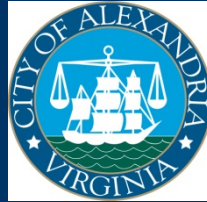


Planning for Transportation-related Impacts

Braddock Metro Neighborhood Plan Worksession #4

January 7, 2008

David Dixon FAIA, Goody Clancy and Brandon Nevers, Yolanda Takesian and Phill Worth
from Kittelson Associates



Tonight's agenda

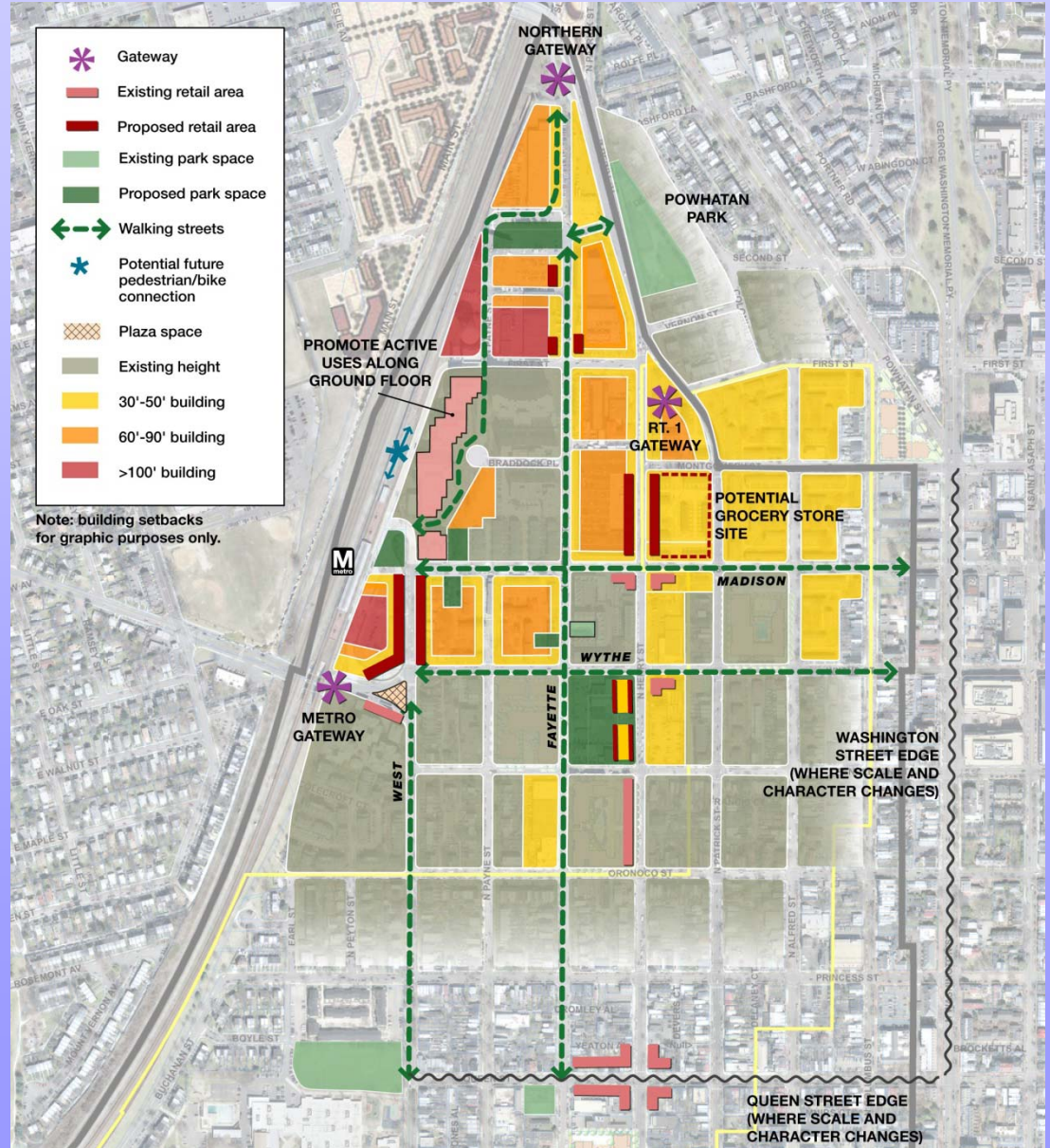
- Brief summary of Worksessions #1 – 3
- Development program
- Five key messages to take from this meeting
- Traffic impact assessment
- Overview of Transportation Demand Management (TDM) programs
- Q & A
- Transportation priorities small-group exercise



