 2015, for a total of 19 meetings as shown in the Work Plan graphic on the following page. All meetings were open to the public and attended by surrounding neighborhood residents as well as other interested community members.

The Advisory Group provided advice to City staff on the planning for Oakville Triangle and the Route 1 Corridor, specifically assisting in developing the Plan principles regarding potential land uses, open space, transportation and

connectivity, urban design and height, and potential community benefits, among other topics.

The community outreach process also involved staff presentations to civic associations in the surrounding area, as well as walking tours with residents in order to allow for a more in-depth description and conversation of potential plan elements.

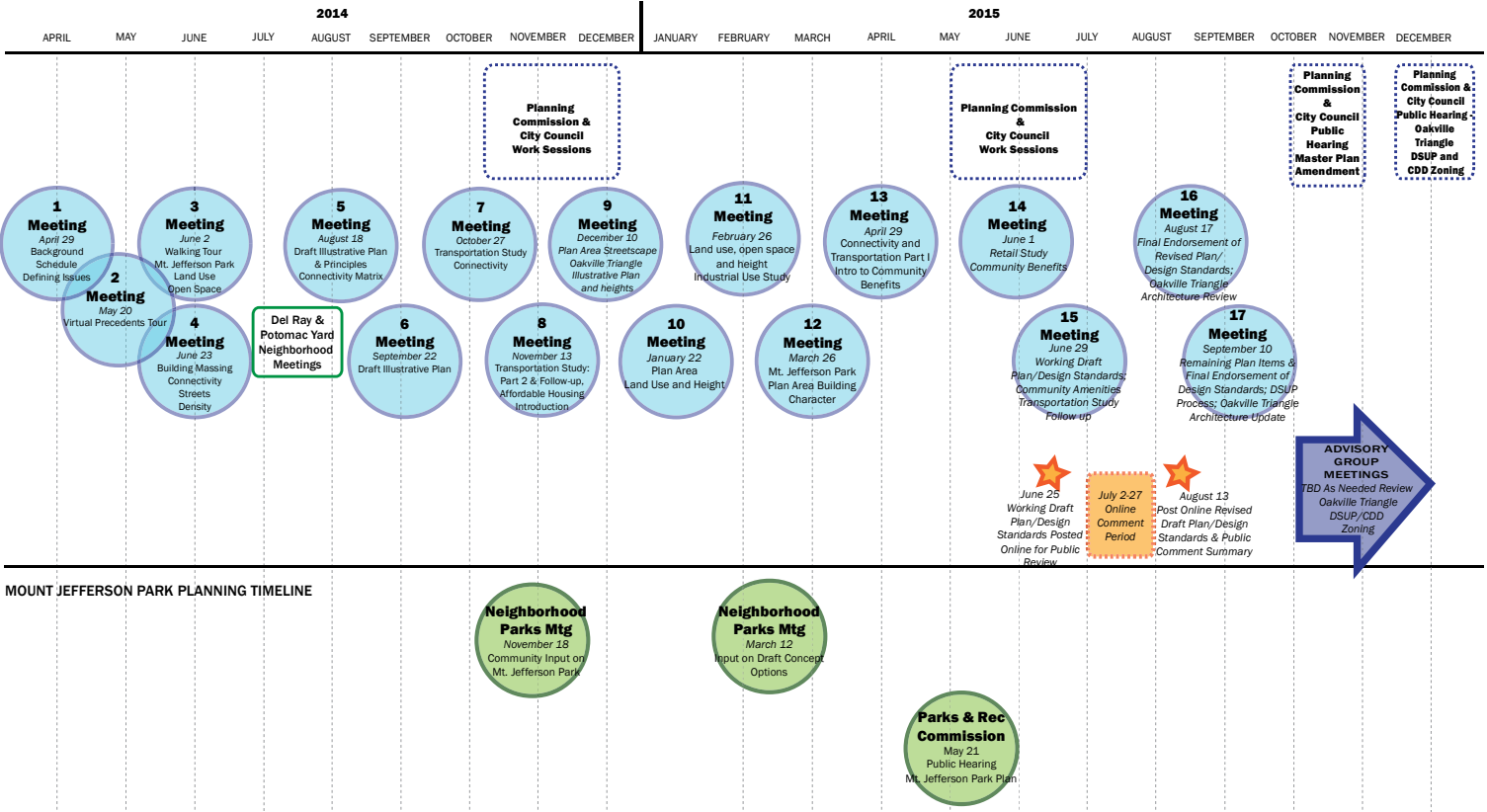
Additionally, separate community meetings were held specifically related to the enhancement of Mount Jefferson Park & Greenway between East Raymond Avenue and Route 1. Community members were given the opportunity to develop and provide feedback on the Park Concept Plan in these meetings as well as in the Advisory Group meetings, and through an online survey prior to the presentation to the Planning Commission.

Upon the completion of the working draft of the Oakville Triangle/Route 1 Corridor Vision Plan and Urban Design Standards & Guidelines, the community was invited to review and comment on the chapters and requirements via AlexEngage, the City's online engagement forum.

In addition to soliciting individual public comment, staff provided briefings to and solicited feedback from the Planning Commission, Transportation Commission, Parks and Recreation Commission, and Alexandria Housing Affordability Advisory Committee (AHAAC).