Worksession #3 on December 13

Developing an Urban Design Framework

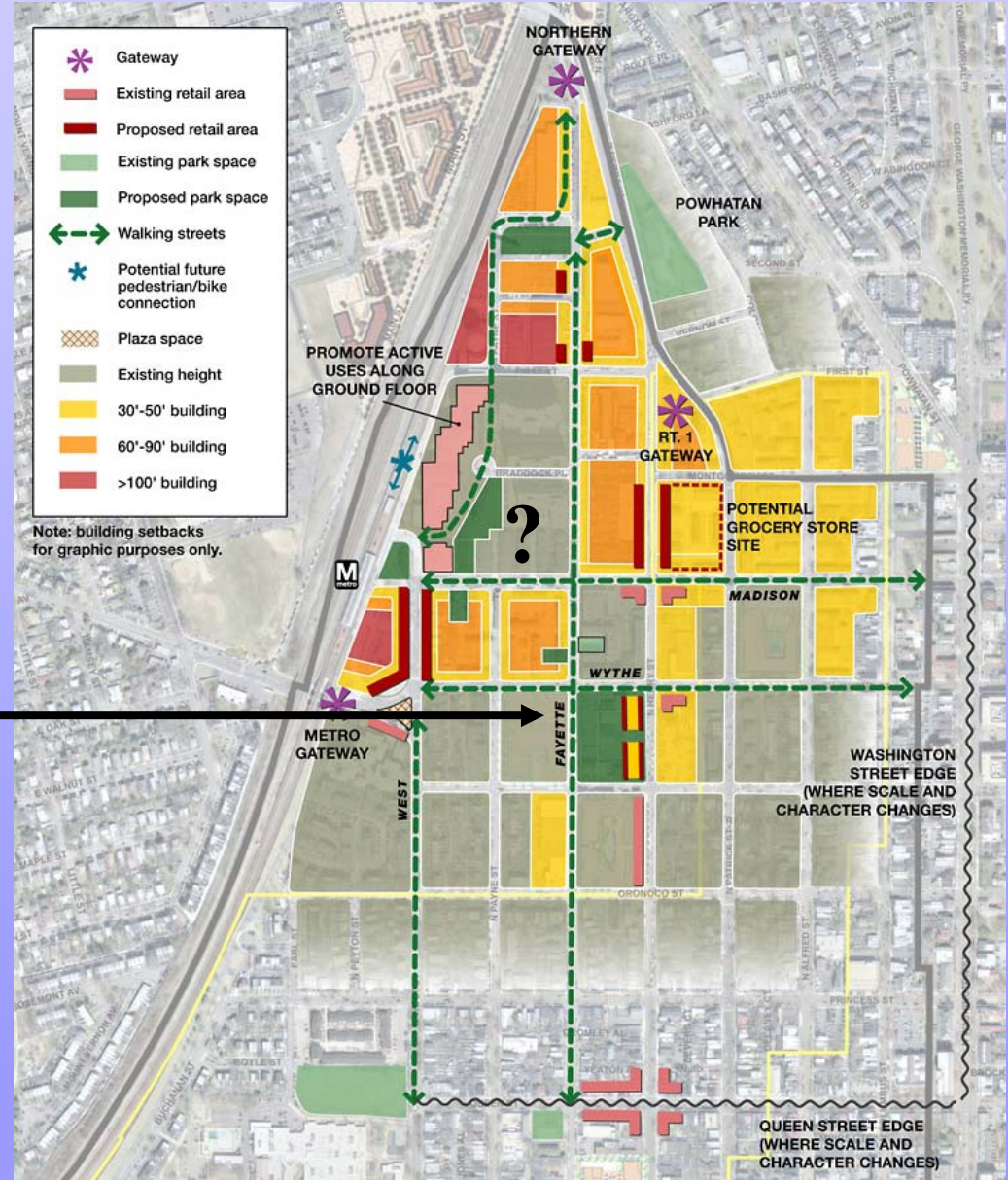
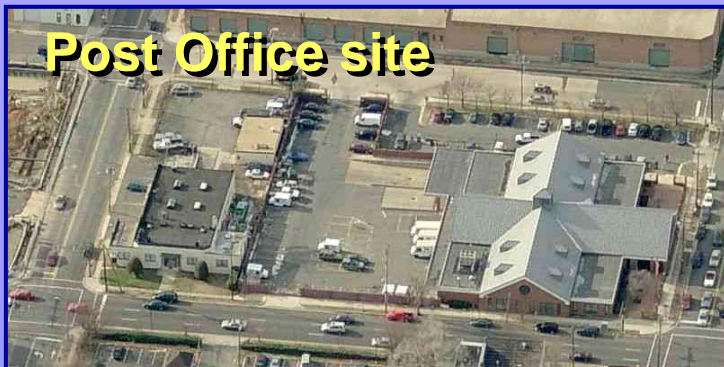
Worksessions #1 - 3