Oakville Triangle Work Plan

OAKVILLE TRIANGLE ROUTE 1 CORRIDOR WORK PROGRAM DIAGRAM



APPENDIX

Oakville Triangle Civic Engagement, Meetings 2014-2015

DATE	GROUP	MEETING TYPE	DATE	GROUP	MEETING TYPE
2014			2015		
April 29, 2014	Advisory Group	Regular Meeting	January 22, 2015	Advisory Group	Regular Meeting
May 20, 2014	Advisory Group	Regular Meeting	February 26, 2015	Advisory Group	Regular Meeting
June 2, 2014	Advisory Group	Regular Meeting	March 12, 2015	RPCA Community Meeting	Mount Jefferson Park
June 23, 2014	Advisory Group	Regular Meeting	March 19, 2015	Parks and Recreation Commission	Update
July 17, 2014	Parks and Recreation Commission	Update	March 26, 2015	Advisory Group	Regular Meeting
July 23, 2014	Del Ray	Walking Tour	March 30, 2015	Potomac Yard Metro Meeting	Update
July 24, 2014	IndCor/Stonebridge*	Tenant Meeting	March 31, 2015	Del Ray Civic Association - Land Use Committee	Update
August 14, 2014	Potomac Yard	Walking Tour	April 8, 2015	Del Ray Civic Association	Update
August 18, 2014	Del Ray Business Association	Update	April 29, 2015	Advisory Group	Regular Meeting
August 18, 2014	Advisory Group	Regular Meeting	May 4, 2015	Lynhaven Civic Association	Update
September 18, 2014	Parks and Recreation Commission	Update	May 21, 2015	RPCA Community Meeting	Mount Jefferson Park
September 22, 2014	Advisory Group	Regular Meeting	June 1, 2015	Advisory Group	Regular Meeting
October 15, 2014	Transportation Commission	Update	June 2, 2015	Planning Commission	Work Session
October 25, 2014	Neighborhood Parks Planning	District 2 Workshop	June 4, 2015	Alexandria Housing Affordability Advisory Committee	Update
October 27, 2014	Advisory Group	Regular Meeting	June 23, 2015	City Council	Work Session
November 7, 2014	Planning Commission	Work session	June 29, 2015	Advisory Group	Regular Meeting
November 13, 2014	Advisory Group	Regular Meeting	July 15, 2015	Lynhaven Civic Association	Walking Tour
November 17, 2014	Arlandria Civic Association	Update	July 21, 2015	Potomac Yard Citizens Group	Update
November 18, 2014	RPCA Community Meeting	Mount Jefferson Park	August 17, 2015	Advisory Group	Regular Meeting
December 9, 2014	City Council	Work Session	September 3, 2015	Alexandria Housing Affordability Advisory Committee	Update
December 10, 2014	Advisory Group	Regular Meeting	September 9, 2015	Del Ray Civic Association	Update
December 18, 2014	Potomac Yard Design Advisory Committee	Update	September 10, 2015	Advisory Group	Regular Meeting
			September 14, 2015	Lynhaven Civic Association	Update

* City did not attend

Oakville Triangle Civic Engagement, Additional Outreach

DATE	OTHER	TYPE
2014		
Pre-Plan Outreach	Advisory Group	Flyer
Pre- Plan Outreach	Advisory Group	Signage
April 15, 2014	Property owners in Plan area contacted	Letter 1
May 1, 2014	Property owners in Plan area contacted	Letter 2
August 1, 2014	Property owners in Plan area contacted	Letter 3
October 14, 2014	Property owners in Plan area contacted	Letter 4
2015		
January 8, 2015	Property owners in Plan area contacted	Letter 5
Other Forms of Outreach		
Throughout Planning Process	Individual property owner meeting	Meeting
Throughout Planning Process	eNews notifications and social media postings prior to each public meeting	Outreach/Notice
Throughout Planning Process	All public meetings posted to City calendar	Outreach/Notice



C. EXISTING CITY PLAN AND POLICIES

i. City Council’s Strategic Plan (2015)

The Plan builds upon existing City plans and policies including the Alexandria Strategic Plan that emphasize transit-oriented development that maximizes the use of public transportation and focuses growth around transportation infrastructure, leveraging investment in these areas with the highest and best use.

ii. Housing Master Plan (2014)

The Plan is consistent with the City’s Housing Master Plan, which recommends focusing affordable housing efforts in areas with the greatest potential for increased density and mixed-use development, and encourages housing options affordable to a range of incomes and welcoming to different household types—including young professionals and families—to support inclusive neighborhoods, including workers who are critical to the economic competitiveness and sustainability of the area. Oakville Triangle, North and South Potomac Yard, and the Route 1 Corridor host (and will continue to attract) a wide variety of employers. The success of these businesses, in part, relies on the availability of a diverse workforce. Providing affordable and life stage-appropriate housing in close proximity to jobs and transit will help improve workers’ and residents’ quality of life, reduce congestion, lessen economic leakage, and strengthen the City’s tax base.

iii. North Potomac Yard (2010)

North Potomac Yard is directly across and slightly north of the Oakville Triangle/Route 1 Corridor planning area. The Plan seeks to compliment and integrate Oakville Triangle with the approved plan for North Potomac Yard and the future Metrorail Station.

iv. Green Building Policy (2009)

This Plan incorporates the development standards set forth in the 2009 Green Building Policy for public and private development that requires a

Development Site Plan (DSP) or Development Special Use Permit (DSUP). New development should achieve the following green building standard:

- Non-Residential: LEED Silver.
- Residential: LEED Certified, LEED for Homes, or ANSUICC-700 2008 National Green
- Mixed use: Each component should follow the applicable rating standard
- Coordinated Development Districts: Approvals for CDD areas yet to be developed will incorporate these standards

In each case, applicable ENERGY STAR systems should be incorporated.

v. Pedestrian and Bicycle Master Plan (2008)

This Plan complies with the City’s Pedestrian and Bicycle Mobility Plan ensuring for the provision of safe and attractive spaces and connections for pedestrians, as well as the addition of bike facilities to provide better connectivity to the neighborhoods and public transit.

vi. Transportation Master Plan (2008)

The Plan is consistent with the goals included within the City’s Transportation Management Plan, adopted in 2008. It accommodates a multi-modal transportation system by establishing a transportation framework consistent with the Complete Streets Policy adopted by City Council in 2011 that prioritizes pedestrians, bikes and transit. It incorporates the Route 1 Metroway to serve the neighborhoods, ensuring an easy walk to transit stations at Swann Avenue, East Custis Avenue and East Glebe Road. The improved transportation system also expands the number of bicycle and pedestrian options by providing additional facilities (trails, sidewalks, bike lanes and shared facilities), and improves pedestrian and bicycle connectivity within the plan area, to adjacent neighborhoods including Del Ray and Potomac Yard, and to the Mount Jefferson Trail. The land use and mix of uses are incorporated as part of the Plan to support transit use.

The Plan improves overall vehicular mobility by creating a more connected and urban roadway network, designed in a grid layout to provide better

circulation and connectivity for all modes of travel. It also improves a number of intersections to accommodate vehicular traffic.

vii. Open Space Master Plan (2002)

The City's Open Space Master Plan establishes a framework for addressing Alexandria's short and long term open space needs. It defines an approach to maximize Alexandria's limited open space opportunities by creating a system to build upon the City's dense, urban condition through 15 simple and direct goals. The Oakville Triangle/Route 1 Corridor Plan helps the City to continue to meet those 15 goals.

In particular, the following Open Space Plan goals are addressed:

- Goal 1 Protect and enrich existing parks
- Goal 5 Create an open space network in new development areas
- Goal 10 Link and expand pedestrian, bicycle and trail system
- Goal 11 Enhance streetscapes and gateways
- Goal 12 Expand citywide street tree program and protect existing trees and woodland areas

The Open Space Master Plan helps the City achieve the five goals listed above through improvements to Mount Jefferson Park and the expansion of Ruby Tucker Park; the provision of new public open spaces on redevelopment sites; new bicycle and pedestrian trail connections; undergrounding utilities on Route 1; and adding street trees throughout the Plan area.

viii. Potomac West Small Area Plan (1992)

The Oakville Triangle/Route 1 Corridor planning area is included within the boundaries of the Potomac West Small Area Plan, adopted by City Council in 1992. References to the planning area in the Potomac West SAP with regard to land use, transportation and zoning for future development were addressed during this process. Some of these references include:

Page 27 Zoning: "Industrial zoning is only found predominantly in the Oakville Triangle. This area should be included in a future City-wide industrial uses study."

Page 32 US Route 1 Corridor: "Numerous properties located on the west side of US Route 1 could be considered potential redevelopment sites. The massive Potomac Yard development, currently underway on the east side of US Route 1, will change the character of this major north/south route and may lead to the redevelopment of parcels on the west side of the road. Detailed evaluation of the future use and design to complement the Potomac Yard development is needed."

"...It is also important that impacts on adjacent residential areas are minimized and may be achieved through more extensive landscaping, masonry walls, or other materials that would buffer any impacts..."

Page 40 Transportation: Improvements to pedestrian safety are strongly encouraged to promote walking and bicycling as transportation alternatives to and from Potomac Yard, the Braddock Road Metro Station, and the neighborhoods west of US Route 1.

Page 52 Land Use Recommendations: "The City holds open the possibility of reevaluating the area [Oakville Triangle] in the future as development occurs in the Route 1 corridor. The area may be considered as a CDD in the future."

ix. Potomac Yard/Potomac Greens Small Area Plan (1992)

Potomac Yard is directly across from the Oakville Triangle/Route 1 Corridor planning area. While considering any potential impact on Potomac Yard as well as the importance of integrating the two planning areas.

x. Potomac Yard Metrorail Station and Transitway

The Plan proposes taller height at Transitway stops and in close proximity to the future Metrorail Station, leveraging the City's transit investments and encouraging future transit use.

■ A P P E N D I X

xi. Parking Standards for New Development Projects
Study (2015)

New development within the Plan area will be developed in accordance with the standards and recommendations established in the parking study and future CDD zoning.

DEMOGRAPHICS & STUDENT GENERATION

A. DEMOGRAPHICS

The characteristics of the area around the Oakville Triangle and Route 1 Corridor planning area were examined by looking at census statistics for the city as a whole, and for those block groups within about one mile of the planning area. The figure below shows the context area considered.

Statistics on age, race and ethnicity, and household and family structure were taken from the 2010 census, since this data was based on a count of most households and is more accurate than sample statistics.

Statistics on employment, income, commuting and similar characteristics were taken from the American Community Survey 2009-2013 5-year average data, which is the most recent information on these characteristics for small geographic areas. This data is based on an approximately 1% sample of households each year, and is subject to substantial sampling variation, and can mask changes that take place during the 5-year period of the average. None of this data predates the recent residential development in Potomac Yard west of Potomac Avenue except for The Station at Potomac Yard.

In comparison to the city as a whole, the context area around the planning area has an age distribution nearly the same as that of the city as a whole. The context area's population has a slightly higher percentage of pre-school children (8.2% vs 7.1% citywide), about the same percentage of school-age children (10.3% vs 10.0% citywide), and somewhat fewer seniors (7.4% of population in the context area was 65 and over in 2010, vs. 9.1% citywide).

Median household income was estimated at \$99,000 per year, compared to \$85,706 for the city as a whole.

The context area is somewhat less racially diverse than the city as a whole according to the 2010 Census, with 66.6% White population compared to 60.9% for the city as a whole. An estimated 14.4% of the context area is Black or African American compared to 21.8% for the city, and 3% is Asian,

compared to 6.0% for the city as a whole. 11.2% of the population of the planning area identified themselves as "some other race," compared to 7.1% for the city as a whole, reflecting a higher percentage of Hispanic or Latino residents in the planning area.

22.6% of the context area population identified themselves as Hispanic or Latino of any race, compared to 16.1% of the population citywide. The context area includes the Arlandria census tract, which has the highest percentage of Hispanic residents of any tract in the city at 62.0%.

50.3% of households in the context area were renter households in 2010, compared to 56.7% for the city as a whole and 34.9% for the U.S. Inner suburbs like Alexandria and central cities of metropolitan areas tend to have a substantially higher percentage of rental housing than the average for the rest of the nation.

14.8% of households in the planning area were married-couple families with children, slightly more than the citywide share of 13.2%. 40.8% were single-person households, slightly less than the citywide rate of 43.4%.