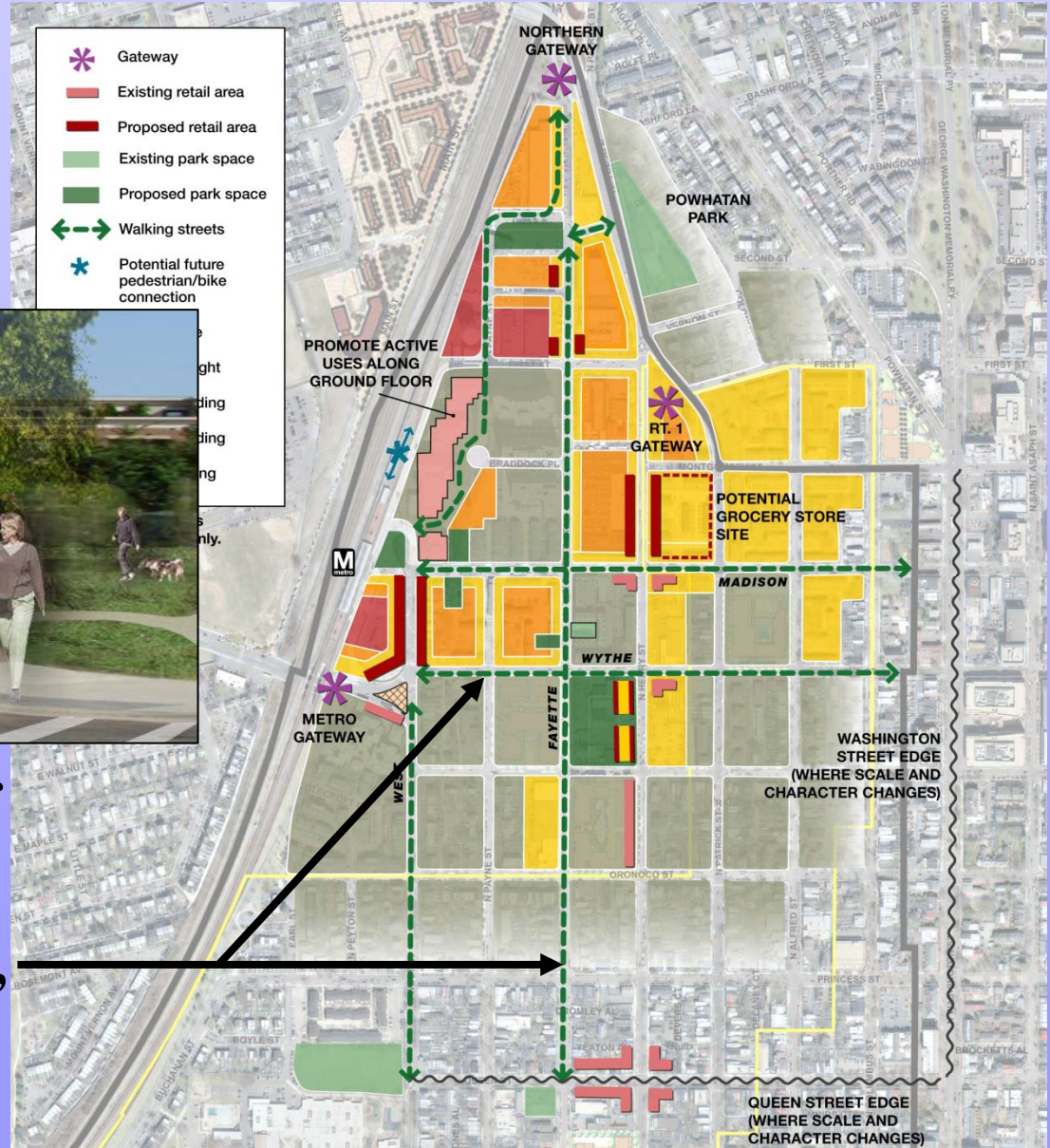
Developing an Urban Design Framework



Element 1: A major new neighborhood park



Developing an Urban Design Framework

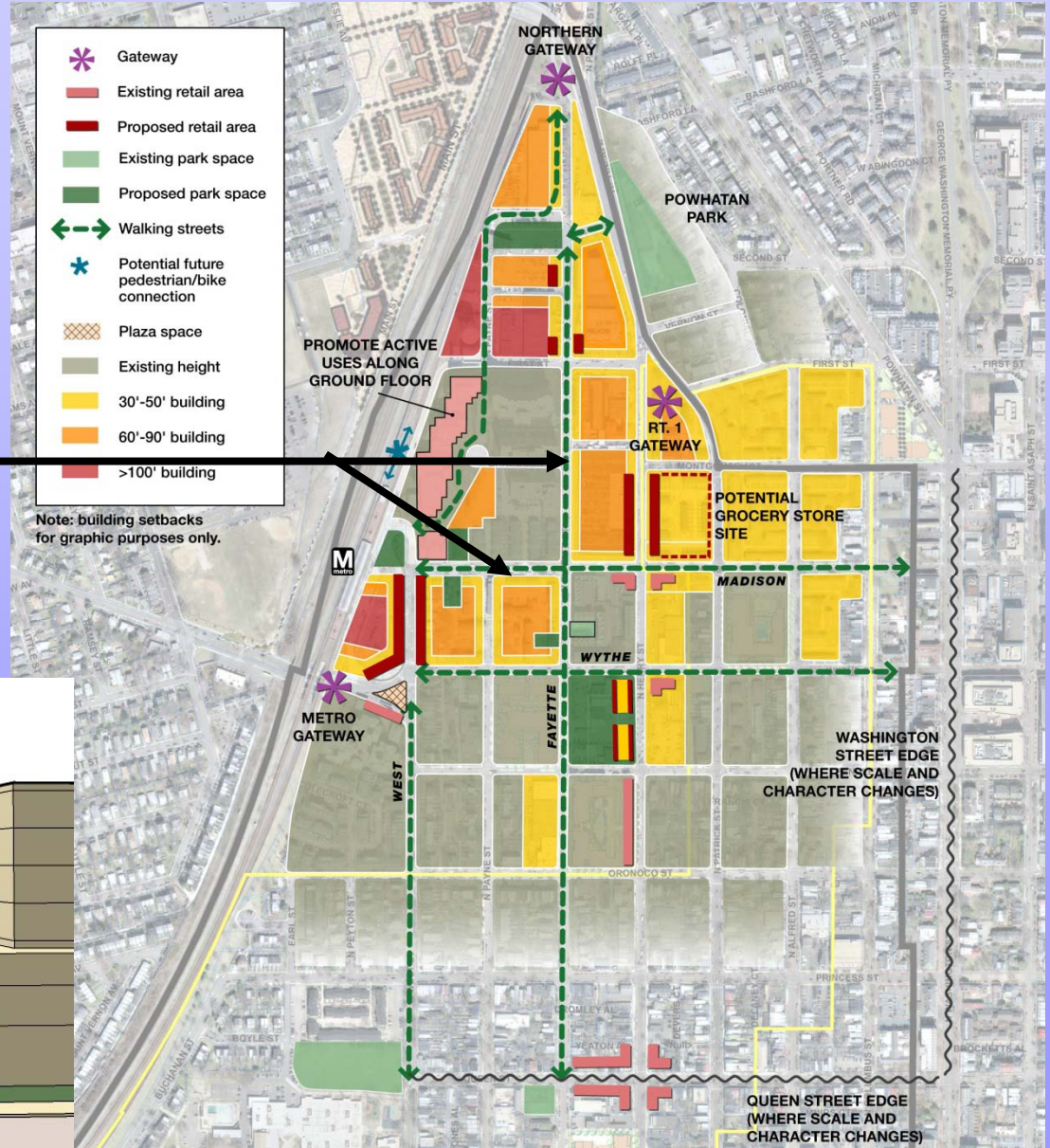


Element 2: A network of “walking streets” (Fayette, West, Madison, Wythe)