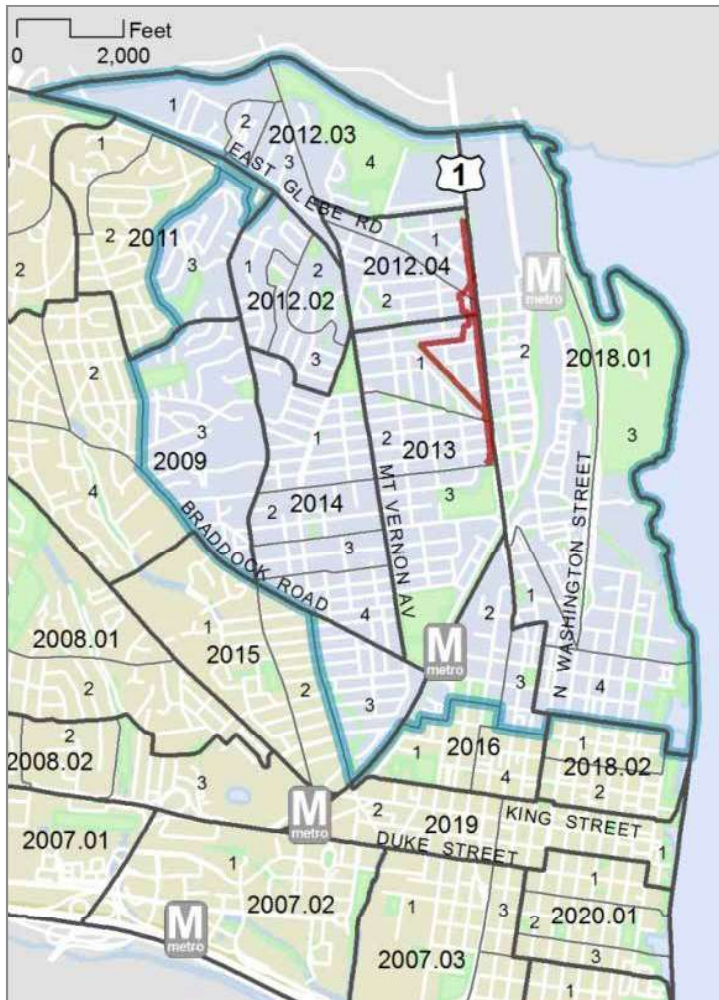
B. STUDENT GENERATION

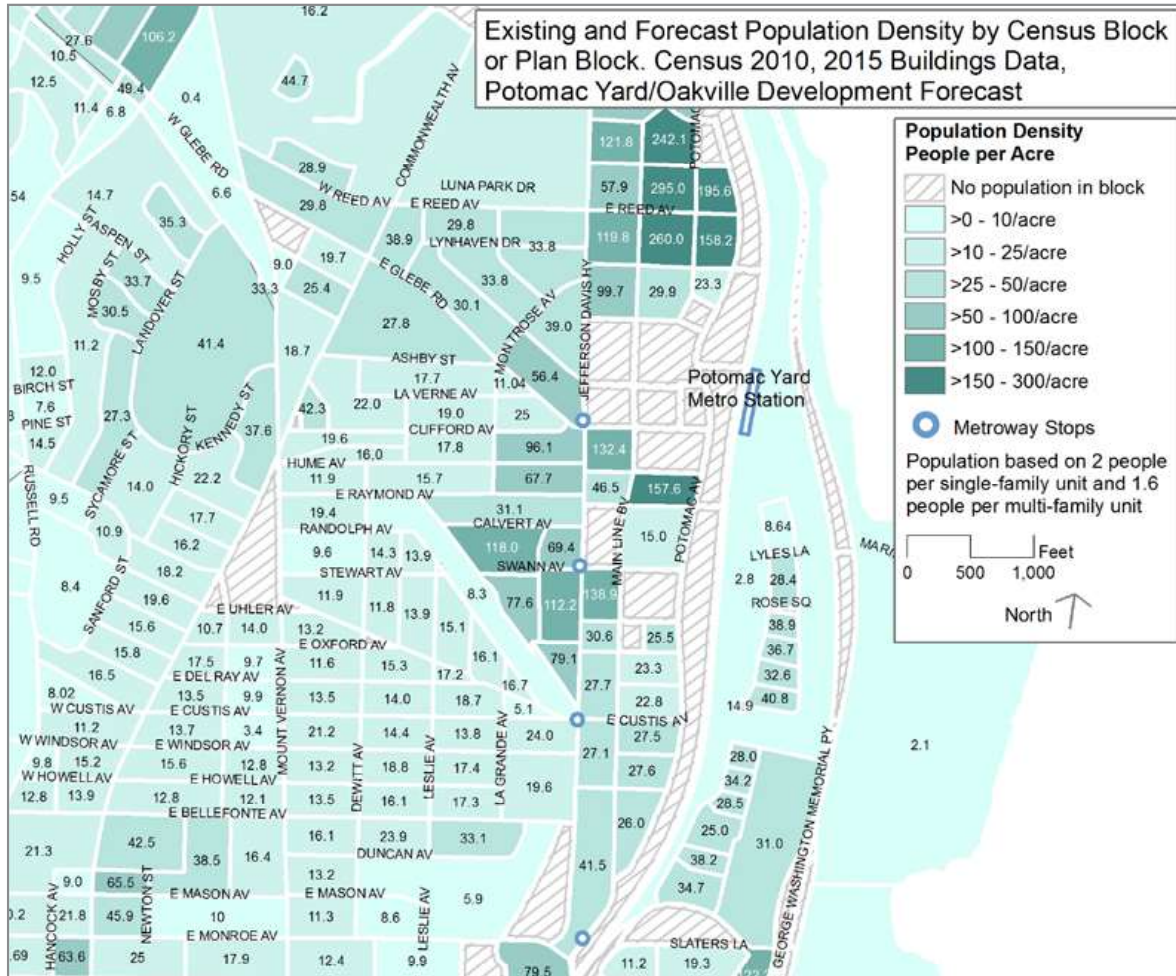
The Oakville Triangle/Route 1 Corridor development is expected to include approximately 1,900 multi-family units and 100 townhouse units when fully developed. Recent school enrollment statistics indicate that new market-rate multifamily developments result in approximately 0.03 students per dwelling unit, or about one student for every 33 new apartment or condominium units. New townhouse units generate approximately 0.1 students per dwelling unit, or one student per 10 units. These generation rates for new development are based on 2013 student enrollment from existing market-rate apartments and condominiums that are less than 30 to 40 years old in Alexandria.