Developing an Urban Design Framework



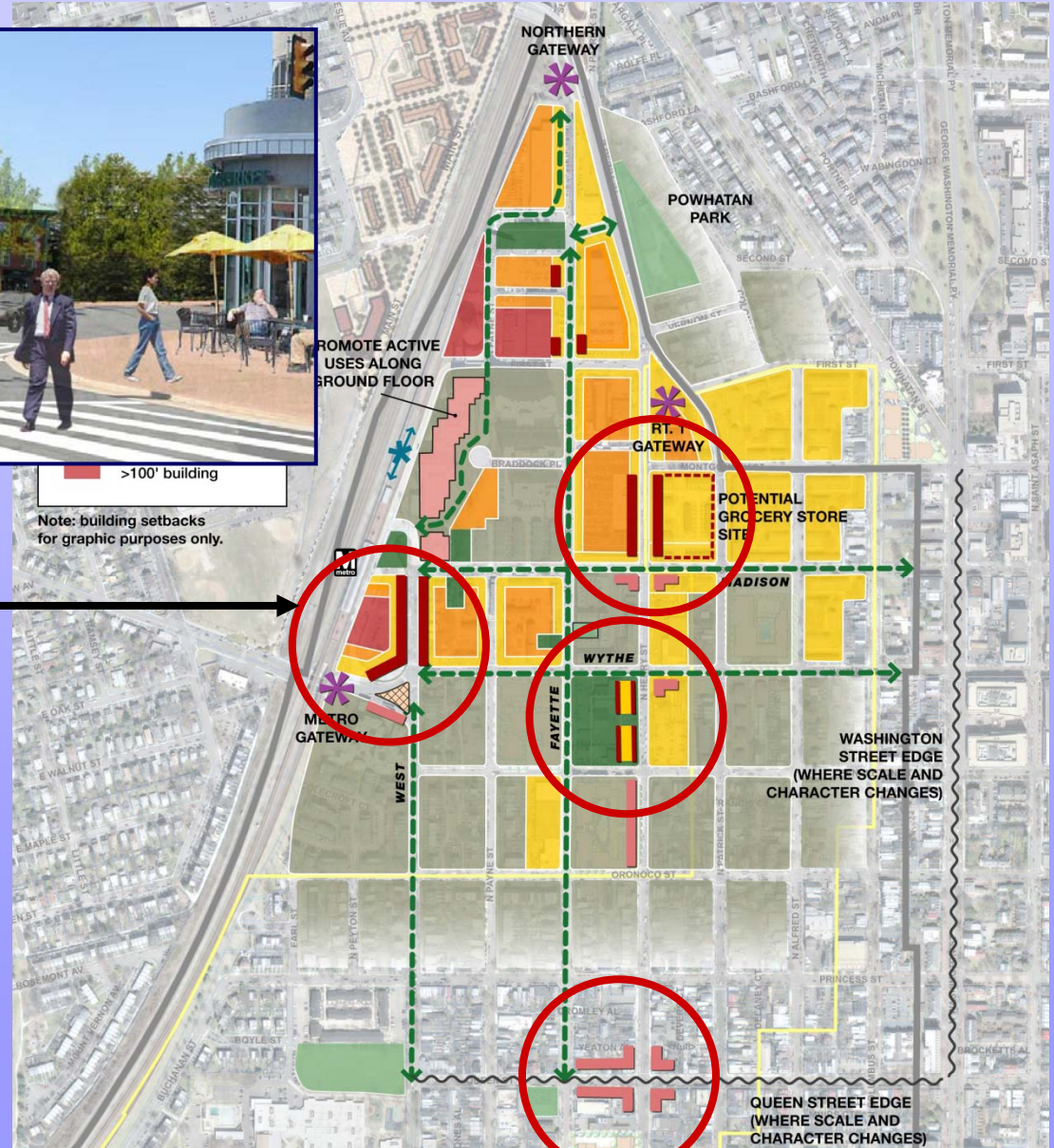
Element 3: “shoulder” buildings along the “walking streets”



Developing an Urban Design Framework



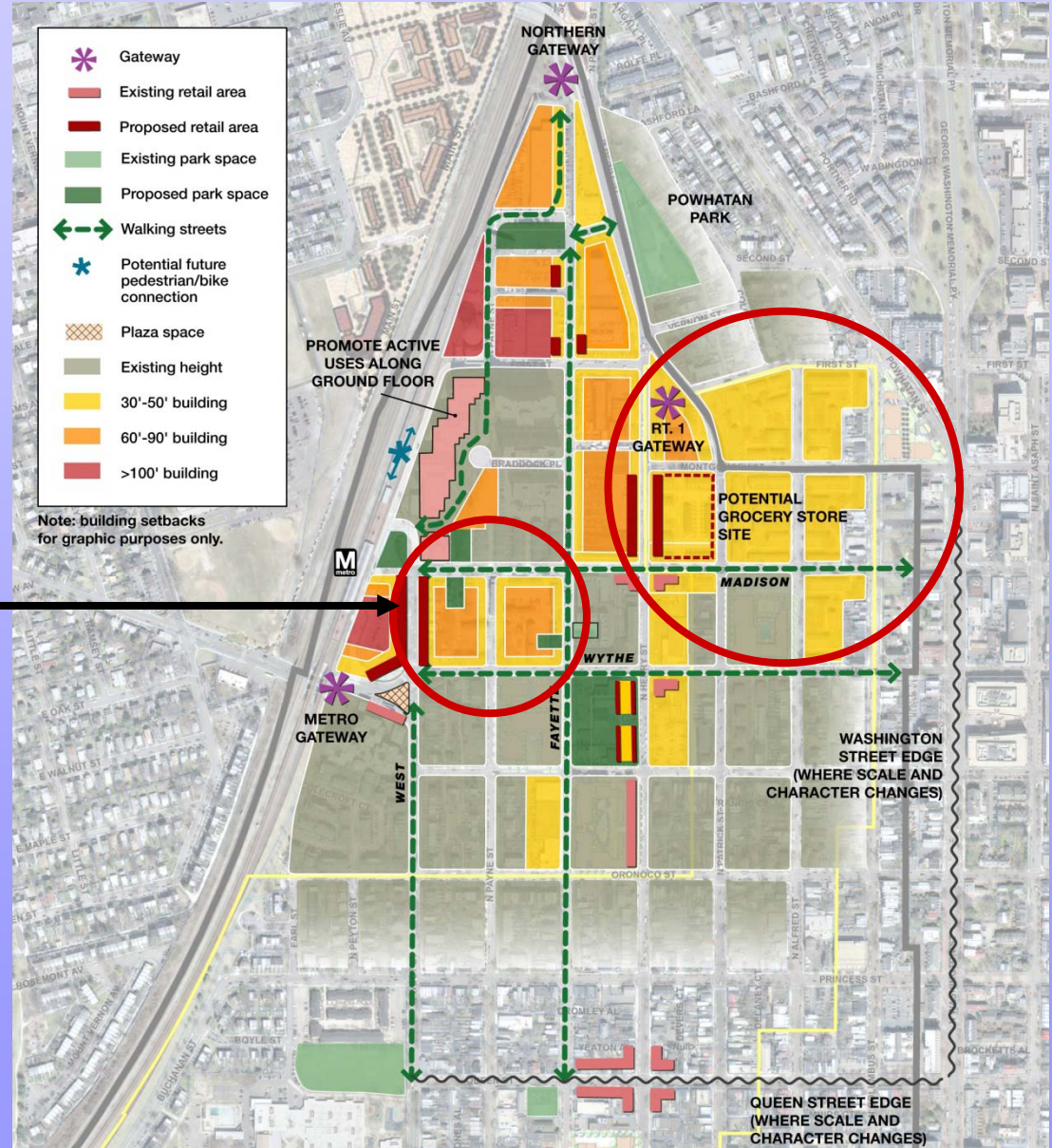
Element 4: Retail and community culture



Developing an Urban Design Framework



Element 5: Planning for mixed-income housing



Developing an Urban Design Framework

Element 6: Height and massing strategy

Strong community support:

- 3-4 story shoulder buildings along walking streets
- Taller buildings that produce more significant public benefits are more appropriate for Jaguar site and Metro