Based on these generation factors, the development anticipated in the Plan area would be expected to generate approximately 57 Alexandria City Public

APPENDIX

CENSUS TRACTS IN THE VICINITY OF THE PLANNING AREA





Note: Anticipated number of new residents in Plan area projected: 3,200. Based on 1900 multi-family units and 100 townhouses.

Schools (ACPS) students in all grades from the proposed approximately 1,900 multi-family units, and 10 students in all grades from the 100 townhouse units, for a total of 67 students at full development.

The Oakville Triangle area is within the attendance area of Mount Vernon Elementary School, which feeds George Washington Middle School. Areas north of Hume Avenue are within the Cora Kelly Elementary School attendance area. Mount Vernon Elementary School enrollment in the 2014-15 academic year exceeded its nominal capacity of 755 students under recently adopted educational specifications. The school has additional capacity based on the number of classrooms available and adopted class size limits, but since this is one of the oldest schools in the system, current classrooms are comparatively small. Cora Kelly enrollment is currently below capacity under new educational specifications, and is expected to continue to be slightly below capacity in 2020.

In addition to market-rate housing development, new development may include affordable housing incorporated in market-rate buildings or funded through affordable housing contributions from both residential and commercial development. Affordable housing designed for families and made affordable for low- and moderate-income households generates about 0.6 students per housing unit. If one assumes that approximately 5 affordable units will be developed or reserved for each 100 net new housing units constructed, 91 affordable units would be constructed or existing units rehabilitated or reserved as affordable units as a result of this project. These affordable units would be expected to generate approximately 55 additional students in ACPS schools. If affordable housing is made available within the project, these students would attend Mount Vernon Elementary School. If affordable units are funded through affordable housing contributions in other locations, then these students could attend any ACPS school.

C. REDISTRICTING PROGRAM

ACPS is initiating a redistricting program that is expected to rebalance school attendance boundaries based on current and projected enrollment. Currently, schools in the West End are over capacity on balance, and some

capacity is available in schools on the east side of Alexandria, including Cora Kelly and the new Jefferson-Houston K-8 school. Redistricting may reduce the available capacity in the east end in order to relieve capacity issues in the west. Additional capacity programs are under way in the West End, including planning for replacement of Patrick Henry school with a K-8 school of increased capacity. Because the Oakville Triangle is on an attendance boundary, it is possible that the project may be placed in a different attendance area as a result of the redistricting program.

D. LONG-RANGE EDUCATIONAL FACILITIES PLAN

Alexandria City Public Schools, in cooperation with the City of Alexandria, recently completed and adopted a Long Range Educational Facilities Plan, focusing on needs for elementary and middle schools. This plan identified substantial potential school capacity problems, primarily in Alexandria's West End. However, schools in the eastern part of the city tend to be the oldest, and therefore have problems with classroom size and facilities to meet current educational needs. The plan identified a number of potential improvements to existing schools, as well as need for additional schools with possible specific locations for some.

With the adoption of the plan, the ACPS Board identified two immediate needs – a financing plan to meet the needs identified in the facilities plan, and development of a facilities plan for additional high school capacity. These two needs are to be addressed in 2015-16. As elementary enrollment continued to grow during the planning process, it became clear that more students are staying in ACPS schools as they reach the upper grades, and that the recently completed T.C. Williams High School and Minnie Howard 9th Grade would not be able to meet growing needs for high school into the 2020s if this trend continues. A financing plan is being developed to consider a variety of means of providing facilities to both rehabilitate or replace aging schools, and to meet capacity demands into the next decade through a combination of capacity improvements at existing schools, new school construction, and potential leasing of existing buildings as permanent or temporary school space. A need for continuing swing space to accommodate enrollment while existing schools are comprehensively rehabilitated was also

identified.