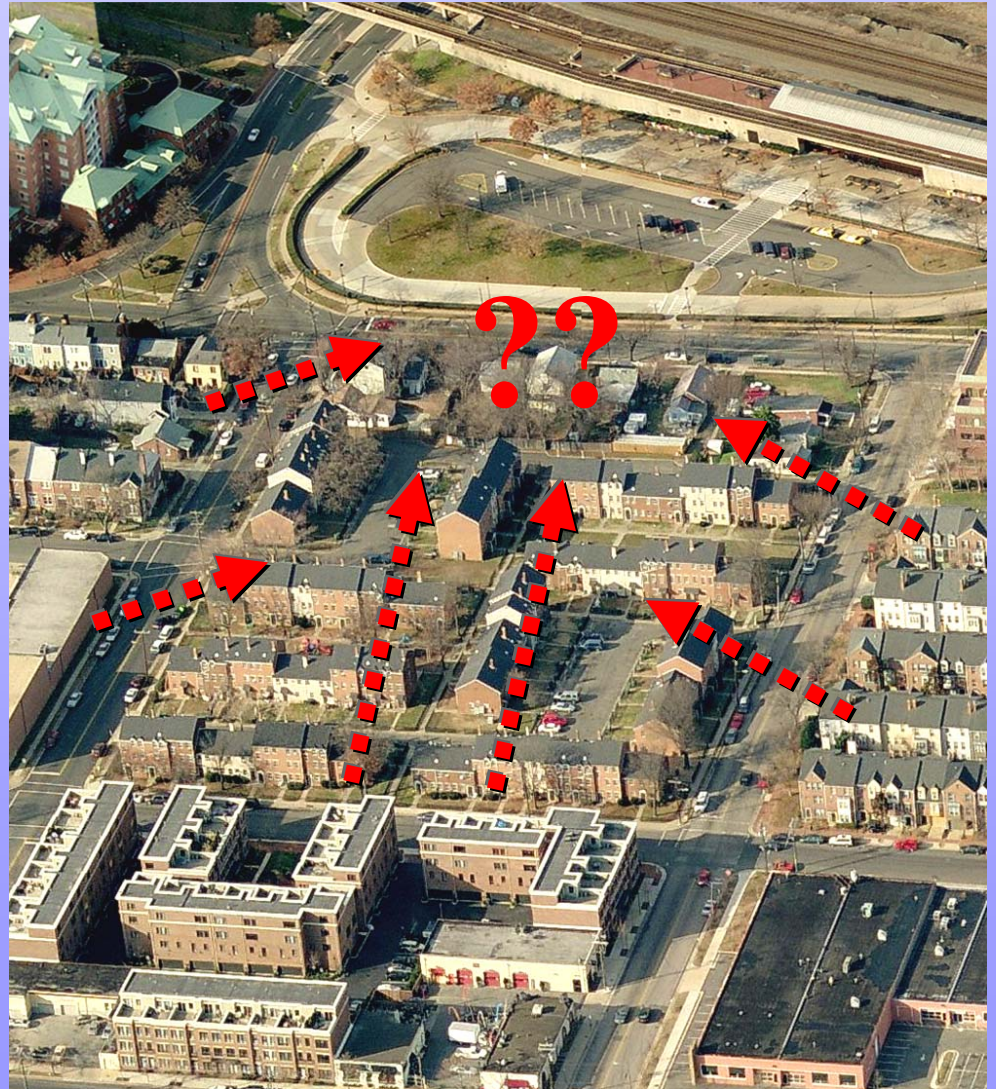


Developing an Urban Design Framework

Element 6: Height and massing strategy

Community concerns:

- Mixed feelings about 90' tall buildings on Adkins block and 120' tall building on the Metro site



Developing an Urban Design Framework

Element 7: Appropriately-scaled, mixed-income housing

Tent City – Boston, MA

- 269 units (25 % low-income, 50 % moderate-income, 25 % market-rate housing)
- 3 acres
- 89 units/acre



Developing an Urban Design Framework

Element 7: Appropriately-scaled, mixed-income housing



Boston view



Alexandria view??

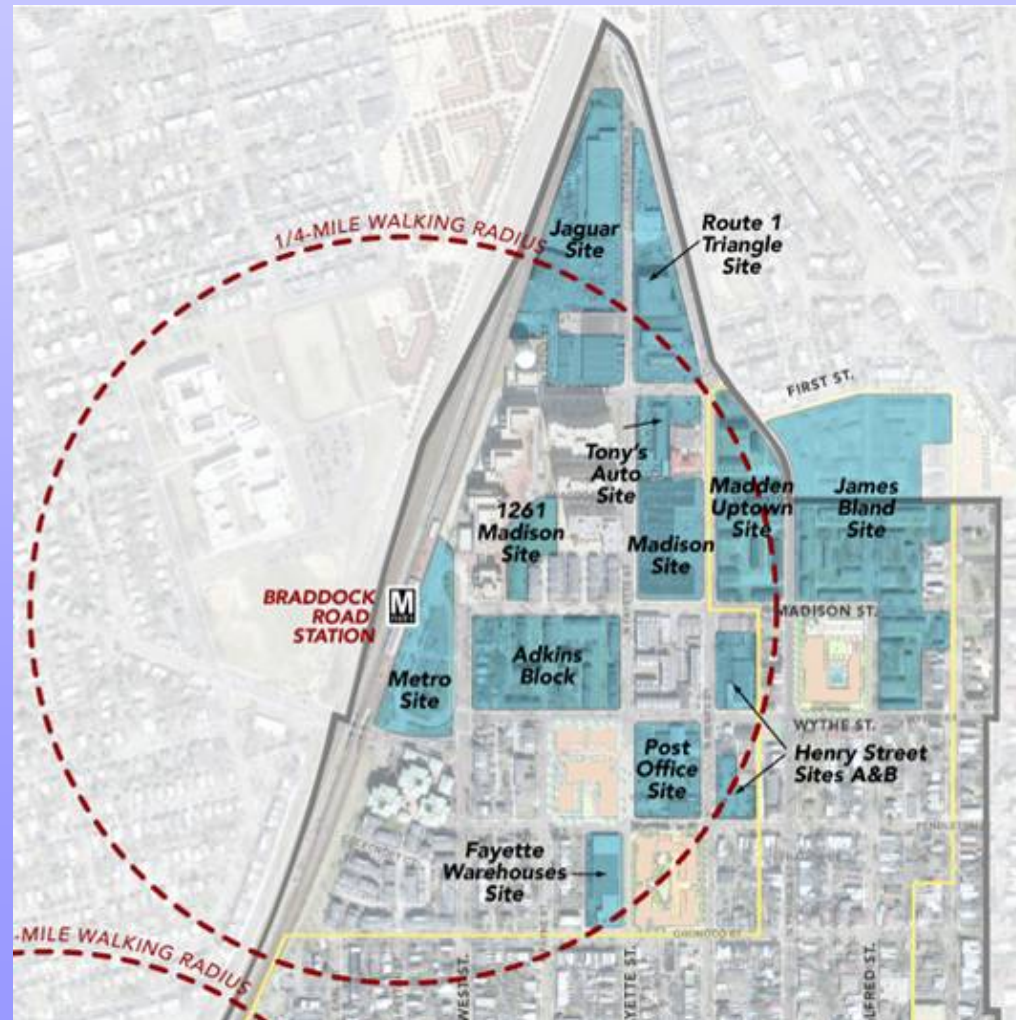
Developing an Urban Design Framework

Element 7: Appropriately-scaled, mixed-income housing



Development program

HOW DO THESE CONCEPTS TRANSLATE TO A DEVELOPMENT PROGRAM??



Development program

	<u>Existing Development</u>	<u>BY-RIGHT 20-year Redevelopment Scenario</u>	<u>DRAFT Plan 20-year Redevelopment Scenario</u>	<u>CURRENT Plan 20-year Redevelopment Scenario</u>
Resid. (units):	3,100...plus:	1,200 add'l <i>(replaces <400)</i>	2,100 add'l <i>(replaces <400)</i>	2,400 add'l <i>(replaces <400)</i>
Total (gsf):	3.6 mill...plus:	1.73 mill. add'l (replaces 700,000 gsf, including 378,000 gsf of public housing: Adkins, Madden & Bland)	2.67 mill. add'l (replaces 470,000 gsf, 148,000 gsf of public housing: Adkins only)	3.14 mill. add'l (replaces 700,000 gsf, 378,000 gsf of public housing: Adkins, Madden & Bland)

Five key messages about transportation to take away from this meeting

1. Because of capacity constraints on Rt. 1 and elsewhere, any increase in local traffic volume due to new development helps to “replace” regional traffic
2. In our collective judgments, traffic 10 or 20 years from now will not feel significantly different to most residents
3. Traffic impacts should not be the primary criterion when evaluating development projects
4. Transportation Demand Management (TDM) programs will make a difference and contribute—along with the other proposed amenities—to creating a livable neighborhood
5. The City is committed to monitor the traffic and parking impacts and ensure that the programs designed to mitigate traffic are working as planned

Ground rules for the break out groups

1. One person speaks at a time
2. No side conversations
3. Be brief and to the point (no speeches)
4. Treat each person's ideas respectfully
5. Be positive and solution-oriented
6. Take responsibility to hear everyone's views
in your group

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