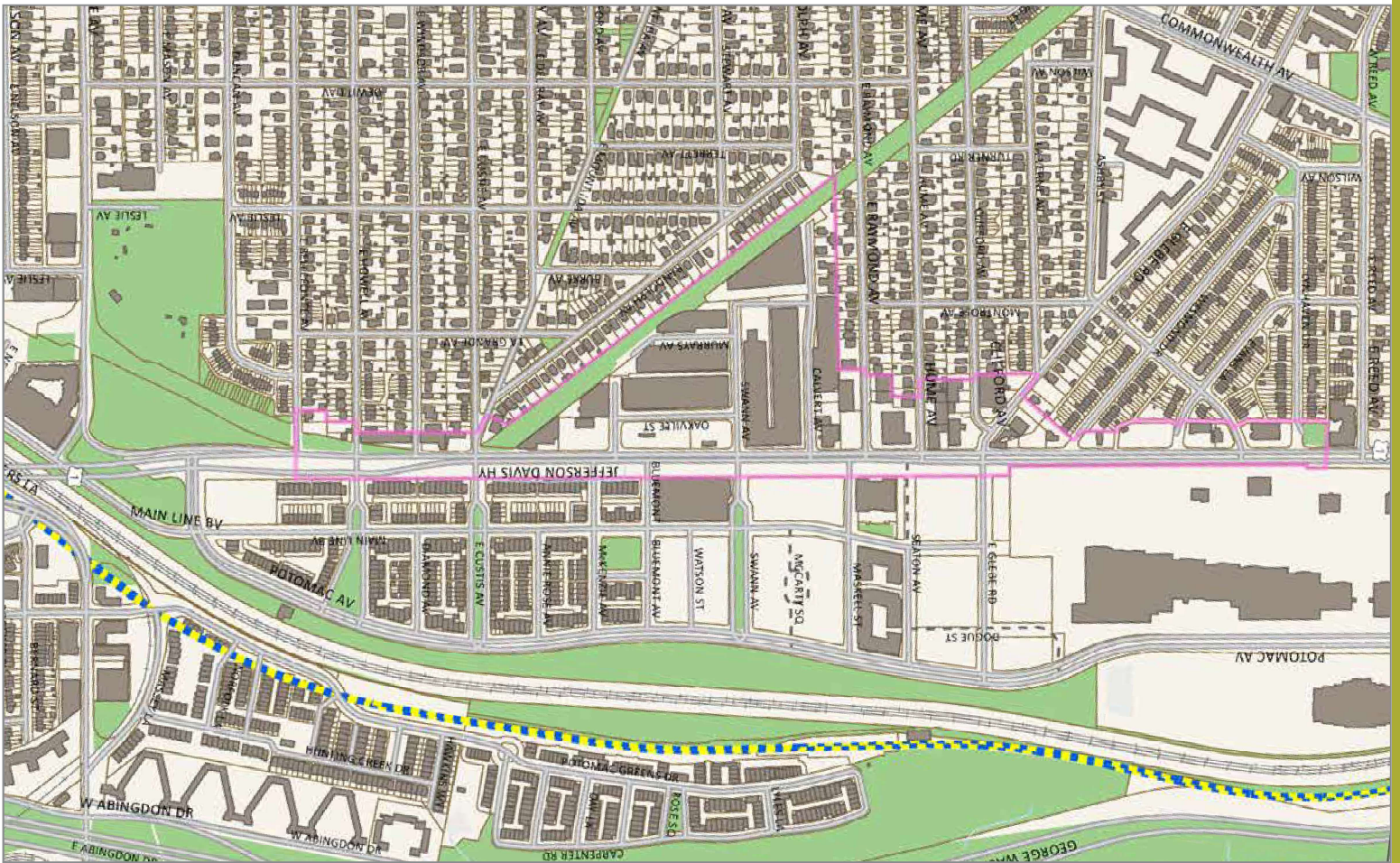
E. CAPITAL IMPROVEMENTS PLANNING

Long-term enrollment projections for ACPS are based on both current patterns of enrollment change from existing development, and enrollment expected from new development. Projections are used to identify the need for new schools and expansion of existing schools. The current long-range forecast anticipates significant further growth in enrollment before an expected slowing of growth in the mid-2020s as the current group of students generated by the recent growth spurt passes through the grades. Capital facilities to provide for growth are generally programmed and budgeted closer to the point of need when projections have been confirmed by continued births, actual growth in school enrollment and follow-through in plans for new construction of residential units. Major capital facilities improvements in the current ACPS Capital Improvement Program Budget include major expansion/reconstruction of Patrick Henry School, currently in the design process, and reconstruction of Cora Kelly School, programmed for 2019-2020. Expansion at James K. Polk Elementary School is programmed for 2017.

F. ADDITIONAL SCHOOL FACILITIES

In the long term, two additional school sites are potentially available and currently reserved for ultimate school use in the east part of Alexandria: a site near Four Mile Run in Potomac Yard that may be suitable for a special academy or urban school, and a larger site sufficient for a full-sized elementary school with shared open space near the new Simpson Fields on Route 1 at Monroe Avenue. That site is has been improved with multi-purpose fields. A new elementary school would provide space for 600 to 800 additional students, more than sufficient to accommodate the additional elementary demand currently expected in the east end of Alexandria well into the future, when combined with improvements to other existing schools to meet new educational specifications

OPENSOURCE AND PARKS



MOUNT JEFFERSON PARK

The Oakville Triangle/Route 1 Corridor Plan area is adjacent to Mount Jefferson Park and will include pedestrian connections in the future. In conjunction with the Oakville Triangle/Route 1 Corridor Plan process and the current Neighborhood Parks planning initiative, the Department of Recreation, Parks and Cultural Activities addressed the relevant section of Mount Jefferson Park (between East Raymond Avenue and Route 1) and how it could best be enhanced and made more accessible to the community.

As part of the planning process for this section of Mount Jefferson Park, staff worked with current park users and neighbors, the Oakville Triangle/Route 1 Corridor Advisory Group, as well as the broader community interested in the park. At their first public meeting in April 2014, the Advisory Group heard from the community that preserving the character of, while at the same time providing access and connections to, Mount Jefferson Park was a priority if/when the Oakville Triangle site redeveloped. Subsequent Advisory Group and park planning meetings reinforced those initial themes.

The planning process and timeline included:

- Advisory Group meetings beginning in April 2014 included discussions about the importance of Mount Jefferson Park and its relationship to the potential new development, and helped to develop a conceptual framework for park improvements.
- September 22, 2014: Advisory Group presentation and feedback on conceptual framework (based on feedback April-August 2014).
- October 25, 2014: Neighborhood Park Planning Workshop presentation and feedback on conceptual framework.
- November 18, 2014: Community Meeting to present planning process, existing conditions, 2013 Needs Assessment information, feedback received through the Oakville Triangle process, and the conceptual framework shown at the September 22 Oakville Triangle/Route 1 Corridor Advisory Group (AG) meeting. Staff received public comments and ideas regarding what they like about this section of Mount Jefferson Park, what needs improvement, and what they would like to see added (if anything). Received feedback regarding the conceptual framework.

- December 2014 to February 2015: Developed draft park plan based on community and Advisory Group feedback.
- March 12, 2015: Community Meeting on draft plan; received feedback on alternatives for each trail section and overall improvement plan.
- March 16 to April 15, 2015: Online survey on draft plan and trail section alternatives.
- March 26, 2015: Advisory Group meeting on draft plan; the group agreed that the plan reflected and balanced community needs and comments received for the park.
- April 15 to May 15, 2015: Developed final draft plan based on community meeting, Advisory Group feedback, and survey results.

EXISTING CONDITIONS



PROPOSED IMPROVEMENTS



SURFACING TREATMENT OPTION
CELL-TEK GRAVEL-LOK PERMEABLE SURFACE (or similar product)



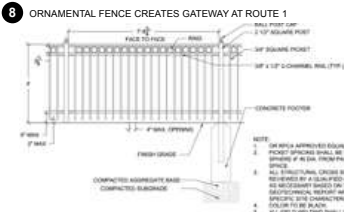
RAIN GARDEN / BEST MANAGEMENT PRACTICE (B.M.P.)



EXISTING TREES TO REMAIN



PROPOSED TREES



309

GENERAL PARK IMPROVEMENTS

- 1 IMPROVE DOG EXERCISE AREA
- 2 TRAIL IMPROVEMENTS
- 3 TRAIL CONNECTION
- 4 IMPROVE SIGNAGE AND WAYFINDING *
- 5 ENHANCE BUFFER //
- 6 PHASED REMOVAL OF INVASIVE SPECIES ☁
- 7 IMPROVE STORMWATER MANAGEMENT AND DRAINAGE
- 8 ROUTE 1 ENTRANCE

OVERALL PARK GOAL:
"MAINTAIN AND ENHANCE THE EXISTING PARK CHARACTER"



Mount Jefferson Park Concept Design

SUMMARY OF COMMUNITY IDEAS AND FEEDBACK:

Throughout the planning process, the community expressed that a key priority for this section of Mount Jefferson Park was to maintain the character of the park to the extent possible. Another priority identified by current users and neighbors to this section of the park was to improve the drainage problems that exist today throughout much of the park.

Addressing concerns about the off leash dog area was a third consistent priority. Currently the area has very poor drainage and no barrier from Raymond Avenue. Many current users run their dogs off leash throughout the park (outside of the designated area), and hoped the park plan would allow them to continue to do so. Other neighbors wanted to see the current rules enforced, more separation between the dog area and the trail, and less overall off leash activity. The final draft of the plan attempts to balance these two, somewhat conflicting needs for the park.

The trail, and the degree of connectedness to Oakville Triangle, is another interest point raised during the planning process. A number of current park users identified a preference for maintaining only the current access points to this section of the park (East Raymond Avenue and Route 1), while the Advisory Group and some neighbors desired connections to and from the Oakville Triangle site. Again, the final draft of the plan provides the latter desired connections to Oakville Triangle, while using landscape and topography to minimize and emphasize the points of access.

The results of the online survey regarding the draft plan showed that the majority of respondents agreed that the draft plan does “reflect and balance the community needs and comments received to date.” The unedited, completed survey responses can be found online at: <http://www.alexandriava.gov/recreation/info/default.aspx?id=83168>.

DETAILS OF THE FINAL DRAFT PLAN:

The final draft plan before the Park and Recreation Commission represents an effort to balance the community needs with the understanding that the adjacent Oakville Triangle property will likely be redeveloped in the future. The overall goals and improvements shown in the plan include:

- Maintaining the character of the park, including keeping the existing topography created by the old rail line;
- Addressing the stormwater/drainage issues;
- Enhancing and expanding the off-leash dog area;
- Providing better separation between the dog exercise area and the trail;
- Enhancing the trail with a pervious surface treatment;
- Providing 3' landscaped berms along the new Park Road (Oakville Triangle site) and the park;
- Improving access and wayfinding at East Raymond Avenue and Route 1;
- Improving a new pedestrian/bicycle access point at Stewart Avenue; and
- Enhancing native buffer plantings along the trail.



IMPLEMENTATION OF THE PLAN:

The Oakville Triangle/Route 1 Corridor Advisory Group, along with the involved community have expressed identified improvements to this section of Mount Jefferson Park as one of the highest priority community benefits. Implementation of this park plan is anticipated to occur in conjunction with the first phase of development of Oakville Triangle, if approved by the City. In the absence of Oakville Triangle redevelopment, the park plan would be included with the 17 Neighborhood Park Plans currently underway, and considered through the City’s Capital Improvement Program (CIP) in future years.

SOLAR STUDY



ASSUMPTIONS:

- The model herein has been geo-referenced in Sketchup and Google Earth to the approximate location of the site in Alexandria, Virginia per the solar conditions on the given date(s) and time(s) analyzed.
- Topography was not included in the model.
- The dates selected were the first days of each season in the 2015 calendar year: March 21 (Spring), June 21 (Summer), September 21 (Fall), December 21 (Winter) at 9am, 12pm, and 5pm (4pm for Winter).

APPENDIX

9AM - SPRING



9AM - SUMMER



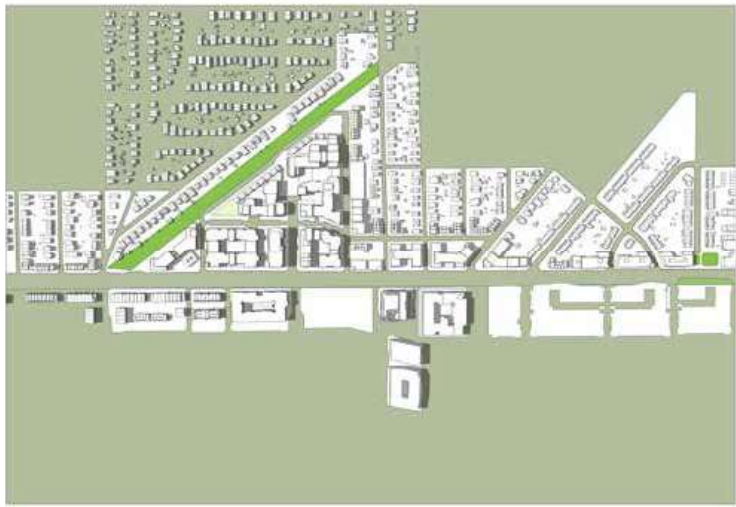
9AM - FALL



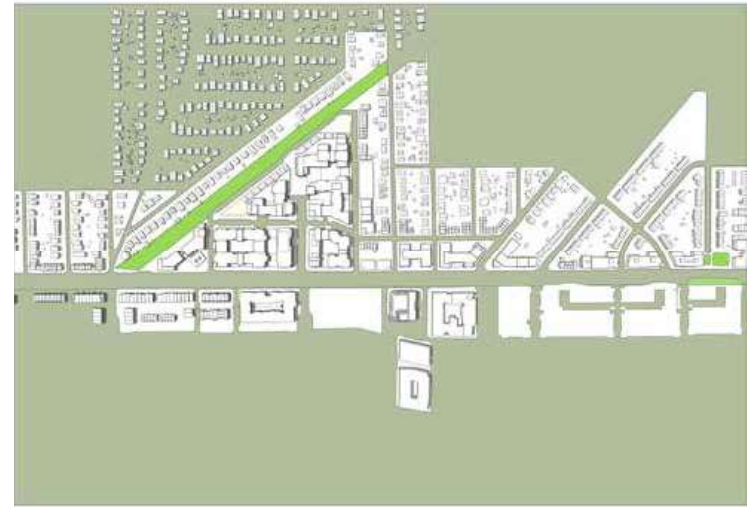
9AM - WINTER



12PM NOON - SPRING



12PM NOON - SUMMER



12PM NOON - FALL



12 PM NOON - WINTER



■ APPENDIX

5PM - SPRING



5PM - SUMMER



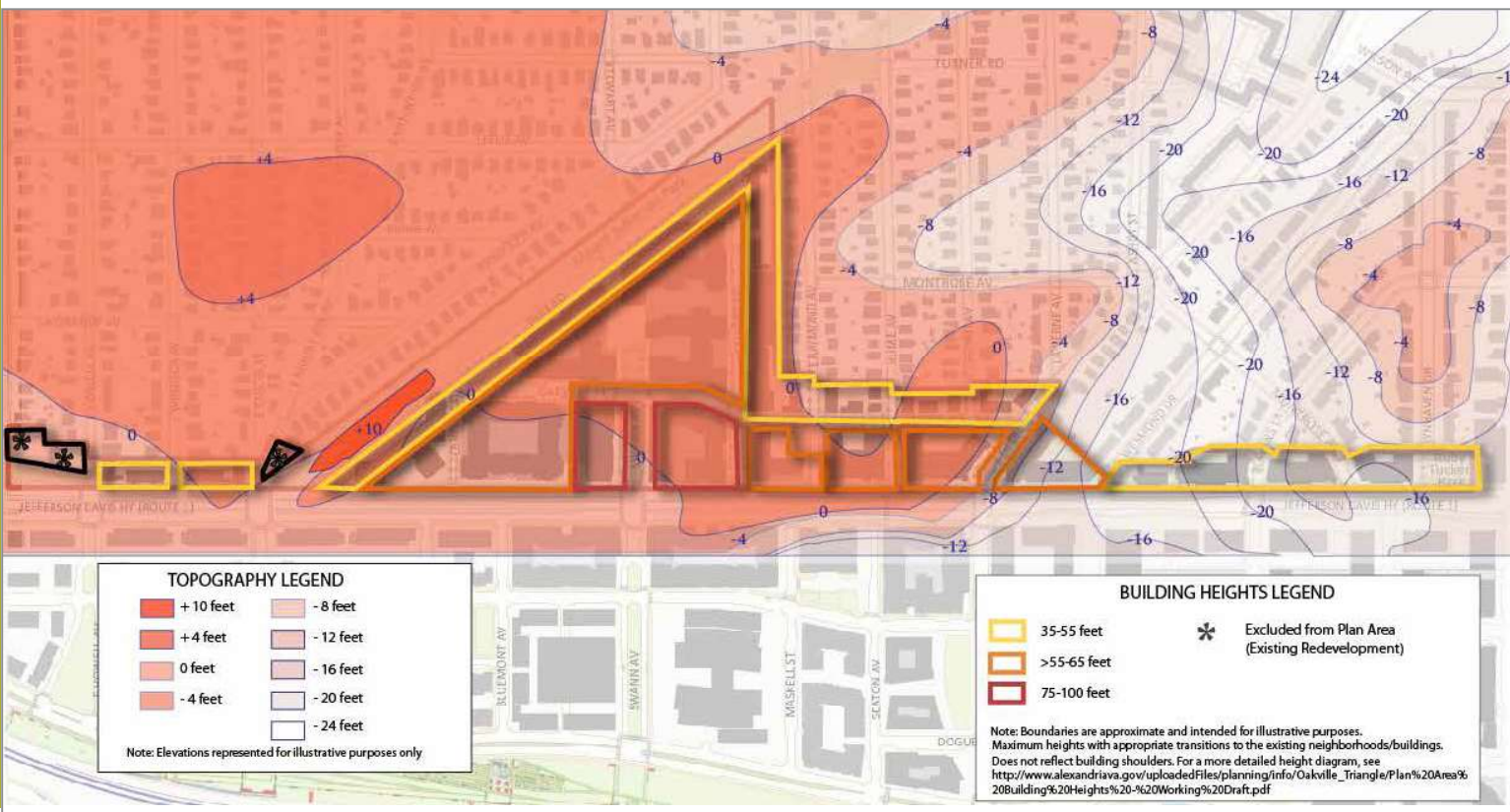
5PM - FALL



4PM - WINTER



PLAN AREA BUILDING HEIGHTS AND TOPOGRAPHY STUDY



MULTIMODAL TRANSPORTATION STUDY

A. [MULTIMODAL TRANSPORTATION STUDY \(MARCH 2015\)](#)

B. [MULTIMODAL TRANSPORTATION STUDY APPENDICES \(MARCH 2015\)](#)

REFERENCE ATTACHED DOCUMENTS via HYPERLINKS

WWW.ALEXANDRIAVA.GOV/76672

RETAIL MARKET ANALYSIS

[OAKVILLE TRIANGLE / ROUTE 1 CORRIDOR PLANNING AREA STUDY | APRIL 27, 2015](#)

Prepared by Delta Associates for StonebridgeCarras

REFERENCE ATTACHED DOCUMENT VIA HYPERLINK

WWW.ALEXANDRIAVA.GOV/76672

INTENTIONALLY LEFT BLANK

OAKVILLE TRIANGLE & ROUTE 1 CORRIDOR

City of Alexandria, VA

